


Memorandum

CS Memorandum No. 1471

To: Distribution

From: Beverly Ward, CMB/Risk Management Assistant 

Date: June 07, 2013

Reference: Project No. M544.1, Contract No. CS-149
Task No. 1-8.02, Change Control

Subject: Configuration Management Board Meeting No. 128

Attached please find minutes for Configuration Management Board Meeting No. 128 held on June 5, 2013.

Attachments: CMB Meeting No. 128 Rev. 0 Minutes with attachments

Cc: David Kuehn, STV (w/attachments) david.kuehn@stvinc.com
Brad Lebovitz, STV (w/attachments) bradley.lebovitz@stvinc.com
Shahnam Farhangi, SFMTA (w/attachments)
Eric Stassevitch, CSP (w/attachments)
Jane Wang, SFMTA (w/attachments)
Aileen Read, CSDG (w/attachments)
CS File No. M544.1.5.0890

Distribution:

Luis Zurinaga, SFCTA luis.zurinaga@sfcta.org
John Funghi, SFMTA
Albert Hoe, SFMTA
Arthur Wong, SFMTA
Roger Nguyen, SFMTA
Ross Edwards, CSP
Richard Redmond, CSP
Mark Benson, CSP
Mark Latch, CSP
Joon Park, SFMTA
Vivian Chow, SFMTA
Brian Kelleher, SFMTA
Sarah Wilson, CSP
Beverly Ward, CSP

CMB Meeting Minutes #128

DATE: June 06, 2013

MEETING DATE: **June 05, 2013**

LOCATION: 821 Howard St, Main Conference Room

TIME: 3:00 PM

ATTENDEES: J Funghi (JF), A. Hoe (AH), R. Nguyen (RN), R Edwards (RE), M. Latch – Part-time (ML), M. Benson (MB), A. Wong (AW), J. Park (JP), R. Redmond, (RR), B. Kelleher (BK), S. Wilson (SW), V. Chow (VC), B. Ward (BW), L. Zurinaga (LZ)

COPIES TO: Attendees: S. Farhangi (SF), Stassevitch (ES), J. Wang (JW), A. Read (AR), B. Lebovitz (BL), D. Kuehn (DK)
File No. M544.1.5.0890

REFERENCE Project No. M544.1, Contract No. 149 Task 1-8.02
Final Design

SUBJECT: Configuration Management Board Meeting # 128 – Rev. No. 0

RECORD OF MEETING *(Italicized text indicates status update of open items)*

ITEM #	DISCUSSION	ACTION BY DUE DATE
7-04/03/13	1252 – R. Edwards presented PCC 012 - Revised Tunnel Alignment for approval of the Re-profile of tunnel to avoid the micropile installed under CN1251. In response to RFI 179 this PCC directs the Contractor to use the revised micropile northbound and southbound profile to avoid conflict with existing micropile. The change is required so that TBM and Tunnel structure is below previously installed micro-piles (see attached). The CMB recommend a request be made to the Contractor to segregate all changes so the information may be included in the documentation regarding the Designers E&O issue. Also the CMB need evidence that the design change has been routed and is signed off by the various disciplines as backup documentation for the files. Notification of the profile change will be submitted to BART and the IRP. Status: <i>06/05 CS Letter No. 1972 Tunnel Design Changes (see attached) sent to the Independent Review Panel (IRP) notified them of recent changes and developments in the tunneling contract. The CMB follow up action item is considered CLOSED.</i>	
8-04/03/13	1252 – Design Services During Construction - M. Benson presented for a later discussion in more detail a summary of CS155-2 (DP2) Task16 Design Services expended for subtask 16.30 and 16.40. Demonstrating actual cost to date of design services expended during the construction phase. The CMB recommend the chart be expanded showing the total budget for the each individual task in column format, also a breakout of the submittal description and detailing the RFI category type reviewed under the headwalls. In addition information regarding hours spent on activities not included in 16.30 and 16.40. The CMB also requested the same level of effort be presented for review for Design Contract CS155-1 (DP1). This item will be brought back to the next CMB meeting. 04/10 M. Benson reported a modification to the DP2 Design Services chart was updated expanding the summary to demonstrate the burnout of the individual task/categories. Further	MB 06/12/13

ITEM #	DISCUSSION	ACTION BY DUE DATE
	<p>investigation still needs to take place to resolve some issues with a conflicting burn rate between the actual progress payment invoice submitted and documentation submitted by the Designer prior to being forward to the Deputy Program Manager for review. A detail presentation will be given at next week's meeting of DP2 and DP1 cost breakdown by category. <i>04/17:</i> An expanded summary spreadsheet was presented showing the break out of the categories for work under subtask 16.30 and 16.40 for - work completed to date, remaining work and reasons for overruns to support the reason for request for additional \$10K in funds. The CMB suggested the CM team put the Contractor on notice that any submittal regarding design substitution for review will be at the Contractor's expense. In addition the CMB requested a presentation of the DP1 Task 16 Design Services hours and cost detail summary also be brought to the CMB for review when finalize. This item will be brought back to next week's CMB. <i>Status: 06/05 This item was not discussed at this week's meeting.</i></p>	
<p>1-05/08/13</p>	<p>1252- Submittal 31 74 17-025 Segments Spalls Repair Procedure, Rev 2 was presented for approval of response comments to SFMTA's review of Rev1 for the proposed Spalling Repair Procedure for stress relief portions of the tunnel segments (yellow and superficial) spalling. The Contractor submitted a revised submittal to address SFMTA's seven points of concerns (see attached). The CMB took exception to the revised submittals deficiency in demonstrating the repair procedure and missing information recommending the proposed design mix for the repairs. In addition a second Submittal 31 74 17-026 Segment Liner Alternate Repair Method data sheet proposing a new repair product <i>CTS Rapid Set Mortar Mix</i> as a possible alternate patching material for minor segment chips/spalls to the previously proposed <i>Speedcrete Red Liner Morta</i> was submitted. The CMB recommends that the CM Team request revised submittals demonstrating a clear line of responsibility for the design liability and a comprehensive and cohesive approach to repairs that specifically respond to the original comments. This item will be brought back to the CMB at a later date. <i>Status: 06/05 This item was not discussed at this week's meeting.</i></p>	<p>ML 06/12/13</p>
<p>2-05/08/13</p>	<p>1252 – M. Benson requested approval for negotiation position for COR79 Compensation Grout – Old Navy additional cost associated with inefficiencies of installing the compensation grout tubes. Justification was based on the premise that the larger piece of equipment could have performed the work if the in place utilities had been found as shown in the Bid documents. Because of the actual configuration of underground utilities was much different than that shown on the Bid documents, payment is warranted for the inefficiencies caused by utilizing the smaller piece of equipment. The CMB raised several questions about the graphical depiction of the As Bid condition and requested that the graphical representation be properly updated to show the As Bid condition. Similarly, the actual condition graphic could be improved to better show actual conditions found. Detailed information demonstrating the cost of the inefficiency appear in order, however the requested NTE amount was not properly justified compared to the computed cost by the Engineer. The CMB was unable to provide approval due to the still open question that the utility obstructions potentially indicate the Contractor should have brought in a smaller rig to begin with. CMB suggested improved graphics and supporting information be developed to justify the requested negotiating position. This item will be brought back to the CMB next week. <i>05/15</i> A revised graphic was shown depicting the actual condition shown found incorporating the second PG&E 12" electrical line. Further determination from the BP dwgs indicates the Contractor could have actually accomplished the work by using the original larger Klemm machine as listed in the bid proposal. AGREE – CMB 0117 Not to exceed value of \$152,000 to be negotiated as a lump sum for inefficiencies associated with having to use a smaller drill rig to work around the existing utilities. In addition the CM Team will obtain the invoice from the Contractor to identify the actual cost for the rental of the smaller Comacchio drill rig. <i>Status: 05/29 Documentation was presented to showing the monthly rental rate for the Comacchio rig. The CMB took</i></p>	

ITEM #	DISCUSSION	ACTION BY DUE DATE
	<p><i>exception to the documentation presented stating the information describes an Equipment agreement value and not a monthly rental invoice. The CMB requested the RE obtain from the Contractor an actual invoice which can be compared to the cost estimate as validation of the rental rate. This item will be brought back to the CMB at a later date. Status: 06/05 Documentation was presented to show the 1st month invoice billed to the Contractor's sub consultant for the rental equipment of the Comacchio rig at \$18K a month. The CMB follow up action item is considered CLOSED.</i></p>	
<p>5- 05/08/13</p>	<p>1252 – M. Benson requested negotiation position for PCC 06 cost comparison of Additional BART Tunnel Instrumentation (see attached). The CMB expressed concerned that the \$624K price to perform the work was too high and requested Jane Wang to contact the IRP in writing to find out if the additional instrumentation monitoring was still warranted given the new lowered tunnel alignment and the cost to implement the work. The RE will confirm the date for which a final decision by SFMTA is to be made to forward to the Contractor. This item will be brought back to the CMB at a later date. Status: 06/05 CS Letter No. 1972 - Tunnel Design Changes (see attached) sent to the Independent Review Panel (IRP) notified them of recent changes and developments in the tunneling contract. In addition SFMTA requested the IRP's reconsideration of concurrence of the need for the three additional tests: bolt pre-load tension, load cells on the bolts, and the rail deflection under train load. SFMTA recommends eliminating these additional instrumentation measures and utilize the instrumentation as included in Contract 1252. This item will be brought back to the CMB at a later date.</p>	<p>JW 06/12/13</p>
<p>1- 06/05/13</p>	<p>1252 – S. Wilson presented BIH's subcontractor Wang Technology's billing rates for PCC06 - Geotechnical Instrumentation and Monitoring work for acceptance of professional services fully burdened hourly rate above prevailing wage proposed by the Contractor. Work required by Wang falls outside of the traditional "prevailing wage" work and as dictated by the Contract under General Provisions "Work Performed by Special Forces (GP 6.05.C)" may be allowed. The CMB recommended that the RE request from Wang a series of old invoices from a prescribed period and their pre published rates prior to negotiation of lump sum cost agreement is reached on PCC 06. This item will be brought back to the CMB. Status: 06/05 This item was not discussed at this week's meeting.</p>	<p>SW 06/12/13</p>
<p>1-</p>	<p>1252 – S. Wilson and A. Wong presented PCC 013 – SFWD Excavation, Support and Backfilling restoration work of 2 each locations to facilitate fitting and valve installation work by SFWD (see attached). The change is required due to the existing water main being in the footprint of the UMS station piles and is to be abandoned. Work is required in order to allow for SFWD to install swing check valves to facilitate fire service connection cutover and cutting off of the existing water main. The CMB agreed was in agreement with the change. The described work must be completed by the end of June 2013. A cost estimate for the work is pending.</p>	
<p>2-</p>	<p>1252 - S. Wilson presented Evacuation of Merit of COR 037 – 12" abandoned steel pipe located at the North UMS headwall – Contractor encountered 12" pipe within the utility demolition limits. Removal of the pipe was necessary for headwall construction. CMB agreed with merit for differing and unforeseen site condition work associated with removal of 12" pipe.</p>	
<p>3-</p>	<p>1254 - R. Edwards presented ECP FD-038 – Elongated Sidewalk Bulb-out at Chinatown Station previously viewed at CMB meeting #100. At the request SF Planning Department and in close conformance with the City's General Plan to create a "station plaza" to extend the planned pedestrian curb at the southwest corner of Washington St. 130 feet to accommodate a bus stop is included in Contract 1300 Addendum No. 3. In addition SF Planning request to install street trees along Stockton Street adjacent to the Chinatown Station and trees along Washington Street is also included in Contract 1300 Addendum No. 3. There are no schedule impact identified and construction cost impact is ROM \$36,200. AGREE – CMB 0121. The CMB contends the Study for utilization of the elongated bulb-out described above as a bus stop is not a Central Subway expense and a</p>	

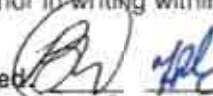
ITEM #	DISCUSSION	ACTION BY DUE DATE
	funding source from SFMTA Planning will need to be established in order to authorize and implement the estimated 60 Hrs. needed for the design study of bus door/mirror tree/bike rack coord.	
4-	The Program Trend/Change Control Log dated 06/05 was not viewed at this week's meeting. The log is updated to include the current items trending for Contract 1252 and is attached for distribution with these meeting minutes.	

ACTION ITEMS

ITEM #	MTG DATE	MTG ACTION DATE	DESCRIPTION	BIC	DUE DATE	STATUS
1	11/07/12	11/07/12	1300 – 1254 SF Planning Request - Cost	RE	06/05/13	CLOSED
7	04/03/13	04/03/13	1252 – PCC 12 – Advise BART and IRP	RE	06/05/13	CLOSED
8	04/03/13	04/03/13	1252 – DP2/DP1 – Design Services hours and cost during construction – DP1 Review	MB	06/12/13	Open
1	05/09/13	05/09/13	1252 - Spalls Repair Procedure - Revision	ML	06/12/13	Open
5	05/09/13	05/09/13	1252 – PCC 06 – Contact IRP – Revisit Monitoring Instrumentation plan	JW	06/12/13	Open
2	05/15/13	05/15/13	1252 – COR 079 – Rental Invoice for Comacchio rig	MB/SW	06/05/13	CLOSED
1	05/29/13	05/29/13	1252 – PCC 06 – Wang Technology's Special Forces - invoices from similar projects	SW	06/12/13	Open

Meeting adjourned at 5:00PM

These meeting minutes have been prepared by B. Ward and reviewed by, R. Edwards and are the preparer's interpretation of discussions that took place. If the reader's interpretation differs, please contact the author in writing within four (4) days of receipt of these minutes.

Signed  [initials of preparer & reviewer] Date: 6/7/2013 [Date review completed]

Meeting Agenda

Project No. M544.1, Contract No. CS-149
Program/Construction Management
Configuration Management Board (CMB) Meeting No. 128
June 05, 2013
3:00pm – 5:00pm
 Central Subway Project Office
 821 Howard St. 2nd Floor
 Main Conference Room

Attendees:

Mark Benson	John Haley	Mark Latch	Eric Stassevitch
Vivian Chow	Albert Hoe	Brad Lebovitz	Beverly Ward
Ross Edwards	Brian Kelleher	Roger Nguyen	Sarah Wilson
Shahnam Farhangi	Jim Kelly	Joon Park	Arthur Wong
John Funghi	David Kuehn	Richard Redmond	Luis Zurinaga

1. **1252** – PCC 13 - SFWD Excavation, Support and Backfilling – *Merit Approval*
 – ~~PCC 06 – Wang Technology Billing Rates – (update) For Approval~~
 – COR 037 - 12” steel pipe removal at the North UMS headwall – *Merit Approval*

2. **1300** – (1254) CTS - Sidewalk Bulb out (SW corner Stockton/Washington) - ECP FD-0038 – *Approval*
 (Previously presented at CMB Mtg #100)

3. **Trend/Change Log** – 06/05/13

4. **Other Business** – Action items – PCC 12 & 06 - Advise BART and IRP – **Tunnel re-design/Addl Monitoring**
 – CN1300 -1254 (CTS) SF Planning Request – **Cost/Schedule impacts**
 – COR 079 - Comacchio Rental Equipment

Meeting Attendance Sheet

Project No. M544.1, Contract No. 149
 Program/Construction Management
 Configuration Management Board Meeting No. 128
 June 05 2013
 3:00 p.m. – 5:00 p.m.
 Central Subway Project Office
 821 Howard, 2nd Floor
 Main Conference Room

Deliver Meeting Attendance Sheet with original signatures/initials to Document Control.

NAME	AFFILIATION	PHONE	E-MAIL (for minutes)	INITIALS
Please enter initials if your name is listed below. Please enter name, affiliation, phone number and email address if your name is not listed below.				
Benson, Mark	CSP	(415) 701-4295	Mark.Benson@sfmta.com	MCB
Chow, Vivian	SFMTA	(415) 701-5264	Vivian.chow@sfmta.com	VC
Edwards, Ross	CSP	(415) 701-5296	Ross.Edwards@sfmta.com	RE
Farhangi, Shahn timer	SFMTA	(415) 554-0721	Shahn timer.Farhangi@sfmta.com	
Funghi, John	SFMTA	(415) 701-4299	John.Funghi@sfmta.com	JF
Haley, John	SFMTA		John.Haley@sfmta.com	
Hoe, Albert	SFMTA	(415) 701-4289	Albert.Hoe@sfmta.com	AH
Kelleher, Brian	SFMTA	(415) 701-5289	Brian.Kelleher@sfmta.com	BK
Kelly, Jim	SFMTA		Jim.Kelly@sfmta.com	
Kuehn, David	STV/PMOC	(510) 464-8053	David.kuehn@stvinc.com	
Latch, Mark	CSP	(415) 701-5294	Mark.Latch@sfmta.com	ML
Lebovitz, Brad	STV/PMOC	(510) 464-8052	Bradley.lebovitz@stvinc.com	

NAME	AFFILIATION	PHONE	E-MAIL (for minutes)	INITIALS
Nguyen, Roger	SFMTA	(415) 701-4312	Roger.Nguyen@sfmta.com	Rd
Park, Joon	SFMTA	(415) 701-4742	Joon.Park@sfmta.com	Jd
Redmond, Richard	CSP	(415) 701-4288	Richard.Redmond@sfmta.com	PR
Stassevitch, Eric	CSP	(415) 701-4426	Eric.Stassevitch@sfmta.com	
Ward, Beverly	CSP	(415) 701-5291	Beverly.Ward@sfmta.com	(Bw)
Wilson, Sarah	CSP	(415) 243-0950	Sarah.Wilson@sfmta.com	SW
Wong, Arthur	SFMTA	(415) 701-4305	Arthur.Wong@sfmta.com	AW
Zurinaga, Luis	SFCTA	(415) 716-6956	Luis.zurinaga@sfcta.org	ZL

PROPOSED CONTRACT CHANGE

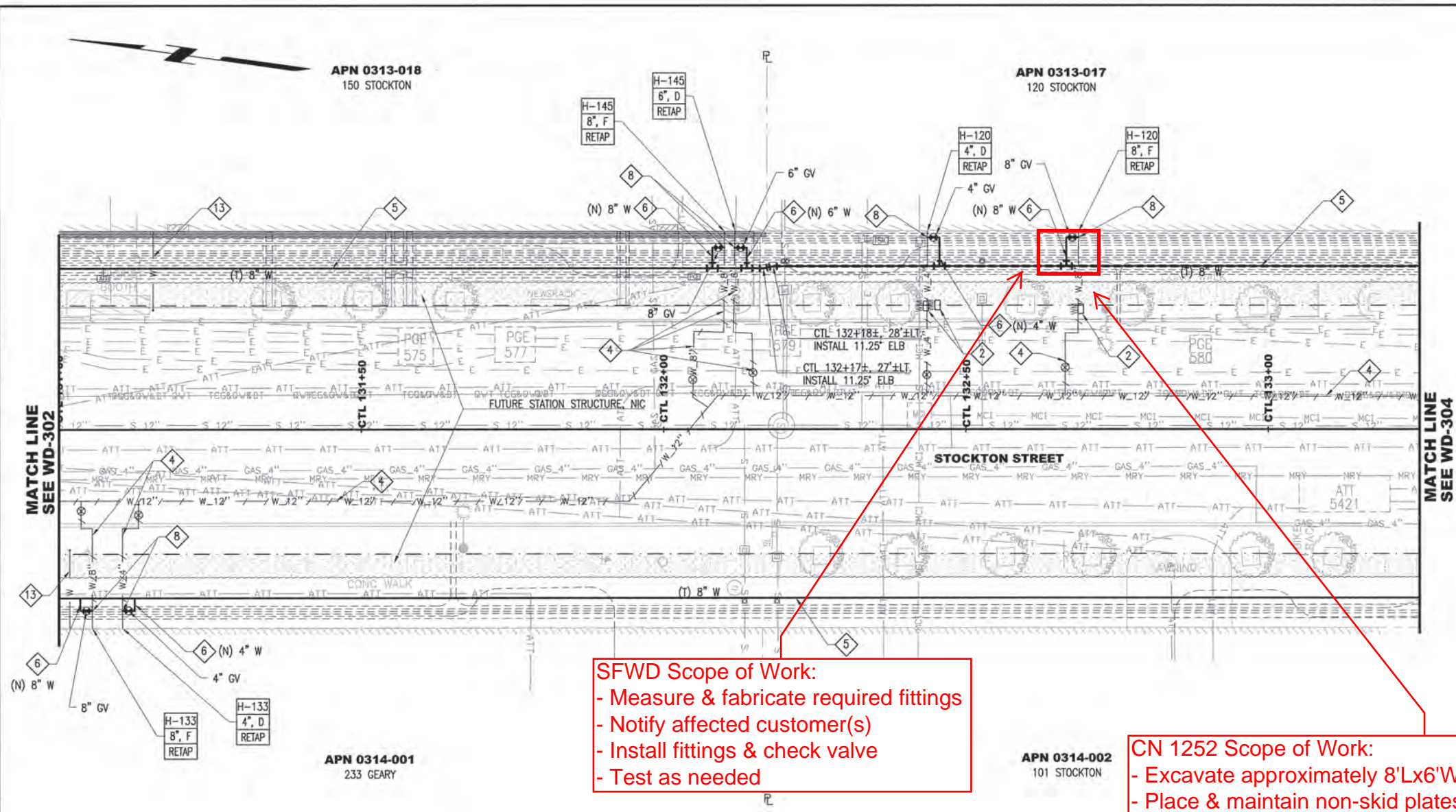
Contract No. 1252 Tunnels Date 6/03/2013
 PCC No. 1252-13
 PCC Title SFWD Excavation, Support and Backfilling

Description of PCC:	
<u>Summary of Changes required at the 2EA SFWD Work Locations:</u>	
<p>1 Perform excavation, support and restoration work at 2EA locations as required to facilitate fitting/valve installation work to be performed by the San Francisco Water Department (SFWD). Work includes, but is not limited to, the following activities:</p> <ul style="list-style-type: none"> a. Establish pedestrian/traffic control b. Demolition and removal of existing asphaltic concrete/street base/sidewalk c. Excavation d. Installation of shoring as required to support open excavation e. Installation of non-skid steel plates (including two removal/re-installations as requested by SFWD personnel) f. Backfilling of excavated areas after completion of SFWD work activities g. Restoration of street/sidewalk <p>Refer to the attached Field Drawings for approximate locations and excavation dimensions. Actual locations and dimensions to be dictated by SFWD personnel.</p>	
Spec Ref.	CN 1252 Tunnels
Drawing Nos.	WD-303 Rev.0, WD-304 Rev.0
Attachments	FS 1252-051, FS 1252-052
<p>The Contractor's proposal in price and time is required on the this proposed contract change to the subject contract in accordance with the General Provisions Section 6.02 B, within 14 days after receipt of a PCC.</p>	

Recommended by:  Date 6/5/13
 Sarah Wilson, Resident Engineer (SFMTA Representative)

Concur in Principle:  Date 6/5/13
 Mark Benson, Construction Manager (SFMTA Representative)

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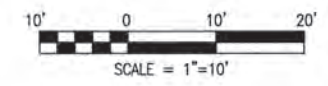


SFWD Scope of Work:
 - Measure & fabricate required fittings
 - Notify affected customer(s)
 - Install fittings & check valve
 - Test as needed

CN 1252 Scope of Work:
 - Excavate approximately 8'Lx6'Wx4'D hole
 - Place & maintain non-skid plates (2 ea.)
 - Remove & re-place plates for SFWD crew (2 occasions)
 - Backfill hole
 - Restore sidewalk

Field Sketch No. FS 1252-051
 PCC 13

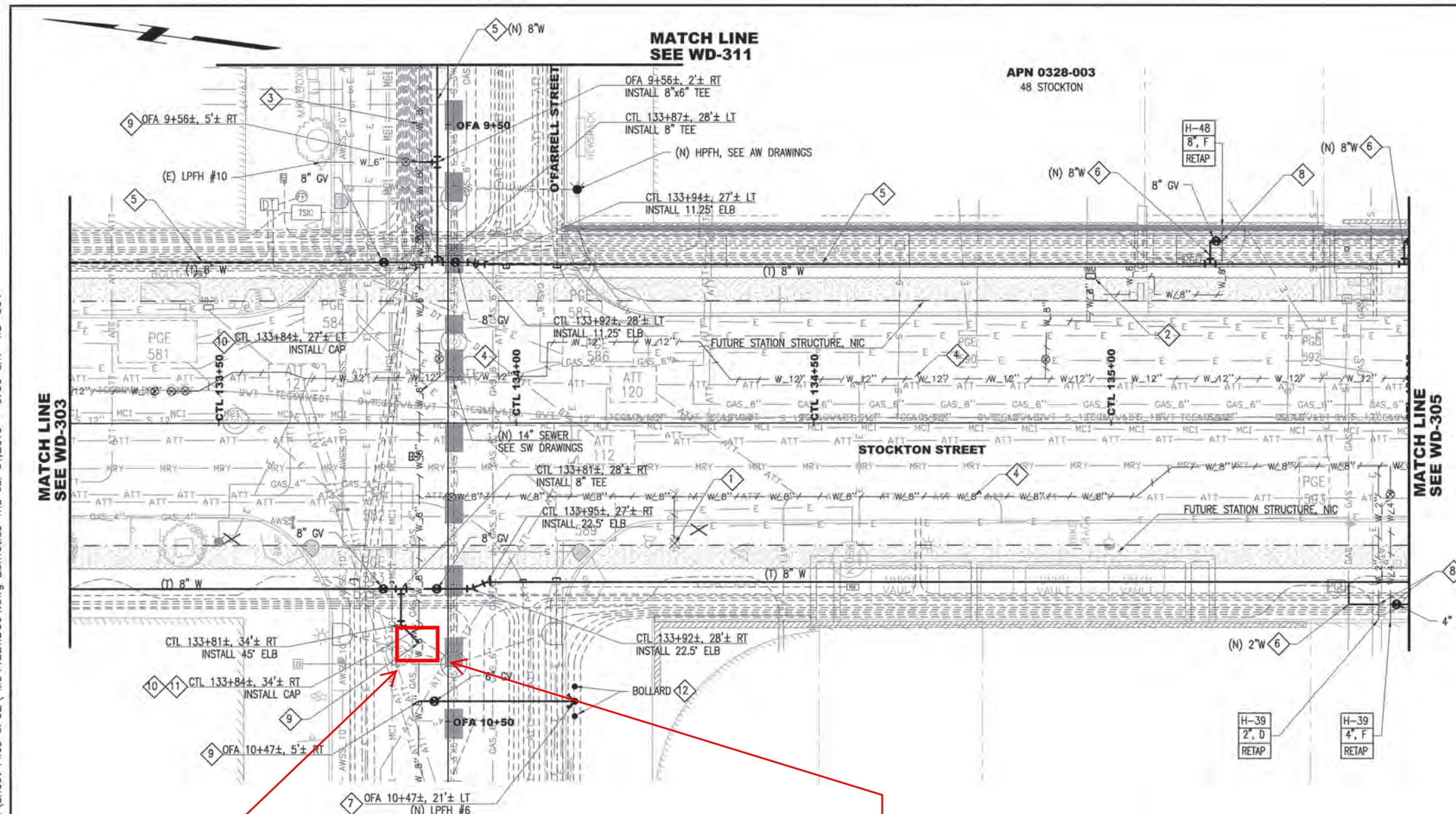
- NOTES:**
1. R/S (E) LPFH. CONTRACTOR SHALL COORDINATE AND EXCAVATE SIDEWALK AT THE HYDRANT TO THE EXTENT NECESSARY FOR SFWD TO REMOVE THE FIRE HYDRANT AND BREAKAWAY RISER. SEE WD-001 GENERAL NOTE 17.
 2. R/S (E) WM BY SFWD. ABANDON WM BOX IN PLACE.
 3. R/C (E) WATER PIPE.
 4. ABANDON (E) WATER PIPE, SEE DRAWING NO. WD-001 GENERAL NOTES 15 AND 16.
 5. (N) OR (T) WATER MAIN.
 6. (N) BUILDING SERVICE, SEE DRAWING NO. WD-001 GENERAL NOTE 8A.
 7. (N) OR (T) 6" LPFH ASSEMBLY, SEE DRAWING NO. WD-001 GENERAL NOTE 8B.
 8. RECONNECT TO (E) WATER LATERAL BY SFWD.
 9. CONNECT TO (E) WATER MAIN BY SFWD.
 10. DISCONNECT AND TERMINATION OF (E) WATER LINE BY SFWD.
 11. FUTURE EXTENSION.
 12. (N) SIDEWALK BOLLARD PER CCSF-DPW STANDARD PLAN NO. 87,178. CONTRACTOR TO MAINTAIN A MINIMUM 3'-0" CLEARANCE AROUND (N) LPFH.
 13. (E) PRIVATE FLOWER STAND WATER SERVICE, SEE DRAWING NO. WD-001 GENERAL NOTE 18.



		DESIGNED: DZ DRAWN: FB CHECKED: FR REVIEWED: MC RECOMMENDED: RE APPROVED: RE DATE: JUL 8 - 2010			CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED: EXECUTIVE DIRECTOR/CEO	THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION UTILITIES RELOCATION WATER DISTRIBUTION CTL 131+00 TO 133+25	CONTRACT NO. 1251 SFMTA CONTROL NO. CL-17833 DRAWING NO. WD-303 SHEET NO. 118	REVISION 0
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BORDER REVISED 09/30/08

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- NOTES:**
- R/S (E) LPFH. CONTRACTOR SHALL COORDINATE AND EXCAVATE SIDEWALK AT THE HYDRANT TO THE EXTENT NECESSARY FOR SFFD TO REMOVE THE FIRE HYDRANT AND BREAKAWAY RISER. SEE WD-001 GENERAL NOTE 17.
 - R/S (E) WM BY SFWD. ABANDON WM BOX IN PLACE.
 - R/C (E) WATER PIPE.
 - ABANDON (E) WATER PIPE, SEE DRAWING NO. WD-001 GENERAL NOTES 15 AND 16.
 - (N) OR (T) WATER MAIN.
 - (N) BUILDING SERVICE, SEE DRAWING NO. WD-001 GENERAL NOTE BA.
 - (N) OR (T) 6" LPFH ASSEMBLY, SEE DRAWING NO. WD-001 GENERAL NOTE BB.
 - RECONNECT TO (E) WATER LATERAL BY SFWD.
 - CONNECT TO (E) WATER MAIN BY SFWD.
 - DISCONNECT AND TERMINATION OF (E) WATER LINE BY SFWD.
 - FUTURE EXTENSION.
 - (N) SIDEWALK BOLLARD PER CCSF-DPW STANDARD PLAN NO. 87,178. CONTRACTOR TO MAINTAIN A MINIMUM 3'-0" CLEARANCE AROUND (N) LPFH.
 - (E) PRIVATE FLOWER STAND WATER SERVICE, SEE DRAWING NO. WD-001 GENERAL NOTE 18.

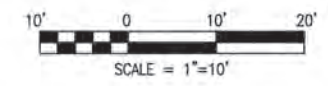
SFWD Scope of Work:

- Measure & fabricate required fittings
- Cut & cap (E) main

CN 1252 Scope of Work:

- Excavate approximately 8'Lx5'Wx4'D hole
- Place & maintain non-skid plates (2 ea.)
- Remove & re-place plates for SFWD crew (2 occasions)
- Backfill hole
- Restore roadway

**Field Sketch No. FS 1252-052
PCC 13**



7/8/10 ISSUED FOR BID DATE	 TELAMON ENGINEERING CONSULTANTS INC.	DESIGNED: DZ DRAWN: FB CHECKED: FR REVIEWED: MC RECOMMENDED: MF APPROVED: RE DATE: JUL 8 - 2010	 	CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED: [Signature] EXECUTIVE DIRECTOR/CD	THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY UNION SQUARE/MARKET STREET STATION UTILITIES RELOCATION WATER DISTRIBUTION CTL 133+25 TO 135+50	CONTRACT NO. 1251 SFMTA CONTROL NO. CL-17834 DRAWING NO. WD-304 SHEET NO. 119	REVISION 0
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FINDING OF FACTS

Contract No. & Name: 1252 - Tunnels Date: 05/31/2013
 PCC No. & Title: 1252-13
 Initiator (Name & Dept): Sarah Wilson, Central Subway Project

What (Description of PCC)

Perform excavation, support and restoration work at 2EA locations as required to facilitate fitting/valve installation work to be performed by the San Francisco Water Department (SFWD). Work includes, but is not limited to, the following activities: Establish pedestrian/traffic control, Demolition and removal of existing asphaltic concrete/street base/sidewalk, Excavation, Installation of shoring as required to support open excavation, Installation of non-skid steel plates (including two removal/re-installations as requested by SFWD personnel), Backfilling of excavated areas after completion of SFWD work activities, Restoration of street/sidewalk.

Why (Reason for Requesting PCC)

The work contained herein is required in order to allow for SFWD personnel to install swing check valves. Installation of said valves will facilitate fire service connection cutover and cutting off of the existing water main. *The existing water main is in the footprint of the UMS Station piles and is to be abandoned.*

Where (Location, Station)

Work to take place at 2EA locations within the Project Limits. Approximate Stations and Offsets are as follows:

Location 1 (sidewalk): CTL 132+67±, 30'±LT
 Location 2 (street): CTL 133+84±, 37'±RT

When (Occurrence date, action date, potential time impact)

The SFMTA is requiring that the work described herein be completed by the end of June 2013. No Contract time impact is anticipated at this time.

How (Recommendation of Action)

Complete work described herein within listed timeframe.

Estimate (Summary of Engineer's Estimate w/EE attached)

\$TBD

Spec Ref: GP 6.02

Drawing Nos.: FS 1252-051, FS 1252-052

The Contractor's proposal in price and time is required on this proposed contract change to the subject contract in accordance with the General Provisions Section 6.02 B within 14 days after receipt of a PCC.

Prepared by:


 Sarah Wilson, Resident Engineer
 SFMTA Representative

6/5/13
 Date

SFMTA Contract No. 1252

Contractor:

Barnard Impregilo Healy JV (BIH)

EVALUATION OF MERIT


COR 037


Recommendation: Accept justification of Merit for COR 037, for the removal of an abandoned 12" steel pipe at the North UMS headwall. There is no time extension requested or recommended for this work.

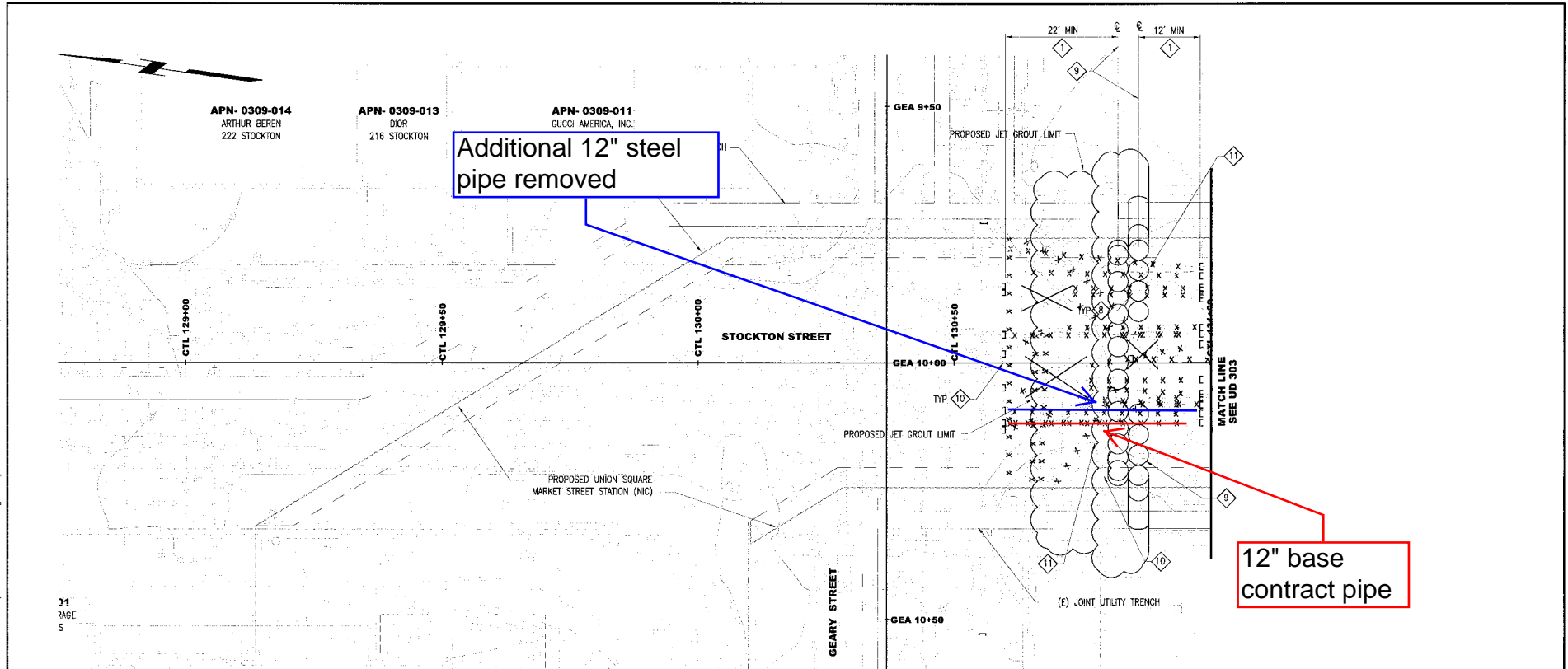
Facts: On August 15th 2012, the Contractor discovered an abandoned 12" steel pipe within the utility demolition limits at the UMS North headwall. On August 16th the Contractor removed the pipe from the utility demolition limits. See attached Field Sketch FS 1252 – 053.

Justification: The Contract Drawings, specifically UD-302 rev.0, did not show the abandoned 12" pipe which constitutes a differing and unforeseen site condition. Removal of the pipe was necessary for headwall construction at this location. SFMTA inspectors have verified that additional labor and equipment resources were required to facilitate removal of the abandoned 12" pipe.

Change Type – (2) Unforeseen Condition

By:  6/5/13
 Sarah Wilson Date
 Resident Engineer

 6-5-13
 Configuration Management Board Date
 Approval

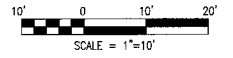


01
PAGE
5

12" base contract pipe

Additional 12" steel pipe removed

Field Sketch No. FS 1252 -053
COR 037



CONFORMED

- NOTES:**
- DEMOLISH AND REMOVE ALL ABANDONED UTILITY FACILITIES, INCLUDING PREVIOUSLY ABANDONED FACILITIES NOT SHOWN ON PLANS, AT JET GROUT OR HEADWALL LIMIT.
 - ACTIVE UTILITIES SHALL BE MAINTAINED UNTIL RELOCATION OR CUT OVER, UNLESS OTHERWISE NOTED. COORDINATE DEMOLITION WITH HEADWALL CONSTRUCTION PHASING.
 - PROTECT (E) SEWER.
 - SEE (WD) DRAWINGS FOR ABANDONMENT AND REMOVAL OF WATER FACILITIES.
 - SEE (AW) DRAWINGS FOR ABANDONMENT AND REMOVAL OF AWSS FACILITIES.
 - PROTECT (E) AT&T FACILITIES.
 - PROTECT (E) JOINT TRENCH.
 - CAP OR PLUG ALL ABANDONED FACILITIES INCLUDING PREVIOUSLY ABANDONED FACILITIES NOT SHOWN ON PLANS AT JET GROUT LIMIT.
 - PROPOSED STATION HEADWALL. SEE (ES) DRAWINGS FOR LOCATION AND DETAILS. CONTRACTOR SHALL VERIFY THE HEADWALL LOCATION PRIOR TO COMMENCEMENT OF DEMOLITION.
 - TEMPORARILY REMOVE (E) STREET LIGHT AND TRAFFIC SIGNAL AND REINSTALL AFTER HEADWALL CONSTRUCTION.
 - CONTRACTOR TO PROVIDE SHORING AND REMOVAL OF (E) BASEMENT WALL TO ACCOMMODATE HEADWALL CONSTRUCTION AND JET GROUTING. COORDINATE WITH SFMTA CN 1251 SD DRAWINGS (SFMTA CL-17767 TO 17776) AND CN 1251 CONTRACTOR'S AS-BUILTS FOR REFERENCE.

CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY		APPROVED EXECUTIVE DIRECTOR/CEO		THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY TUNNELS		CONTRACT NO. 1252		DRAWING NO. CL-18027		DRAWING NO. UD-302		SHEET NO. 80		REV. NO. 0		DATE 07/21/2011		ISSUED FOR BIDDING		DATE 05/18/2010	

\\s6lnas003\ms644\1\Final\Design\Drawing-821\2-Civil\Utility Demolition\Sheet Files CP03\M54423UD302.dwg FBoyami Wed Jan 26, 2011 - 1:25 pm UD-302



Base Contract
12" Steel Pipe

Additional 12"
Steel Pipe

North



Base Contract
12" Steel Pipe

08/15/2012



CMB Change No.: CMB - 0121

Initial Implementing Change Control Procedure No.: 1254 - ECP - FD038

GENERAL

Proposed Change Sponsor: Ross Edwards Received by CMB: 06/05/2013
(Date)

Affected Disciplines: DPW Infrastructure Div. Architecture
OCS Utilities
Landscape Architecture Sustainable Streets (Traffic)

Impacts of Change: Elongated Sidewalk Bulb-out Chinatown Station at Stockton St.
(CTS)1254R - 1300 Package

Contract(s) Directly Affected by this Proposed Change:

1250	1251	1252	1253	1254	1255	1256
1 <input type="checkbox"/>	2 <input type="checkbox"/>	3 <input type="checkbox"/>	4 <input type="checkbox"/>	5 <input checked="" type="checkbox"/>	6 <input type="checkbox"/>	7 <input checked="" type="checkbox"/>
(CP01)	(CP02)	(CP03)	(CP04)	(CP05)	(CP06)	(CP07)

CONFIGURATION MANAGEMENT BOARD APPROVALS

Signatures

	Agree with the Change	Disagree with the Change	Date
Senior Program Manager:			
Deputy Program Manager:			
PM Project Services:			
PM Project Development/Delivery:			
SFMTA O & M Manager:			
SFMTA Safety and Security			
SFCTA PMO			5/6/13

Comments

NOT A CENTRAL SUBWAY EXPENSE

STEP 1

Title: Elongated Sidewalk Bulb-out at Chinatown Station		ECP #: FD-038
Submitted By: H. Quon Chin Contract#: CS155.2/1300; Package CTS-1254R Task #: 15.20 Task Name: Post-Design Services	Date: 06/03/2013 11/07/2012-orig.	Signature: 

STEP 2

Change Affects: FEIS/FEIR: Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Design Criteria: Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> PE Package: Drawings, Specifications Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Safety or Security: Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Multiple Disciplines: Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Muni or DPT O&M: Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>	Other Tasks/ Disciplines Affected: Design: DPW Infrastructure Div. Architecture Landscape Architecture Construction: Utilities SFMTA OCS SFMTA Sustainable Streets (Traffic)
Construction Schedule Impact (if any):	None Identified (at close of Bid Phase)
*Design Schedule Impact (if any):	Included in Contract 1300 Addendum
Construction Cost Impact (if any):	Estimated Construction Cost ROM \$36,200 IN CONTRACT 1300 BID
*Design Cost Impact (if any):	Estimated ROM 97 Hrs: DPW Pavement/Sidewalk Design Modification Estimated ROM 60 Hrs: CSDG bus door/mirror/tree/bike rack coord.

NEED ADDITIONAL FUNDING

Description of Change:

To bring the Central Subway Project – Chinatown Station in closer conformance with the City's General Plan, SF Planning Department in its May 7, 2012 GPR Letter (recommends) making design changes, specifically the extension of the sidewalk bulb-out at Stockton Street to help create a "station plaza". This elongated bulbout on the southwest corner (SWC) of Stockton and Washington Streets in front of the station headhouse would also include bike racks, benches, trees and other landscape features.

Street trees are a requirement of Planning Code Section 138.1: requiring one street tree for every 20 feet of frontage, with an additional tree required for every remaining 10 feet of frontage. See reference document "Tree Planting and Protection Check List" cited below. Pursuant to Planning Code section 138.1, the Planning Department has required the installation of five (5) street trees along Stockton Street adjacent to the Chinatown Station (and 3 trees along Washington). Installation of these street trees is a provision of the Planning Department's Notice of Special Restrictions and a condition of the Planning Department's approval to DBI Building Permit #2012 0403 7405.

The new sidewalk extension requires design modification to the sidewalk pavement design, tree locations, final traffic striping, and possibly relocation of an OCS pole and utility work to an existing ductbank. Under

Contract 1300 Addenda, four (4) RP-series drawings were modified by DPW for sidewalk pavement design work:

- RP-102 r2 Plan,
- RP-202 r1 Vertical Alignment,
- RP-401 r1 Sections,
- RP-403 r1 Sections and Details,

in addition to Addenda No. 3 which included LA-100 (r1), AR-114 (r3), AR-115 (r2) and AR-214 (r3).

DPW also prepared a curb profile geometry drawing for a TASC submittal to initiate the sidewalk legislation.

The possible relocation of an OCS pole and utilities (AT&T ductbank, W-6" and cable) need to be assessed during construction at Chinatown Station. Utility relocations in the area of the sidewalk extension were not in Final Design scope of work as the need was not evident from available information; hence potholing investigations for invert (depth) and location of existing utilities will be part of the work required to confirm final placement of trees along Stockton Street.

From a service integration perspective, incorporation of the elongated bulb-out will also provide the opportunity for Central Subway to coordinate a bus stop relocation proposal from SFMTA Service Planning coinciding with the Planning sidewalk extension. The existing southbound bus stop on Stockton Street is presently located mid-block between Jackson and Washington Streets or approximately half a block distance for the connection between bus and rail service. If this bus stop relocation becomes a reality, the coexistence of a bus stop, SFMTA/Muni bus shelter, proposed trees, bike racks and other features requires coordination between Central Subway, SFMTA Service Planning and SF Planning Department.

Reason and Justification for Change:


This change incorporates the conditions for SF Planning Department approvals of:

1. General Plan Referral (GPR) application – Case No. 2011.1202R;
2. Conditional Use Application – Case No. 2012.0641C and adoptions noted in
3. Planning Commission Motion No. 18699, September 06, 2012

Reference Documents:

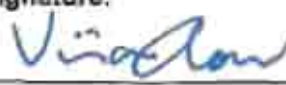
1. **SF Planning Department** GPR Case No. 2011.1202R, Central Subway Project-Chinatown Station, *May 7, 2012*
2. **SF Planning Department** Planning Commission Motion No. 18699, Case No. 2012.0641C, 933-949 Stockton Street, *September 6, 2012*
3. **SF Planning Department** Tree Planting and Protection Checklist, Case No. 2012.0403.7405 – Completed w/DBI Building Permit Application.
4. **Exhibit CTS Trees1** – w/package to Planning Commissioners
5. **Contract 1300/Package CTS-1254R** Architectural Site Plan Drawing AR-100, Rev. 0, Issued for Bid - *October 22, 2012*
6. **Contract 1300/Package CTS-1254R** AR-114 Floor Plan, Surface Level-4, Rev. 3, *Addendum No. 3, January 18, 2013*
Contract 1300/Package CTS-1254R LA-100 Landscaping Planting, Rev. 1, *Addendum No. 3, January 18, 2013*
Contract 1300/Package CTS-1254R RP-102 Pavement Renovation Plan, Rev. 2, *Addendum No. 4, February 15, 2013*

STEP 3

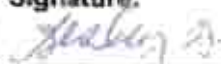
Reviewed By ECP Submittal Design Package Project Manager: Aileen Read, CSDG	Date: 6/5/13	Signature: 
Comments: No objections		
Change Recommended: Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>		


*Design schedule and/or cost impacts provided here are for reference only and acceptance of the subject design change does not constitute acceptance of either the design schedule or design cost impacts.


STEP 4

Reviewed By Project Controls Manager (Cost and Schedule): Vivian Chow, SFMTA	Date: 6/5/2013	Signature: 
Comments:		

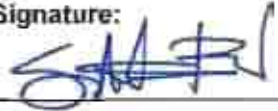
STEP 5

Approved for Distribution By Discipline Lead: Lesley Wong, DPW IDC Hydraulic Engineering	Date: 6/5/2013	Signature: 
Comments: Sidewalk bulbout was expansion was incorporated via addendum. additional sidewalk legislation process still needs to be completed.		
Change Recommended: Yes <input type="checkbox"/> No <input type="checkbox"/>		

Approved for Distribution By Discipline Lead: Mona Tamari, Architectural (Kwan Henmi)	Date: 6/5/13	Signature:  for Mona Tamari
Comments: Bike racks were incorporated via addendum; any revisions to accommodate bus stop would need to be addressed via construction change order		
Change Recommended: Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>		

Approved for Distribution By Discipline Lead: Charu Sharma, Miles Stevens Architect (LA)	Date: 6/5/13	Signature: 
Comments: Additional trees (including coordination with architect) were incorporated via addendum. Any revisions to accommodate bus stop would need to be addressed via construction change order		
Change Recommended: Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>		

STEP 6

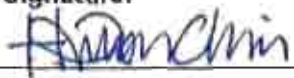
Reviewed By Impacted Contract/Design Package Project Manager: Aileen Read, CSDG (DP2)	Date: 6/5/13	Signature: 
Comments: No objections (still)		
Change Recommended: Yes <input type="checkbox"/> No <input type="checkbox"/>		

Reviewed By Impacted Contract/Design Package Project Manager:	Date:	Signature:
Comments:		
Change Recommended: Yes <input type="checkbox"/> No <input type="checkbox"/>		


Reviewed By Impacted Contract/Design Package Project Manager:	Date:	Signature:
Comments:		
Change Recommended: Yes <input type="checkbox"/> No <input type="checkbox"/>		

Add as many Contract/Design Package Project Manager review blocks as necessary

STEP 7

Reviewed By SFMTA Design Manager: H. Quon Chin, CSP	Date: 6/5/2013	Signature: 
Comments:		
Change Recommended (If yes, forward to Project Manager): Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>		

STEP 8

Reviewed By Project Controls Manager (Cost and Schedule): Vivian Chow, SFMTA	Date: 6/5/2013	Signature: 
Comments:		
Change Recommended: Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>		

STEP 9

Reviewed By Program Manager Project Development: Ross Edwards <i>[Signature]</i>	Date: 6/5/13	Signature: <i>[Signature]</i>
Comments:		
Change Approved: Yes <input type="checkbox"/> No <input type="checkbox"/> Change to be forwarded to CMB: Yes <input type="checkbox"/> No <input type="checkbox"/>		

Verification of Design Change Implementation	
The undersigned verifies that changes have been made to all documents impacted by the above approval design change.	
Design Package: <input type="checkbox"/> 1 <input checked="" type="checkbox"/> 2 <input type="checkbox"/> 3	
Design Package QC Manager: <i>Changes for trees & bike racks addressed in Addendum No. 3 (Contract 1300); changes to accommodate bus stop, if feasible, will be by construction change order.</i>	Aileen Read, CSDG Print <i>[Signature]</i> Signature Date 6/5/13

ECP-FD038

Attachment 1

SF Planning Case No. 2011.1202R
General Plan Referral

Chinatown Station



SAN FRANCISCO PLANNING DEPARTMENT

May 7, 2012

Mr. John Funghi
San Francisco Municipal Transportation Agency
One South Van Ness, 7th Floor
San Francisco, CA 94103

**RE: CASE NO. 2011.1202R - CENTRAL SUBWAY PROJECT
Chinatown Station (future terminus of Third Street Light Rail Phase 2)**

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

Dear Mr. Funghi:

The Department received your request for a General Plan Referral as required by Section 4.105 of the Charter and Section 2A.53 of the Administrative Code.

PROJECT DESCRIPTION

The proposed Central Subway Project is the second phase of the San Francisco Municipal Transportation Agency's (SFMTA) Third Street Light Rail Project. The Central Subway Project will extend Muni transit service improvements from the present terminus of the Third Street Light Rail Line at Fourth and King Streets through South of Market, Downtown terminating in Chinatown. The proposed project under review is the Chinatown Station, located at 935 Stockton Street, at Washington.

The Central Subway Project was reviewed for consistency with General Plan policies and with the Eight Priority Policies of the Planning Code Section 101.1 in General Plan Referral Case 2008.0849R. However, that referral noted that numerous project elements that have the potential to impact land use, urban design features and historic resources had not yet been developed to a level required for a conformity determination, and stated that all above grade structures as well as the interface between Central Subway elements and the street including subway entrances would need to be reviewed by the Planning Department in a separate General Plan referral.

This General Plan referral covers only the proposed subway station located at 935 Stockton Street, and its associated public realm improvements; it does not cover any additional development that may be located on the site in the future.

ENVIRONMENTAL REVIEW

The Project was reviewed as part of the Central Subway Final Supplemental Environmental Impact Statement/Supplemental Environmental Impact Report (FSEIS/FSEIR) and was certified by the Planning Commission on August 7, 2008 and approved by the SFMTA Board on August 19, 2008. The SEIS/SEIR identified impacts resulting from project construction including noise, dust,

vibration, historic resources impacts, and transit/traffic operational impacts. Subsequent evaluation of the project by the Planning Department found that the current proposal is compliant with the requirements of the Central Subway Final SEIS/SEIR, including mitigation measures to lessen the significance of the loss of a contributing building within a historic district.

GENERAL PLAN CONSISTENCY

The Chinatown Station has been reviewed for consistency with General Plan policies and with the Eight Priority Policies of the Planning Code Section 101.1 and the findings are attached (in Attachments 3 and 4, respectively). In general, we find the project to be **in conformance** with the City's *General Plan*; however, making the following minor design changes to the proposed project would bring the project in closer conformance with the *General Plan*.

- No bike parking is shown. Bicycle parking should be provided at surface level, either in adjacent public right-of-ways, in the proposed plaza, or adjacent to the entry inside the transit station.
- No street trees or furnishings are located along the Washington or Stockton Street frontages, with the exception of a single tree at the Washington St bulb-out. Street trees should be installed every 20 feet on center, and street furnishings such as seating included where appropriate.
- Existing sidewalk paving should be continued along Washington and Stockton Street frontages. Special station paving should be limited to the segment of sidewalk directly in front of the station entrance (enfronting the plaza along Washington Street and the bulb-out), and should not extend out onto the sidewalk, except directly in front of the station entrance.
- The sidewalk bulb-out at Stockton Street should extend the width of the station head to help create a "station plaza", which should include bike racks, benches, trees, and other landscape features.

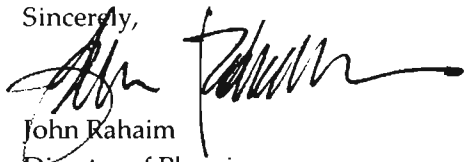
However, Planning retains significant concerns about future development of the station site. The Chinatown Station design progressed on the assumption that transit oriented development and/or a public plaza/open space would be implemented at the site in conjunction with the station construction, yet development plans for the full site are not complete at this time. The current site plan leaves a significant portion of the site unconsidered and unprogrammed. It is critical that design plans for the unprogrammed area move forward quickly in cooperation with Planning staff; including Preservation and Environmental Review; and that a well-designed open space or additional development that is compatible with the historic district be fully constructed and in place by the station's opening.

The Department urges SFMTA to ensure that current designs for the station will not preclude any options for future development, and to move forward with feasibility studies and designs for full site development as soon as possible.

FURTHER PLANNING DEPARTMENT REVIEW

The project will require additional entitlements, including a Conditional Use authorization to permit public use at the ground floor in a NCD district. In working towards that authorization, the SFMTA should continue to collaborate with the Planning Department on the long-term design and development of the site as noted above.

Sincerely,



John Rahaim
Director of Planning

Attachments:

1. Central Subway Chinatown Station Location: Site Map
2. General Plan Findings
3. Planning Code Sec. 101.1(b) Priority Policy Findings

cc K. Rich, OEWD
L. Gibson, Planning Department
M. Jacinto, Planning Department
T. Tam, Planning Department
E. Watty, Planning Department

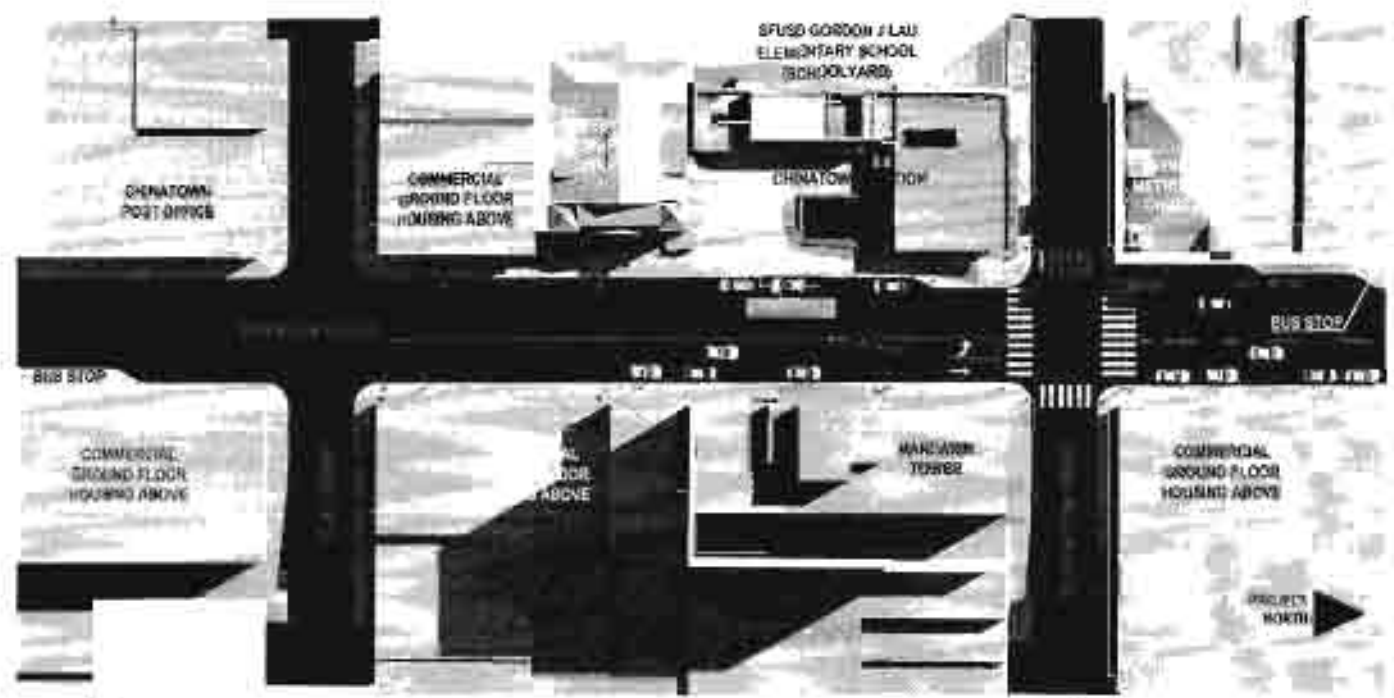
SITE MAP: ATTACHMENT 1


SITE MAP ATTACHMENT 1

central  subway

Connecting people. Connecting communities.

Chinatown Station
context plan



 Central Subway Station symbol

SFMTA | Municipal Transportation Agency

GENERAL PLAN FINDINGS: ATTACHMENT 2

RE: **CASE NO. 2011.1202R**
CENTRAL SUBWAY PROJECT
Chinatown Station (future terminus of Third Street Light Rail Phase 2)

STAFF REVIEWER: SARAH DENNIS PHILLIPS

GENERAL PLAN CONSIDERATIONS

General Plan Objectives, Policies, and Principles are in **bold font**, and staff comments are in *italic font*.

TRANSPORTATION ELEMENT

OBJECTIVE 1

MEET THE NEEDS OF ALL RESIDENTS AND VISITORS FOR SAFE, CONVENIENT AND INEXPENSIVE TRAVEL WITHIN SAN FRANCISCO AND BETWEEN THE CITY AND OTHER PARTS OF THE REGION WHILE MAINTAINING THE HIGH QUALITY LIVING ENVIRONMENT OF THE BAY AREA.

POLICY 1.3

Give priority to public transit and other alternatives to the private automobile as the means of meeting San Francisco's transportation needs, particularly those of commuters.

By creating a visible station at the centre of Chinatown's neighborhood commercial district, the station gives priority to public transit and provides a high traffic location for residents and visitors to access transit.

POLICY 2.4

Organize the transportation system to reinforce community identity, improve linkages among interrelated activities and provide focus for community activities.

The Chinatown Station is located in the heart of the Chinatown community, and will link the area's residents to neighborhoods in the southeastern section of the city.

The Central Subway Public Arts Program will work with communities along the project corridor to develop a comprehensive arts program to reflect the rich culture and history of the neighborhoods in which this new transit system will be located.

POLICY 11.2

Continue to favor investment in transit infrastructure and services over investment in highway development and other facilities that accommodate the automobile.

As the one of the only underground subway stations built in San Francisco in over 25 years, the station, and the overall project, represents a significant investment in the City's public transit infrastructure.

POLICY 14.7

Encourage the use of transit and other alternative modes of travel to the private automobile through the positioning of building entrances and the convenient location of support facilities that prioritizes access from these modes.

The location of Central Subway transit stations at key locations, including the Chinatown Station at Stockton and Washington Streets, will make access to the Central Subway easily available.

OBJECTIVE 21

DEVELOP TRANSIT AS THE PRIMARY MODE OF TRAVEL TO AND FROM DOWNTOWN AND ALL MAJOR ACTIVITY CENTERS WITHIN THE REGION.

POLICY 21.2

Where a high level of transit ridership or potential ridership exists along a corridor, existing transit service or technology should be upgraded to attract and accommodate riders.

URBAN DESIGN ELEMENT

POLICY 4.13

Improve pedestrian areas by providing human scale and interest.

Surface entrance areas to underground stations provide an opportunity to improve the pedestrian environment and wayfinding along Stockton Street. Station areas should be designed with careful attention to urban design and street and sidewalk design recommendations contained in the City's Better Streets Plan.

CHINATOWN AREA PLAN

POLICY 1.4

Protect the historic and aesthetic resources of Chinatown.

The implementation of the Central Subway project would result in the loss of an historic building in the Chinatown Historic District at 933-949 Stockton Street. Mitigation measures to reduce the impact of the demolition of the 933-949 Stockton Street building include: documentation of the existing historic building; salvage of architecturally significant building features for incorporation into an interpretative display in the new subway station; and employing an architectural historian in the design development of the new station and adjoining building to ensure that the design is culturally appropriate to the Chinatown District. As designs for the full station site are not yet complete, future review by the Planning Department, including the Preservation Coordinator, Environmental Review Officer, and a historic architect hired by SFMTA will be required of any development or landscape design to ensure that development of the site meets the Secretary of the Interior's Standards for the Treatment of Historic Properties based on compatibility with the character-defining features of the historic district.

OBJECTIVE 7

MANAGE TRANSPORTATION IMPACTS TO STABILIZE OR REDUCE THE DIFFICULTIES OF WALKING, DRIVING, DELIVERING GOODS, PARKING OR USING TRANSIT IN CHINATOWN.

POLICY 7.2

Make MUNI routes more reflective of and responsive to Chinatown ridership, including bilingual signage, schedules, maps.

The station will include bilingual signage and information on Muni routes.

DOWNTOWN PLAN

OBJECTIVE 17
DEVELOP TRANSIT AS THE PRIMARY MODE OF TRAVEL TO AND FROM
DOWNTOWN.

POLICY 17.1

Build and maintain rapid transit lines from downtown to all suburban corridors and major centers of activity in San Francisco.

The rapid connection from Chinatown to Downtown created by the Central Subway connects Chinatown's residents to the City's major center of activity. .

The proposal is X in conformity not in conformity with the General Plan.

EIGHT PRIORITY POLICIES FINDINGS: ATTACHMENT 3

**RE: CASE NO. 2011.1202R
CENTRAL SUBWAY PROJECT
Chinatown Station (future terminus of Third Street Light Rail Phase 2)**

STAFF REVIEWER: SARAH DENNIS PHILLIPS

The subject project is found to be consistent with the Eight Priority Policies of Planning Code Section 101.1 in that:

- 1. The project would have no adverse effect on neighborhood serving retail uses or opportunities for employment in or ownership of such businesses.**
Central Subway construction activities will have impacts to neighborhood retailers adjacent to and in the vicinity of the station; however, these disturbances will cease once construction is completed. Construction of the subway and the opening of the Chinatown station will bring additional visitors and consumers to neighborhood serving retail, with a spin-off effect increasing employment in the area.
- 2. The project would have no adverse effect on the City's housing stock or on neighborhood character.**
While construction of the station would demolish 18 rent-controlled units at 933-949 Stockton Street, SFMTA has committed to funding an off-site housing development of up to 75 new affordable units in an alternative location in Chinatown. 19 of these units would be reserved for former residents of 933-949 Stockton Street.
- 3. The project would have no adverse effect on the City's supply of affordable housing.**
As noted above, the replacement units would be developed as permanently affordable, and bring an additional 56 units of affordable housing with their construction.
- 4. The project would not result in commuter traffic impeding Muni transit service or overburdening the streets or neighborhood parking.**
By providing an exclusive right-of-way on the surface or in a subway that does not have to compete with traffic on congested surface streets, the reliability of transit service would be improved and travel times would be reduced for transit riders. Temporary disruption to traffic and Muni service is likely to occur during construction activities but will cease once completed.
- 5. The project would not adversely effect the industrial or service sectors or future opportunities for resident employment or ownership in these sectors.**
As an improvement in the public right-of-way, the Central Subway would not have a direct impact on the displacement of industrial and service sectors.
- 6. The project would have no adverse effect the City's preparedness to protect against injury and loss of life in an earthquake.**
The Chinatown station is not located on any active faults and therefore rupture resulting from displacement along a fault is not likely to occur. The subway station would be designed to current seismic standards to withstand a major earthquake (magnitude~7) on the San Andreas Fault.

- 7. The project would have no adverse effect on landmarks or historic buildings.**
While the implementation of the Central Subway project would result in the loss of an historic building in the Chinatown Historic District at 933-949 Stockton Street to accommodate the construction of the Chinatown Station, demolition of this building was identified in the FSEIS/FSEIR as an unavoidable significant impact. Mitigation measures to reduce the impact of the demolition of the 933-949 Stockton Street building have been taken, including are outlined in the Central Subway FSEIS/FSEIR and include: documentation of the existing historic building; salvage of architecturally significant building features for incorporation into an interpretative display in the new subway station. Additionally, Preservation staff has determined that the proposed project is compatible with the surrounding potential historic district and provides reference to several of the district's character-defining features, and therefore meets the requirements of the Central Subway Final SEIS/SEIR, Mitigation Measure HARC-1b.

- 8. The project would have no adverse effect on parks and open space or their access to sunlight and vistas.**
The new station is designed to meet up with the ground level of the Gordon Lau Elementary School playground, and would not create any shadow impacts. Should the podium above the station head house be developed to include additional structures, it would need to be developed in consultation with the Planning Department and the Chinatown community to ensure that the exterior building articulation is done in such a way as to minimize the shadow impacts on the Gordon Lau Elementary School playground.

ECP-FD038

Attachment 2

SF Planning Commission

Motion No. 18699

Case No. 2012.0641C

Chinatown Station



SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- Affordable Housing (Sec. 415)
- Jobs Housing Linkage Program (Sec. 413)
- Downtown Park Fee (Sec. 412)
- First Source Hiring (Admin. Code)
- Child Care Requirement (Sec. 414)
- Other

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

Planning Commission Motion No. **18699**

HEARING DATE: AUGUST 9, 2012
CONTINUED TO: SEPTEMBER 06, 2012

Date: August 30, 2012
Case No.: **2012.0641C**
Project Address: **933-949 STOCKTON STREET**
Zoning: Chinatown Residential Neighborhood Commercial (CRNC) Zoning District
 Chinatown Transit Station Special Use District
 65-85-N Height and Bulk District
Block/Lot: 0211/001
Project Sponsor: John Funghi
 San Francisco Municipal Transportation Agency
 821 Howard Street, 2nd Floor
 San Francisco, CA 94103
Staff Contact: Elizabeth Watty – (415) 558-6620
Elizabeth.Watty@sfgov.org

ADOPTING FINDINGS RELATING TO THE APPROVAL OF A CONDITIONAL USE AUTHORIZATION PURSUANT TO PLANNING CODE SECTIONS 145.3, 303, 812.82, 890.80, AND 812.14, TO ALLOW CONSTRUCTION OF THE CHINATOWN TRANSIT STATION ENTRANCE STRUCTURE (A “PUBLIC USE”) WITH STREET FRONTAGES GREATER THAN 50 FEET, WITHIN THE CHINATOWN RESIDENTIAL NEIGHBORHOOD COMMERCIAL ZONING DISTRICT, AND 65-85-N HEIGHT AND BULK DISTRICT.

PREAMBLE

On May 16, 2012, John Funghi of the San Francisco Municipal Transportation Agency (hereinafter “SFMTA”) (hereinafter “Project Sponsor”) filed an application with the Planning Department (hereinafter “Department”) for Conditional Use authorization under Planning Code Sections 145.3, 303, 812.82, 890.80, and 812.14, to allow construction of the Chinatown Transit Station entrance structure with street frontages greater than 50 feet, within the Chinatown Residential Neighborhood Commercial (hereinafter “CRNC”) Zoning District, and 65-85-N Height and Bulk District.

On August 9, 2012, the San Francisco Planning Commission (hereinafter "Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Application No. 2012.0641C.

The physical effects of the demolition of 939-949 Stockton Street and the construction of a station building were analyzed in the Central Subway/Third Street Light Rail Phase 2 Final Supplemental Environmental Impact Statement/Final Supplemental Environmental Impact Report ("Final SEIS/SEIR") certified by the Planning Commission in 2008, and no subsequent or supplemental environmental review is required under the California Environmental Quality Act Guidelines Section 15162. Planning Department staff has reviewed the design of the station and determined that the station as designed would not result in new significant impacts, require new or modified mitigation measures, or cause impacts of greater severity than previously reported in the Final SEIR/SEIS. On August 09, 2008, the SFMTA adopted the project, including findings under CEQA, in SFMTA Resolution No. 08-150, which CEQA findings are incorporated by reference in this Motion.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Conditional Use requested in Application No. 2012.0641C, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The project is located at the southwest corner of Stockton and Washington Streets, Block 0211, Lot 001. The subject property is located within the CRNC Zoning District, the 40-X Height and Bulk District. The property was developed with a two-story mixed-use building with commercial occupancy at the street level and single room occupancy lodging above. All residential and commercial tenants have been relocated and the building has been secured in preparation for demolition.

On April 4, 2012, Ordinance No.'s 0040-12 and 0041-12 became law, which together amended the Planning Code by adding Section 249.66, creating the Chinatown Transit Station Special Use District (SUD). The Chinatown Transit Station SUD authorized the demolition of the two-story, mixed-use building located on the subject lot without a concurrent approval of a replacement building. The Planning Commission and Board supported the amendment to allow demolition without concurrent new construction approval because the MTA had time constraints surrounding the need for demolition approval in order to secure federal funding, before the design of the proposed "head house" was ready to bring to the Commission for approval.

- 3. Surrounding Properties and Neighborhood.** The Chinatown Residential Neighborhood Commercial District extends along Stockton Street between Sacramento and Broadway and along Powell Street between Washington Street and Broadway. It is generally west and uphill from Grant Avenue and is close to the relatively intensely developed residential areas of lower Nob and Russian Hills. Stockton Street is a major transit corridor which serves as "Main Street" for the Chinatown neighborhood. Both Stockton and Powell Streets contain a significant amount of housing as well as major community institutions supportive to Chinatown and the larger Chinese community. This daytime-oriented district provides local and regional specialty food shopping for fresh vegetables, poultry, fish and meat. Weekends are this area's busiest shopping days.

Because Stockton Street is intended to remain principally in its present character, the Stockton Street controls are designed to preserve neighborhood-serving uses and protect the residential livability of the area. The controls promote new residential development compatible with existing small-scale mixed-use character of the area. Consistent with the residential character of the area, commercial development is directed to the ground story. Daytime-oriented use is protected and tourist-related uses, fast-food restaurants and financial services are limited.

Housing development in new and existing buildings is encouraged above the ground floor. Institutional uses are also encouraged. Existing residential units are protected by limits on demolition and conversion.

- 4. Project Description.** The proposed Central Subway Project is the second phase of the SFMTA's Third Street Light Rail Project. The Central Subway Project will extend Muni transit service improvements from the present terminus of the Third Street Light Rail Line at Fourth and King Streets through South of Market, Downtown, and terminating in Chinatown. The proposed project under review in this Conditional Use is the Chinatown Station entrance building, located at 935 Stockton Street, at Washington.

The proposed project involves construction of a new one-story transit station. The majority of the proposed station, including the subway tube and platforms, will be located underground and is not subject to Commission review. At the street level, however, the project includes construction of a one-story station entrance building, known as the "head house", containing the station entrance and exit, the enclosed glass skylight that illuminates the escalators, back-of-house circulation and mechanical features, and an open plaza at the south end of the site. These features are subject to the Commission's review and approval as part of this Conditional Use authorization for a Public Use and Street Frontages Greater than 50'-0" in the CRNC District.

The proposed building is composed as a three part scheme including a base, glazed body, and a top/fascia element. A public art installation, which has been approved by the Arts Commission, will wrap around the building at the cornice level. The top of the head house (approximately 24 feet above the sidewalk at the corner) is a flat slab capable of supporting future Transit Oriented Development (TOD) or a public park/open space.

However, at this time, the feasibility of TOD and/or public plaza/open space has not been determined. Any Request for Proposals for the TOD will include review requirements by the Planning Department's Preservation staff, and a qualified historic resources consultant hired by SFMTA to ensure that any future proposal meets the Secretary of the Interior's Standards for the Treatment of Historic Properties (SOIS's) based on compatibility with the character-defining features of the Chinatown Historic District (a district listed on the California Register of Historic Resources, but not a locally recognized district).

Attached as Exhibit C, is a memo from the Office of Economic and Workforce Development outlining the proposed schedule for public outreach to conceptually define the site's future programming, with the goal of having a TOD and open space proposal finalized in time for construction of the head house. Within one year of this Conditional Use Authorization, Planning staff will return to the Commission with a full report on the community process's outcomes and resulting site design and program, as conditioned in Exhibit A of this motion.

5. **Public Comment.** To date, the Department has not received any correspondence regarding this Conditional Use authorization.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:

- A. **Public Use.** Planning Code Section 812.82 states that a public use, as defined by Planning Code Section 890.80, is permitted as a Conditional Use at the first floor and above.

The Project includes a transit station, which is a public use, at the ground floor of the new building. A Conditional Use authorization is being sought under this application, the findings for which are outlined in Section 8 below.

- B. **Use Size.** Planning Code Section 812.20 allows a use size up to 2,500 gsf as-of-right, and any use size between 2,501 to 4,000 gsf requires a Conditional Use authorization.

The Project will include approximately 260 gsf of floor area that is not dedicated to public transit circulation, and thus meets the principally permitted use size controls for this District. Access ways to public transit are excluded from gross floor area, pursuant to Planning Code Section 102.9.

- C. **Floor Area Ratio.** Planning Code Section 812.19 allows a floor area ratio (FAR) up to 1.0 to 1.

The project will result in an FAR of .025 to 1, and thus complies with the FAR controls for this District.

- D. **Street Trees.** Planning Code Section 138.1 requires one street tree for every 20 feet of frontage, with an additional tree required for every remaining 10 feet of frontage.

The project includes a combined frontage of 210.9 feet along Washington and Stockton Streets, resulting in a total of 11 required street trees. The project proposes the installation of eight street trees: five along Stockton Street and three along Washington Street. The remaining three required street trees will be paid via an in-lieu fee, as conditioned in Exhibit A. The Zoning Administrator has reviewed and approved the request to waive the requirement to plant the three remaining street trees and instead pay the in-lieu fee, due to several unique features of the site that are associated with its use as a public transit facility. These include an emergency egress hatch located in under the Washington Street Sidewalk, as well as a desire for there to be visibility into the station entrance.

- E. **Street Frontage in Neighborhood Commercial Districts.** Section 145.1 of the Planning Code requires that Mixed Use Districts provide the following: 1) “active uses” within the first 25 feet of building depth on the ground floor from any facade facing a street at least 30 feet in width; 2) street-facing ground-level spaces that open directly onto the street; and 3) frontages that are fenestrated with transparent windows and doorways for no less than 60 percent of the street frontage at the ground level and that allow visibility to the inside of the building.

The project will not include active uses along the western 19'-6" portion of the Washington Street frontage, where the property abuts Gordon Lau Elementary School. This space is dedicated to mechanical functions of the building along with egress stairs, and is enclosed by glass fiber reinforced cementitious panels.

Planning Code Section 145.1(c)(3) allows the Zoning Administrator to exempt projects from the active use requirement of Section 145.1 in instances where features such as mechanical electrical, and plumbing functions are provided within the front 25 feet of building depth in such a fashion as to not negatively affect the quality of the ground floor space. The Zoning Administrator reviewed the project's street frontage design and has authorized administrative approval to allow certain non-active uses within the front 25 feet of the building since those features do not negatively impact the quality of the ground floor space. The proposal provides active uses for the remainder of the building's two frontages – at depths of more than 25 feet deep – and includes more than the required amount of façade transparency. Furthermore, due to the unique use associated with the building, the topography of the property along Washington Street, and the architectural treatment of this non-active use, the non-active use portion of the Washington Street façade does not negatively impact the quality of the ground floor space.

- F. **Maximum Street Frontages.** Section 145.3 of the Planning Code limits the street frontage of buildings in Chinatown to a maximum of 50 feet in width.

The project includes building street frontages greater than 50 feet in width. An exception to this requirement is being sought under this Conditional Use authorization, the findings for which are outlined below in Section 7.

- G. **Hours of Operation.** Planning Code Section 890.48 allows hours of operation from 6:00 a.m. until 11:00 p.m. as-of-right in the CRNC District, and requires Conditional Use authorization to operate between the hours of 11:00 p.m. and 2:00 a.m.

A transit station is not considered a "commercial establishment", and is thus not subject to these hours of operation controls.

H. **Signage.** Currently, there is not a proposed sign program on file with the Planning Department. Any proposed signage will be subject to the review and approval of the Planning Department.

7. **Planning Code Section 145.3** establishes street frontage maximums of 50 feet wide in Chinatown. Street frontage exceptions may be approved as a Conditional Use in accordance with the criteria outlined below:

A. Projects having more than 50 feet of street frontage shall be divided in architectural treatment to appear as two or more independent buildings reflecting the typical scale of older buildings in the Chinatown area. Architectural treatments may include varied types of windows and entries, individual storefronts and the use of differing colors and textures.

The site design and building massing of the Chinatown Station is designed such that each major component of the building along Stockton and Washington Streets does not exceed 50 feet in width. The proposed project is compatible with the surrounding character and scale of older buildings in the Chinatown area and provides reference to several of the district's character-defining features. The project includes a continuous yet articulated façade, simple rectangular massing, a flat roof, and a regular rhythm of bays and fenestration pattern. The overall low-scale massing, simple horizontal lines, and materials palette of the new construction allows the building to blend in with the surrounding neighborhood.

B. Flat facade surfaces shall be broken up at least every 30 feet by the projection of bay windows or by vertical recesses.

The façade is broken up at least every 30 feet by projecting fins, punched vertical openings, exterior seating elements, or large vertical recesses at the entrance.

C. Facade divisions shall be reinforced by matching changes in height for portions of the building.

The project includes three major changes in height, differentiating the angled escalator feature, the main head house, and the back-of-house features on Washington Street. These three height changes correlate to several of the façade divisions that break up the massing of the building.

8. **Planning Code Section 303** establishes criteria for the Planning Commission to consider when reviewing applications for Conditional Use approval. On balance, the project does comply with said criteria in that:

- A. The proposed new uses and building, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable, and compatible with, the neighborhood or the community.

The Chinatown transit station head house building at Stockton and Washington Streets will anchor the historical restoration of the original north-south rail transit axis connecting Chinatown with the Union Square, Market Street, and South of Market Districts. Here, the route under Stockton and Fourth Streets will be underground to increase speed and reliability over the original surface rail line that until the 1940's used the underground Stockton tunnel constructed to speed street cars between Chinatown and the City center.

The single story head house will be the street edge, public portion of the station situated at the northern end corner of the site at Washington and Stockton Streets with the entrance on Stockton Street. The top of the head house will be lower than the mixed-use structure that previously occupied the site. The walls will be largely glass, giving an overall impression of transparency.

The station entrance presents an inviting landmark, integrating the community needs to access mobility, display public art, and provide open space. The result of this public use will be less crowded bus stops, less crowded buses, and faster bus service throughout the Stockton Street commercial corridor, serving a population that is heavily dependent on public transportation. It will also improve congestion on the streets through Chinatown.

- B. The proposed project will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity. There are no features of the project that could be detrimental to the health, safety or convenience of those residing or working the area, in that:

- i. Nature of proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

The above ground portion of the station will be a net reduction of gross floor area of non-residential space in comparison to the gross floor area of the previous structure on the lot. The size and scale of the station are appropriate to its setting. The intersection of Washington and Stockton Streets is a natural discontinuity in the pervasive character defining mixed-use building typology. The intersection is inhabited by buildings that are either large in size and/or scale, have anomalous proportions, or are institutional in massing. As a tall one-story building with features common to the adjacent street front facades, the design's materiality, features, size, scale, proportion and massing are respectful of, and compatible with, the institutional character of the Stockton and Washington Street corner.

- ii. The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

The proposed transit station will improve accessibility for pedestrians and relieve congestion from buses that currently require extensive passenger loading times due to overcrowding. In keeping with the City's Transit First policy, the transit station's design does not include off-street parking or loading facilities.

- iii. The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;

The proposed transit station will not produce noxious or offensive emissions such as noise, glare, dust or odor. The emergency ventilation shafts are situated to the east of the site along the retaining wall, to minimize its proximity to the public realm in case of emergency. The vents extend from the southern side of the head house to the southern property line, and are approximately 25 feet wide in the east west direction. This element is an opaque wall backdrop to the open space south of the station entrance building.

- iv. Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

The project site will include a landscaped area at grade. Conditions of approval outlined in Exhibit A ensure that a community process is in place to plan for a possible future TOD and open space development at this property. The project includes eight new street trees along the perimeter of the site, and does not include any off-street parking or loading.

- C. That the use as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

The project complies with all relevant requirements and standards of the Planning Code, and is consistent with Objectives and Policies of the General Plan as detailed below.

- 9. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

COMMERCE AND INDUSTRY ELEMENT

OBJECTIVE 1

MANAGE ECONOMIC GROWTH AND CHANGE TO ENSURE ENHANCEMENT OF THE TOTAL CITY LIVING AND WORKING ENVIRONMENT.

Policy 1.1

Encourage development which provides substantial net benefits and minimizes undesirable consequences. Discourage development which has substantial undesirable consequences that cannot be mitigated.

The proposed transit station will provide substantial net benefits to the Chinatown community and the City as a whole by linking the neighborhoods in the southeastern portion of the City with the retail and employment centers in the City's downtown and Chinatown Neighborhoods. The underground transit station and rail lines will help to minimize above ground congestion and facilitate improved access to jobs, shopping, and regional destinations such as Chinatown.

This project includes development that has minimized undesirable consequences. All existing residential and commercial tenants have been provided relocation packages that conform to the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970. The SFMTA has also minimized the undesirable consequences of losing 18 rent-controlled dwelling units (19 households) from the City's housing stock by committing to fund an off-site affordable housing development near Chinatown.

OBJECTIVE 2

MAINTAIN AND ENHANCE A SOUND AND DIVERSE ECONOMIC BASE AND FISCAL STRUCTURE FOR THE CITY.

Policy 2.1

Seek to retain existing commercial and industrial activity and to attract new such activity to the city.

Policy 2.3

Maintain a favorable social and cultural climate in the city in order to enhance its attractiveness as a firm location.

The proposed transit station will provide substantial benefits to the City as a whole by linking the neighborhoods in the southeastern portion of the City with the retail and employment centers in the City's downtown and Chinatown Neighborhoods. The underground transit station and rail lines will help to minimize above ground congestion and facilitate improved access to jobs, shopping, and regional destinations such as Chinatown. Improved transit access to social and cultural destinations, as well as the new linkage with CalTrain Station at 4th and King Street, will help enhance the City's attractiveness as a place where companies want to locate their businesses.

OBJECTIVE 4

IMPROVE THE VIABILITY OF EXISTING INDUSTRY IN THE CITY AND THE ATTRACTIVENESS OF THE CITY AS A LOCATION FOR NEW INDUSTRY.

Policy 4.4

When displacement does occur, attempt to relocate desired firms within the city.

Policy 4.7

Improve public and private transportation to and from industrial areas.

The proposed transit station will provide substantial net benefits to Chinatown community and the City as a whole by linking the neighborhoods in the more industrial southeastern portion of the City with the retail and employment centers in the City's downtown and Chinatown Neighborhoods. The underground transit station and rail lines will help to minimize above ground congestion and facilitate improved access throughout these neighborhoods.

OBJECTIVE 8

ENHANCE SAN FRANCISCO'S POSITION AS A NATIONAL CENTER FOR CONVENTIONS AND VISITOR TRADE.

Policy 8.3

Assure that areas of particular visitor attraction are provided with adequate public services for both residents and visitors.

Chinatown is one of the City's prominent visitor destinations. Public services such as transit are especially important in areas of particular visitor attraction. Provision of high quality transit services is one direct method that the City can employ to promote visitor trade in San Francisco. Additional public transit facilities, such as the Central Subway extension through Chinatown, will serve expanding visitor trade and should be supported.

URBAN DESIGN ELEMENT

OBJECTIVE 4

IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY

Policy 4.13

Improve pedestrian areas by providing human scale and interest.

Surface entrance areas to underground stations provide an opportunity to improve the pedestrian environment and wayfinding along Stockton Street. Station areas should be designed with careful attention to urban design and street and sidewalk design recommendations contained in the City's Better Streets Plan.

CHINATOWN AREA PLAN

OBJECTIVE 1

PRESERVE THE DISTINCTIVE URBAN CHARACTER, PHYSICAL ENVIRONMENT AND CULTURAL HERITAGE OF CHINATOWN.

Policy 1.4

Protect the historic and aesthetic resources of Chinatown.

Although the implementation of the Central Subway project would result in the loss of an historic building in the Chinatown Historic District at 933-949 Stockton Street, SFMTA has complied with the mitigation measures outlined in the SEIS/SEIR help to reduce the impact of the demolition of the building. These measures include documentation of the existing historic building; salvage of architecturally significant building features for incorporation into an interpretative display in the new subway station; and employment of an architectural historian in the design development of the new station and adjoining building to ensure that the design is culturally appropriate to the Chinatown District. Preservation staff has determined that the proposed project is compatible with the surrounding potential historic district, and therefore meets the requirements of the Central Subway Final SEIS/SEIR, Mitigation Measure HARC-1b. SFMTA will continue to work with the Planning Department, including the Preservation staff, on final landscape design details. Any future proposals for TOD and/or open space will be reviewed by a qualified historic resources consultant hired by SFMTA to ensure that future development of the site meets the Secretary of the Interior's Standards for the Treatment of Historic Properties based on compatibility with the character-defining features of the historic district.

OBJECTIVE 7

MANAGE TRANSPORTATION IMPACTS TO STABILIZE OR REDUCE THE DIFFICULTIES OF WALKING, DRIVING, DELIVERING GOODS, PARKING OR USING TRANSIT IN CHINATOWN.

Policy 7.2

Make MUNI routes more reflective of and responsive to Chinatown ridership, including bilingual signage, schedules, and maps.

The station will include bilingual signage and information on Muni routes.

DOWNTOWN PLAN

OBJECTIVE 17

DEVELOP TRANSIT AS THE PRIMARY MODE OF TRAVEL TO AND FROM DOWNTOWN.

Policy 17.1

Build and maintain rapid transit lines from downtown to all suburban corridors and major centers of activity in San Francisco.

The rapid connection from Chinatown to Downtown created by the Central Subway connects Chinatown's residents to the City's major center of activity.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 1

MEET THE NEEDS OF ALL RESIDENTS AND VISITORS FOR SAFE, CONVENIENT AND INEXPENSIVE TRAVEL WITHIN SAN FRANCISCO AND BETWEEN THE CITY AND OTHER PARTS OF THE REGION WHILE MAINTAINING THE HIGH QUALITY LIVING ENVIRONMENT OF THE BAY AREA.

Policy 1.3

Give priority to public transit and other alternatives to the private automobile as the means of meeting San Francisco's transportation needs, particularly those of commuters.

By creating a visible station at the center of Chinatown's neighborhood commercial district, the station gives priority to public transit and provides a high traffic location for residents and visitors to access transit.

Policy 2.4

Organize the transportation system to reinforce community identity, improve linkages among interrelated activities and provide focus for community activities.

The Chinatown Station is located in the heart of the Chinatown community, and will link the area's residents to neighborhoods in the southeastern section of the city. The Central Subway Public Arts Program will work with communities along the project corridor to develop a comprehensive arts program to reflect the rich culture and history of the neighborhoods in which this new transit system will be located.

Policy 11.2

Continue to favor investment in transit infrastructure and services over investment in highway development and other facilities that accommodate the automobile.

As the one of the only underground subway stations built in San Francisco in over 25 years, the station, and the overall project, represents a significant investment in the City's public transit infrastructure.

Policy 14.7

Encourage the use of transit and other alternative modes of travel to the private automobile through the positioning of building entrances and the convenient location of support facilities that prioritizes access from these modes.

The location of Central Subway transit stations at key locations, including the Chinatown Station at Stockton and Washington Streets, will make access to the Central Subway easily available.

OBJECTIVE 21

DEVELOP TRANSIT AS THE PRIMARY MODE OF TRAVEL TO AND FROM DOWNTOWN AND ALL MAJOR ACTIVITY CENTERS WITHIN THE REGION.

Policy 21.2

Where a high level of transit ridership or potential ridership exists along a corridor, existing transit service or technology should be upgraded to attract and accommodate riders.

Chinatown has one of the City's highest transit ridership rates in the City, and the Central Subway Project, particularly the station in Chinatown will enhance and upgrade transit service and technology to accommodate Chinatown riders.

10. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the Project complies with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

Central Subway construction activities will have impacts to neighborhood retailers adjacent to and in the vicinity of the station; however, these disturbances will cease once construction is completed. Construction of the subway and the opening of the Chinatown station will bring additional visitors and consumers to neighborhood serving retail, with a spin-off effect increasing employment in the area.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

While the previously approved demolition of the building located on the subject property would result in the loss of 18 rent-controlled units at 933-949 Stockton Street, all commercial and residential tenants were successfully relocated pursuant to federal and state guidelines, and SFMTA has committed to funding an off-site housing development of up to 75 new affordable units in an alternative location in Chinatown. 19 of these units would be reserved for former residents of 933-949 Stockton Street.

The new construction of the Chinatown Transit Station head house, however, will have no adverse effect on housing or neighborhood character. Rather, the addition of a transit facility within the neighborhood will help to conserve and protect the cultural and economic diversity of the neighborhood by bringing improved access to this neighborhood and tourist destination. The architectural treatment of the new station was designed in consultation with the Chinatown community, and the Arts Commission.

- C. That the City's supply of affordable housing be preserved and enhanced.

As noted above, the SFMTA will provide funding for replacement units which would be developed as permanently affordable units, and bring an additional 56 units of affordable housing with their

construction. The relocation of the 19 households was successfully completed in December of 2011 in full compliance with the federal Uniform Relocation Act and the State of California Relocation Act.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The implementation of the Central Subway project, the second and final phase of the multi-year Third Street Light Rail Program, is specifically designed to enhance transit service between the southeast and northeast districts of San Francisco in keeping with the City's Transit First policy.

By providing an exclusive right-of-way on the surface or in a subway that does not have to compete with traffic on congested surface streets, the reliability of transit service would be improved and travel times would be reduced for transit riders. This project would serve employment and population growth in this corridor and while reducing neighborhood street and sidewalk congestion. Temporary disruption to traffic and Muni service is likely to occur during construction activities but will cease once completed.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

As an improvement in the public right-of-way, the Central Subway would not have a direct effect on the displacement of industrial and service sectors. The Central Subway does, however, improve the link of the Stockton/4th Street corridor directly to the South of Market and Eastern Neighborhoods where industrial and service sectors are prevalent.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Chinatown Transit Station is not located on any active faults and therefore rupture resulting from displacement along a fault is not likely to occur. The subway station would be designed to current seismic standards to withstand a major earthquake (magnitude-7) on the San Andreas Fault.

- G. That landmarks and historic buildings be preserved.

While the implementation of the Central Subway project would result in the loss of an historic building in the Chinatown Historic District at 933-949 Stockton Street to accommodate the construction of the Chinatown Station, demolition of this building was identified in the FSEIS/FSEIR as an unavoidable significant impact. Mitigation measures consistent with the Central Subway FSEIS/FSEIR have been taken to reduce the impact of the demolition of the 933-949 Stockton Street building, including: documentation of the existing historic building and salvage of architecturally significant building features for incorporation into an interpretative display in the new subway station. Additionally, Preservation staff has determined that the proposed project is compatible with the surrounding

potential historic district, and therefore meets the requirements of the Central Subway Final SEIS/SEIR, Mitigation Measure HARC-lb.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The new station is designed to meet up with the ground level of the Gordon Lau Elementary School playground, and would not create any shadow impacts. The building is less than 40'-0" tall, and is thus exempt from the Section 295 Shadow controls. Should the podium above the station head house be developed to include additional structures as part of a future TOD, it would need to be developed in consultation with the Planning Department and the Chinatown community to ensure that the exterior building articulation is done in such a way as to minimize the shadow impacts on the Gordon Lau Elementary School playground, as well as any other public open spaces.

11. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
12. The Commission hereby finds that approval of the Conditional Use authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Conditional Use Application No. 2012.0641C** subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated July 07, 2012, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Conditional Use Authorization to the Board of Supervisors within thirty (30) days after the date of this Motion No. 18699. The effective date of this Motion shall be the date of this Motion if not appealed (After the 30-day period has expired) OR the date of the decision of the Board of Supervisors if appealed to the Board of Supervisors. For further information, please contact the Board of Supervisors at (415) 554-5184, City Hall, Room 244, 1 Dr. Carlton B. Goodlett Place, San Francisco, CA 94102.

I hereby certify that the Planning Commission ADOPTED the foregoing Motion on September 06, 2012.

Linda D. Avery
Commission Secretary

AYES: Commissioners Antonini, Borden, Fong, Hillis, Moore

NAYS:

ABSENT:

RECUSED: Commissioners Sugaya, Wu

ADOPTED: September 06, 2012

EXHIBIT A

AUTHORIZATION

This authorization is for a Conditional Use to allow a Public Use, specifically the Chinatown Transit Station entrance building, with street frontages greater than 50 feet at 935 Stockton Street, Block 0211, Lot 001 pursuant to Planning Code Sections 145.3, 303 812.82, and 890.80, within the **Chinatown Residential Neighborhood Commercial District** and a **65-85-N Height and Bulk District**; in general conformance with plans, dated **July 7, 2012**, and stamped "EXHIBIT B" included in the docket for Case No. **2012.0641C** and subject to conditions of approval reviewed and approved by the Commission on **September 06, 2012**, under Motion No. 18699. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on **September 06, 2012, 2012**, under Motion No. 18699.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. 18699 shall be reproduced on the Index Sheet of construction plans submitted with the Site or Building permit application for the Project. The Index Sheet of the construction plans shall reference to the Conditional Use Authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new Conditional Use authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

Validity and Expiration. The authorization and right vested by virtue of this action is valid for five years from the effective date of the Motion. A building permit from the Department of Building Inspection to construct the Project and/or commence the approved use must be issued as this Conditional Use authorization is only an approval of the proposed Project and conveys no independent right to construct the Project or to commence the approved use. The Planning Commission may, in a public hearing, consider the revocation of the approvals granted if a site or building permit has not been obtained within three five (5) years of the date of the Motion approving the Project. Once a site or building permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. The Commission may also consider revoking the approvals if a permit for the Project has been issued but is allowed to expire and more than five (5) years have passed since the Motion was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Extension. This authorization may be extended at the discretion of the Zoning Administrator only where failure to issue a permit by the Department of Building Inspection to perform said tenant improvements is caused by a delay by a local, State or Federal agency or by any appeal of the issuance of such permit(s).

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

PROVISIONS

First Source Hiring. The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org

Street Trees. Pursuant to Planning Code Section 138.1, the Project Sponsor shall submit a site plan to the Planning Department prior to Planning approval of the building permit application indicating the installation of eight (8) street trees: five (5) on Stockton Street and three (3) on Washington Street. The installation of the remaining three (3) required street trees has been waived by the Zoning Administrator and shall be accommodated through the payment of an in-lieu fee.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

DESIGN – COMPLIANCE AT PLAN STAGE

Final Materials. The Project Sponsor shall continue to work with Planning Department staff on the final building and landscape design. Final materials, glazing, color, texture, landscaping, and detailing shall

be subject to Department staff – including Preservation staff – review and approval as part of the Architectural Addenda and prior to issuance of the Certificate of Final Occupancy.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

Future TOD/Open Space Design. The Project Sponsor shall adhere to the public engagement schedule outlined in Exhibit C, in order to conceptually define the site’s possible future programming and design, with the goal of having a TOD and open space proposal finalized in time for concurrent construction with the transit station head house.

Within approximately one year from the date of this approval, Planning Staff shall return to the Commission with a full report on the outcome of that community process, along with the resulting site proposal.

Should planning for the TOD be deemed feasible and move forward, the Request for Proposal for any TOD shall require that that TOD design be developed with and reviewed by the Planning Department’s Preservation staff, and a qualified historic resources consultant hired by SFMTA to ensure that the TOD development meets the Secretary of the Interior’s Standards for the Treatment of Historic Properties based on compatibility with the character-defining features of the historic district.

Furthermore, should the TOD be deemed feasible by the SFMTA, the final TOD design – including any design changes that affect the exterior of the station head house – shall be reviewed and approved by the Planning Commission.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

MONITORING - AFTER ENTITLEMENT

Enforcement. Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

Revocation due to Violation of Conditions. Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

OPERATION

Community Liaison. Prior to issuance of a building permit to construct the Project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

ECP-FD038

Attachment 3

SF Planning Commission
Tree Planting and Protection Checklist
Case No. 2012 0403 7405

Chinatown Station

REQUIRED CHECKLIST FOR Tree Planting and Protection

1. Applicant Information

CONTACT FOR PROJECT INFORMATION: John Funghi	
ADDRESS: 821 Howard Street, 2nd Floor San Francisco, CA 94103	TELEPHONE: (415) 701-5262
	EMAIL: john.funghi@sfmta.com

2. Location and Classification of Property

STREET ADDRESS OF PROJECT: 933-949 Stockton Street		
CROSS STREETS: Washington		
ASSESSOR'S BLOCK/LOT: 211 / 001	LENGTH OF ALL LOT FRONTAGE(S): 210.9	ZONING DISTRICT: CRNC
RELATED BUILDING PERMIT APPLICATION AND/OR CASE NO. 2012.0403.7405		

3. Scope of Project

Requirements for new street trees and tree protection apply to the types of projects identified in the chart below. Please check all boxes which apply to your project. If no boxes are checked, you do not need to complete this form.

NEW CONSTRUCTION PROJECTS	
<input checked="" type="checkbox"/>	construction of a new building
<input type="checkbox"/>	relocation of a building
<input type="checkbox"/>	padding or repadding more than 200 square feet of the front setback
<input type="checkbox"/>	addition of gross floor area (GFA) equal to 20% or more of the GFA of the existing building
<input type="checkbox"/>	addition of a new dwelling unit
<input type="checkbox"/>	addition of one or more parking spaces
<input type="checkbox"/>	addition of a garage

5. Impact of Project on Existing Protected Trees

If your responses above indicate that any Protected Tree(s) exist on, over, or adjacent to the subject property, please check the applicable boxes below.

- BOX 1** **The project will not remove or have any other impact on Protected Trees, as follows:** No construction-related activity whatsoever will occur within the dripline of any Significant Tree or Street Tree. This includes, but is not limited to, the following: (1) No grading or excavation will take place within the dripline of any Significant Tree or Street Tree. (2) No construction staging and/or storage of materials and/or equipment will occur within the dripline of any Significant Tree or Street Tree. (3) Any pruning of Significant Trees or Street Trees will be limited and consistent with applicable regulations. (4) No dumping of trash and/or liquids (such as project waste-water) will take place within the basin or dripline of any Significant Tree or Street Tree.

If you have checked this box, a Tree Protection Plan is not required.

- BOX 2** **The project involves the removal of one or more Protected Trees.** A permit from DPW is required in order to remove any Protected Tree. The Planning Department will not approve a building permit for a project which involves the removal of a Protected Tree unless DPW has first reviewed the proposal and found it to be consistent with applicable rules and regulations.

If you have checked this box, a Tree Protection Plan is not required, however you must provide evidence to the Planning Department that DPW has reviewed the removal request and found it to be "approvable."

- BOX 3** **The project may have an impact on one or more Protected Trees which are not proposed for removal, as follows:** Either (1) any construction-related activity, no matter how minor, is planned or is reasonably foreseeable to occur within the dripline of a Significant Tree or a Street Tree or (2) regardless of the location of construction activity, the property contains a Landmark Tree.

If you have checked this box, a Tree Protection Plan must be submitted to the Department of Public Works Bureau of Urban Forestry prior to the commencement of any construction activity.

Such plan must meet the following minimum standards:

- ✓ The Tree Protection Plan must be developed by an International Society of Arboriculture (ISA) Certified Arborist.
- ✓ The project sponsor must submit a written declaration that the protections specified in the Tree Protection Plan will be completely in place prior to the start of any construction, demolition, or grading.
- ✓ Full-size site plans submitted along with the associated construction project must clearly indicate the street, curb, sidewalk, driveway, structure(s), and the locations of all Protected Trees and non-protected trees. Protected Trees must also be shown to include accurate tree height, accurate canopy dripline and trunk and canopy diameters. The plans must graphically depict implementation of all measures called for in the Tree Protection Plan. Additionally, the Tree Protection Plan itself along with the written declaration must be reproduced on full-size plans.

6. Calculation of Number of New Required Street Trees

One street tree is required for each 20 feet of street frontage of the subject property, with fractions of 0.5 rounded up, however credit is given for existing street trees. Please complete the table below to determine the number of street trees required for your project. If no street trees are required, please skip to the Applicant's Affidavit at the end of this form and once signed, return it to the Planning Department along with your Building Permit Application or other application.

COMBINED LENGTH OF ALL STREET FRONTAGES	DIVIDED BY TREE SPACING REQUIREMENT	GROSS NUMBER OF TREES REQUIRED	MINUS NUMBER OF EXISTING TREES	NET STREET TREE REQUIREMENT
210.9	20'	10.545 <small>(rounded)</small>	0	11

Unless site conditions physically prevent the planting of a street tree, a waiver or modification of street tree requirements is available only under extremely limited circumstances and **only outside of Residential Districts** (i.e. RH, RM, RTO, RED). Be aware that even when available, an in-kind improvement or in-lieu payment is required for every such waiver. Please contact the Planning Department for information regarding the waiver process.

7. Applicable Requirements for New Street Trees

The Planning Department has developed three distinct "Tree Schedules" to aid in the implementation of the Planning Code's street tree requirements. The particular Tree Schedule applicable to your project will depend on the zoning district in which your property is located, the scope of your project, and the type of authorization that your project requires. In general terms, Tree Schedule A applies to small-scale projects in residential or industrial zoning districts, Tree Schedule B applies to moderate-scale projects or projects in commercial or mixed-use zoning districts, and Tree Schedule C applies to larger projects. In the following chart, please check the applicable box based on the characteristics of your project.

TREE SCHEDULE	PROJECT CHARACTERISTICS	
<input type="checkbox"/>	A	The project is located in a Residential (RH, RM, RTO, RED), Industrial (M) or Production/Distribution/Repair (PDR) Zoning District and does not involve a Planned Unit Development (PUD). A PUD is a special authorization granted by the Planning Commission that applies only to major projects involving large properties.
<input checked="" type="checkbox"/>	B	<p>1. The project is located in a RH, RM, RTO, RED, M or PDR Zoning District and involves a PUD</p> <p style="text-align: center;">OR</p> <p>2. The project is located outside of an RH, RM, RTO, RED, M or PDR Zoning District and meets neither OR one of the following criteria, but not both:</p> <ul style="list-style-type: none"> <input checked="" type="checkbox"/> It is located on a parcel that contains (1) more than 1/2-acre in total area or (2) more than 250 feet of total street frontage or (3) street frontage which spans the entire block face between the nearest two intersections. <input checked="" type="checkbox"/> It involves (1) the construction of a new building or (2) the addition of more than 20% of the gross floor area of the existing building or (3) a change of use of more than 50% of the existing square footage of the building.
<input type="checkbox"/>	C	The project is located outside of an RH, RM, RTO, RED, M or PDR Zoning District and meets both criteria of Tree Schedule B(2), above.

TREE SCHEDULE A

REQUIREMENT	REGULATION
<input checked="" type="checkbox"/> Location	either in the public right-of-way (e.g. sidewalk) adjacent to the property or within an unbuilt area at the front of the property
<input checked="" type="checkbox"/> Size	minimum of 24-inch box size

TREE SCHEDULE B

REQUIREMENT	SPECIFICATION
✓ Location	shall be in the public right-of-way (e.g., sidewalk) adjacent to the property or within an unfull area at the front of the property
✓ Size	minimum 2 inch caliper, measured at breast height branch a minimum of 80 inches above sidewalk grade
✓ Opening	be planted in a sidewalk opening of at least 16 square feet have a minimum soil depth of 3 feet 8 inches include a basin edged with decorative treatment, such as pavers or cobbles (edging will not count against the minimum 16 square foot opening if the edging material is permeable. A permeable material is one that allows stormwater to infiltrate the underlying soils. Permeable surfaces shall include, but not be limited to, vegetative planting beds, porous asphalt, porous concrete, single-street aggregate, open-jointed blocks, stone, pavers or brick that are loose-set and without mortar. Permeable surfaces are required to be contained so neither sediment nor the permeable surface discharges off the site.

TREE SCHEDULE C

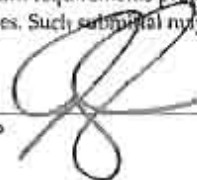
REQUIREMENT	SPECIFICATION
✓ Location	As set forth in Schedule B, above.
✓ Size	
✓ Opening	
✓ Trenching	Trees must be planted in a continuous soil-filled trench parallel to the curb, such that the basin for each tree is connected. The trench may be covered by permeable surfaces (as described above), except at required tree basins, where the soil must remain uncovered.

Applicant's Affidavit

I hereby attest under penalty of perjury that the information I have entered on this document is true and correct to the best of my knowledge, and that I have read and understood this form, and that I am the property owner or authorized agent of the property owner, familiar with the property, and able to provide accurate and complete information herein.

The undersigned agrees to the conditions of this form. I understand that knowingly or negligently providing false or misleading information in response to this disclosure requirement may lead to denial or rescission of my permit or other authorization and may constitute a violation of the San Francisco Municipal Code, which can lead to criminal and/or civil legal action and the imposition of administrative fines.

I understand that should my project be subject to a required Tree Protection Plan, that I will have a plan meeting or exceeding the minimum requirements prepared and submit it to the Department of Public Works prior to the commencement of any construction activities. Such submittal may be in person, by mail or via email at urbanforestrypermits@sfdpw.org.

Signature 

Date 7-18-12

John Funghi, Program Director
Print Name Indicate whether owner, or authorized agent.
Owner Authorized Agent

(415) 701-5262
Phone Number

Phone Number

john.funghi@sfmta.com
Fax or Email

Planning Department Determination

TO BE COMPLETED BY STAFF ONLY. DO NOT LEAVE ANY SECTION BLANK.

PROJECT NUMBER / CASE #:	
PLANS DATE:	
New Street Trees	<input type="checkbox"/> New street trees are not required as part of this project. <input type="checkbox"/> Street Trees are required as part of this project. Number of new street trees required: _____ Applicable Tree Schedule: 1. A 2. B 3. C Compliance with as-of-right requirements shown on plans? 1. YES 2. NO - MODIFICATION OR WAIVER APPROVED; EXPLAIN IN COMMENTS, BELOW
Existing Tree Protection	<input type="checkbox"/> A Tree Protection Plan is not required: Box 1 or Box 2 in Section 5 has been marked. <input type="checkbox"/> A Tree Protection Plan is required: Box 3 in Section 5 has been marked.
Existing Tree Removal	<input type="checkbox"/> No Protected Trees are proposed for removal. <input type="checkbox"/> One or more Protected Trees are proposed for removal.
STAFF TO SIGN UNLESS A WAIVER OR MODIFICATION HAS BEEN APPROVED, IN WHICH CASE A SIGNATURE IS REQUIRED.	
Signature _____	Print Name _____ Date _____
Comments (if any): _____	

Staff Checklist

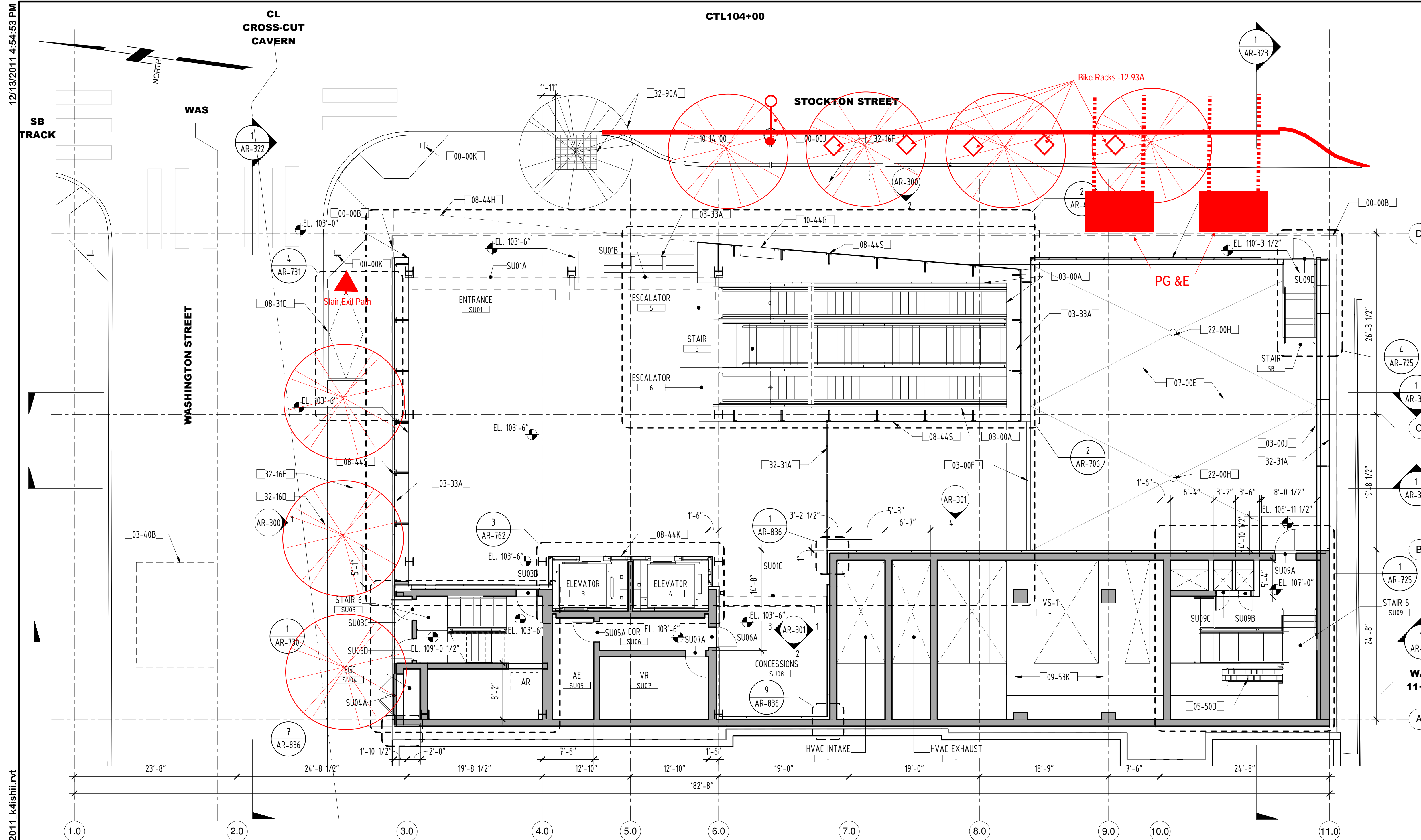
- ✓ The applicant has completed this entire checklist including the affidavit on the preceding page.
- ✓ If street trees are required, a building permit cannot be approved until the applicant provides evidence from DPW that the required planting permit can be issued.
- ✓ If Protected Trees are proposed for removal, a building permit cannot be approved until the applicant provides evidence from DPW that tree removal permits can be issued.
- ✓ If a Tree Protection Plan is required, the applicant has been informed verbally and/or in writing of his or her obligation to submit one directly to DPW prior to the commencement of construction.
- ✓ Once signed, a copy of this checklist has been returned to the applicant. The original has been included in the project file or, if processed over-the-counter, it has been routed upstairs for scanning by support staff.

ECP-FD038

Attachment 4

SF Planning Commission
Exhibit "CTS Trees1"
Case No. 2012.0641C

Chinatown Station

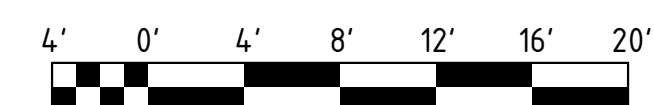


- GENERAL NOTES**
1. REFER TO SHEET AR-001 FOR GENERAL INFORMATION.
 2. FOR PLATFORM EDGE AND OFFSET FROM TRACK ALIGNMENT, REFER TO TRACK ALIGNMENT (TC) DRAWINGS
 3. ELEVATIONS ARE TO TOP OF FINISH U.O.N.

- SHEET NOTES**
- 00-00B PROPERTY LINE
 - 00-00J STREET LIGHT; TYP.
 - 00-00K TRAFFIC LIGHT; TYP.
 - 03-00A COORD. W/ STRUCT. FOR EDGE OF SLAB, TYP.
 - 03-00F 6" CONC. CURB
 - 03-00J CONC. SLURRY WALL; SEE ST DWGS
 - 03-33A 03 33 00: ARCHITECTURAL CONCRETE; GREY CEMENT & INTEGRATED COLOR & SEALANT W/ HD0 FORMLINER; PROVIDE RUBBED FINISH. SMOOTH FINISH TO BE EQUIVALENT TO ASTM C840, LEVEL 5 FINISH. SEE AR-863.
 - 03-40B MAINTENANCE HATCH BELOW
 - 05-50D 05 50 00: ALTERNATING TREAD LADDER
 - 07-00E GRAVEL O/ DRAINAGE COMPOSITE BOARD O/ WATERPROOFING MEMBRANE O/ SLOPED TOPPING SLAB.
 - 08-31C 08 31 00: IN-GRADE EMERGENCY EGRESS HATCH @ STAIR 7; SEE AR-813
 - 08-44H GLASS PANELS ABOVE
 - 08-44K 08 44 13: GLASS ELEVATOR ENCLOSURE, TEMPERED LAMINATED GLASS
 - 08-44S 08 44 26: GLASS PANELS SUPPORTED BY GLASS FIN STABILIZERS W/ SST CONNECTOR FITTINGS (WALL)
 - 09-53K ALL UTILITIES WITHIN EMERGENCY VENT SHAFT SHALL BE ENCLOSED WITHIN A 2-HOUR FIRE RATED METAL SOFFIT ENCLOSURE
 - 10 14 00 SIGNAGE
 - 10-44G 10 44 00: SFFD COMMAND POST CABINET
 - 12-93A 12 93 00: BICYCLE RACK, DERO 3V OR EQUAL, SS SATIN FINISH
 - 22-00H FLOOR DRAIN; SEE MP DWGS
 - 32-16D SIDEWALK CURB
 - 32-16F (N) SIDEWALK PAVEMENT; SEE RP DWGS
 - 32-31A 32 31 13: MTL. POST & CORRUGATED/PERFORATED MTL. PANEL FENCE ON A CONCRETE CURB. POSTS ARE HSS 4"x4" 4'-0" O.C. SEE ST DRAWINGS FOR CONNECTION DETAILS. PANELS ARE S.S. CENTRIA ECOSCREEN ECONOLAP 3/4" 40% OPEN AREA ATTACHED TO STEEL POSTS.
 - 32-90A STREET TREE AND TREE WELL; SEE LANDSCAPING DWGS

8.0 SURFACE - SECTOR 4

1 PLAN
AR-114 1/8" = 1'-0"



LEGEND

STRUCTURAL ELEMENTS; SEE ST DWGS.

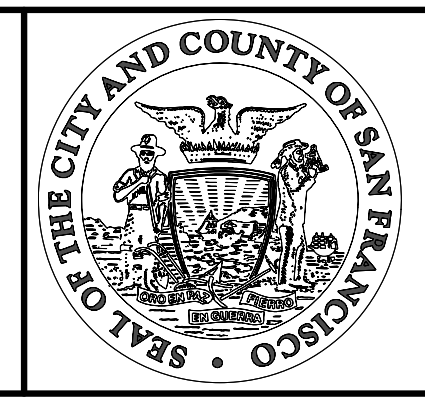
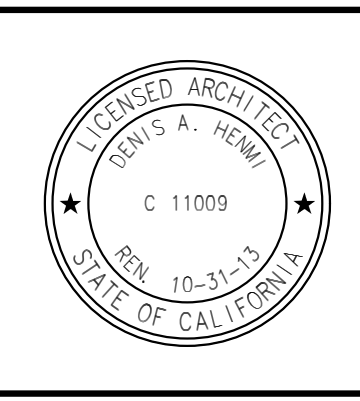
KEY PLAN

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DATE	ISSUED FOR BID	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
12/14/2011	ISSUED FOR BID		0			

central subway design group

DESIGNED
M. TAMARI
DRAWN
C. CASADELLA
CHECKED
J. TRAN
REVIEWED
D. HENMI
RECOMMENDED
A. READ
APPROVED
R. EDWARDS
DATE
12/14/2011



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED

DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY CHINATOWN STATION

ARCHITECTURE
FLOOR PLAN - SURFACE LEVEL - 4

CONTRACT NO. **1254**

SFMTA CONTROL NO. **CL-19901**

DRAWING NO. **AR-114**

SHEET NO. **4**

REVISION **0**

ECP-FD038

Attachment 5

Contract 1300
Work Package CTS-1254R

Drawing AR-100 (IFB)
Architectural Site Plan

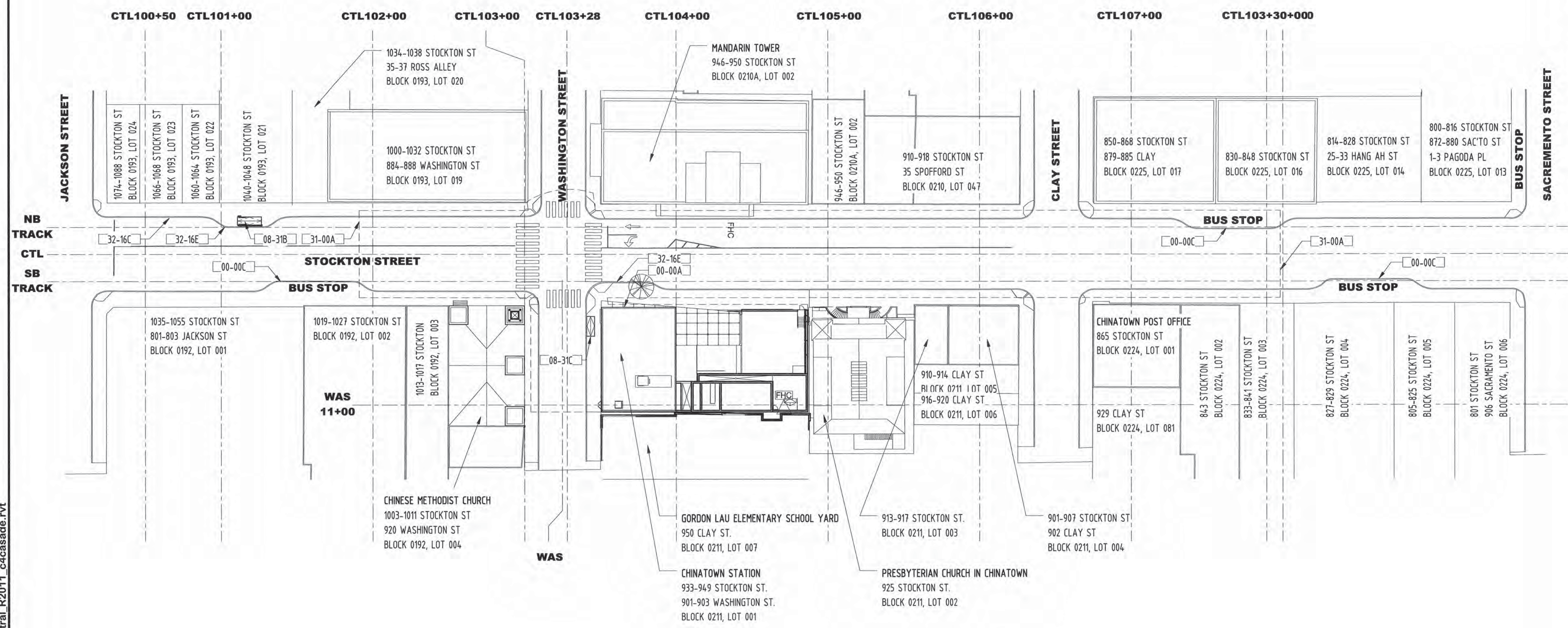
Chinatown Station

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- GENERAL NOTES**
- REFER TO SHEET AR-001 FOR GENERAL INFORMATION.
 - FOR PLATFORM EDGE AND OFFSET FROM TRACK ALIGNMENT, REFER TO TRACK ALIGNMENT (TC) DRAWINGS
 - ELEVATIONS ARE TO TOP OF FINISH U.O.N.

- SHEET NOTES**
- 00-00A STATION ENTRY
 - 00-00C (E) BUS STOP
 - 08-31B 08 31 00: IN-GRADE EMERGENCY EGRESS HATCH @ STAIR 4; SEE AR-813
 - 08-31C 08 31 00: IN-GRADE EMERGENCY EGRESS HATCH @ STAIR 7; SEE AR-813
 - 31-00A OUTLINE OF STATION BELOW
 - 32-16C SIDEWALK
 - 32-16E (N) BULBOUT SEE RP DWGS



SITE PLAN
1 AR-100 1" = 40'-0"

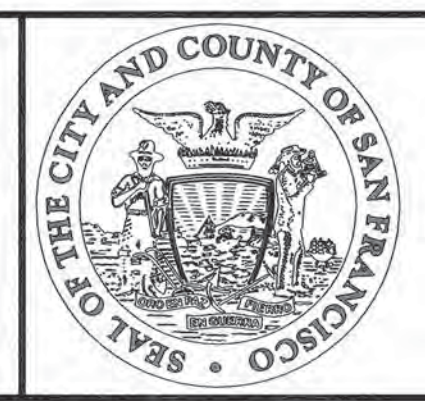


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DATE	DESCRIPTION	REV NO.	BY	CHECKED	APPROVED
08/20/2012	ISSUED FOR 1254R	0			
12/14/2011	ISSUED FOR BID	0			

central subway design group

DESIGNED: M. TAMARI
 DRAWN: C. CASADELLA
 CHECKED: J. TRAN
 REVIEWED: D. HENMI
 RECOMMENDED: A. READ
 APPROVED: R. EDWARDS
 DATE: 12/14/2011



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED: [Signature]
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 CHINATOWN STATION

**ARCHITECTURE
 SITE PLAN**

CONTRACT NO.	1254 R
SFMTA CONTROL NO.	CL-19899
DRAWING NO.	AR-100
SHEET NO.	411
REVISION	0

CONFORMED

ECP-FD038

Attachment 6

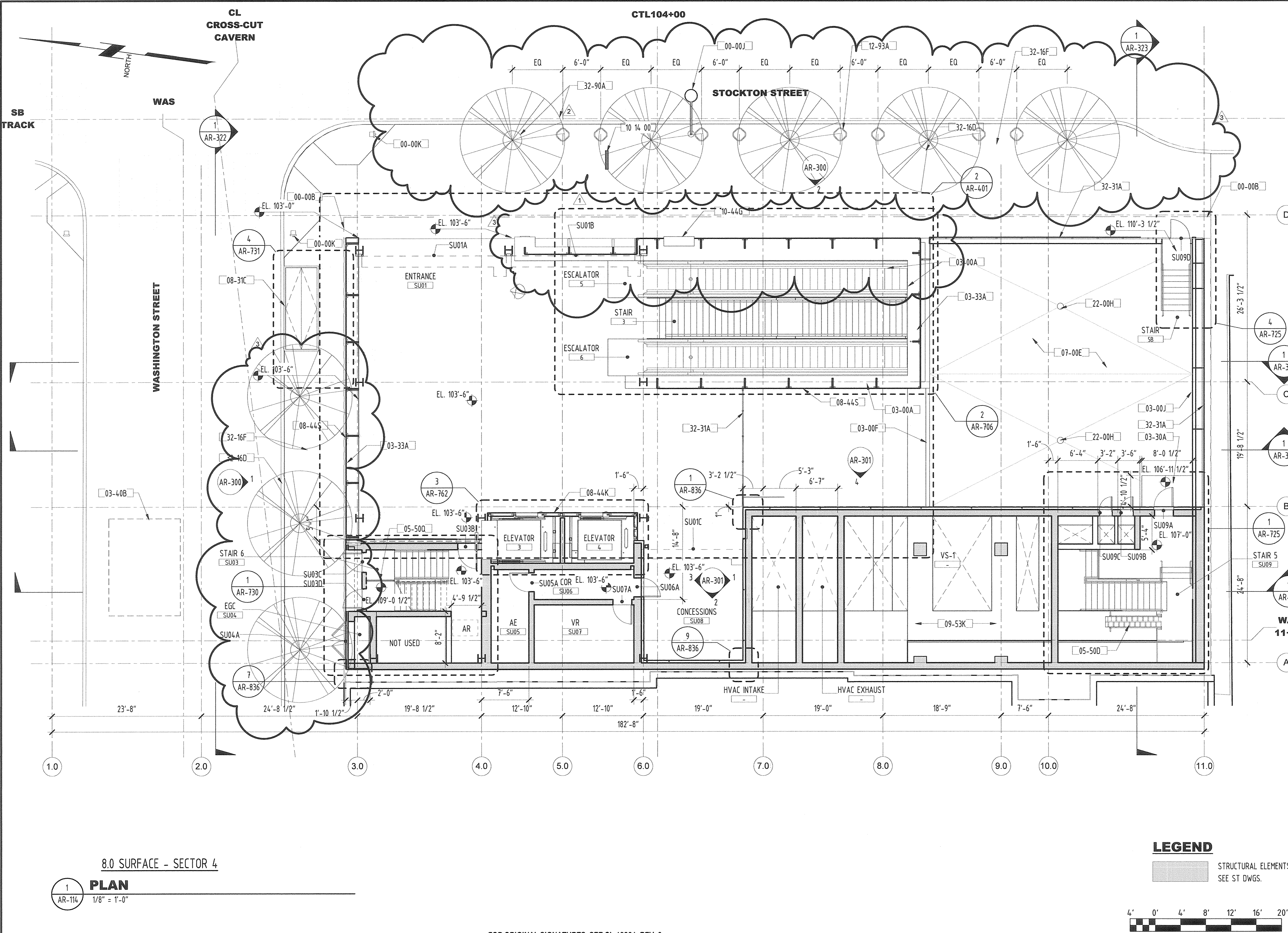
Contract 1300
Work Package CTS-1254R

Drawing AR-114 (Add. #3)
Drawing LA-100 (Add. #3)
Drawing RP-101 (Add. #4)

Chinatown Station

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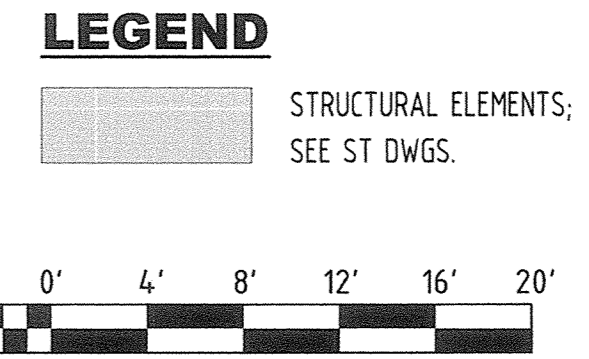


- GENERAL NOTES**
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 - FOR PLATFORM EDGE AND OFFSET FROM TRACK ALIGNMENT, REFER TO TRACK ALIGNMENT (TC) DRAWINGS
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- SHEET NOTES**
- 00-00B PROPERTY LINE
 - 00-00J STREET LIGHT; TYP.
 - 00-00K TRAFFIC LIGHT; TYP.
 - 03-00A COORD. W/ STRUCT. FOR EDGE OF SLAB, TYP.
 - 03-00F 6" CONC. CURB
 - 03-00J CONC. SLURRY WALL; SEE ST DWGS
 - 03-30A 03 30 00: CIP CONCRETE PAD OVER DRAINAGE COMPOSITE AND WATERPROOF MEMBRANE
 - 03-33A 03 33 00: ARCHITECTURAL CONCRETE; GREY CEMENT & INTEGRATED COLOR (DAVIS 'GRAPHITE' OR SIM.) & SEALANT W/ HDO FORMLINER; PROVIDE RUBBED FINISH. SMOOTH FINISH TO BE EQUIVALENT TO ASTM C840, LEVEL 5 FINISH. SEE AR-863.
 - 03-40B MAINTENANCE HATCH BELOW
 - 05-50D 05 50 00: ALTERNATING TREAD LADDER TO COMPLY W/ REQUIREMENTS OF 2007 CBC 1009.9
 - 05-50Q 05 50 00: METAL POST & FENCE FLOOR TO CEILING W/ 1" DIA. ROUND HORIZONTAL RAILS 4" O.C.
 - 07-00E GRAVEL O/ DRAINAGE COMPOSITE BOARD O/ WATERPROOFING MEMBRANE O/ SLOPED TOPPING SLAB.
 - 08-31C 08 31 00: IN-GRADE EMERGENCY EGRESS HATCH @ STAIR 7; SEE AR-813
 - 08-44K 08 44 13: GLASS ELEVATOR ENCLOSURE, TEMPERED LAMINATED GLASS
 - 08-44S 08 44 26: GLASS PANELS SUPPORTED BY GLASS FIN STABILIZERS W/ SST CONNECTOR FITTINGS (WALL TYPE 1 SEE SPECS)
 - 09-53K ALL UTILITIES WITHIN EMERGENCY VENT SHAFT SHALL BE ENCLOSED WITHIN A 2-HOUR FIRE RATED METAL SOFFIT ENCLOSURE
 - 10 14 00 SIGNAGE
 - 10-44G 10 44 00: SFFD COMMAND POST CABINET
 - 12-93A 12 93 00: BICYCLE RACK, DERO 3V OR EQUAL, SS SATIN FINISH
 - 22-00H FLOOR DRAIN; SEE MP DWGS
 - 32-16D SIDEWALK CURB
 - 32-16F (N) SIDEWALK PAVEMENT; SEE RP DWGS
 - 32-31A 32 31 13: MTL. POST & CORRUGATED/PERFORATED MTL. PANEL FENCE ON A CONCRETE CURB. POSTS ARE GALVANIZED HSS 4"x4" 4'-0" O.C. SEE ST DRAWINGS FOR CONNECTION DETAILS. PANELS ARE S.S. CENTRIA ECOSCREEN ECONOLAP 3/4" 40% OPEN AREA ATTACHED TO STEEL POSTS.
 - 32-90A STREET TREE AND TREE WELL; SEE LANDSCAPING DWGS

8.0 SURFACE - SECTOR 4

PLAN
1/8" = 1'-0"



FOR ORIGINAL SIGNATURES, SEE CL-19901, REV. 0.

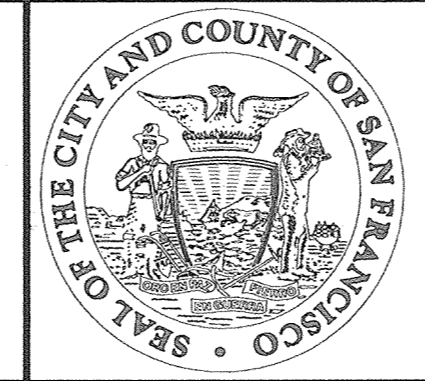
DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
01/18/2013	ISSUED FOR CONTRACT 1300 ADDENDUM NO. 3	3	MT	DH	FRE
08/20/2012	ISSUED FOR 1254R	2	MT	DH	FRE
04/10/2012	ISSUED FOR ADDENDUM NO. 2	1	MT	DH	FRE
12/14/2011	ISSUED FOR BID	0			

central subway design group

KH
KwanHenmi

DESIGNED BY M. TAMARI
DRAWN BY C. CASADELLA
CHECKED BY J. TRAN
REVIEWED BY D. HENMI
RECOMMENDED BY A. READ
APPROVED BY R. EDWARDS
DATE 12/14/2011

REV. 0
SEALED BY
D. HENMI



CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY

APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
CHINATOWN STATION

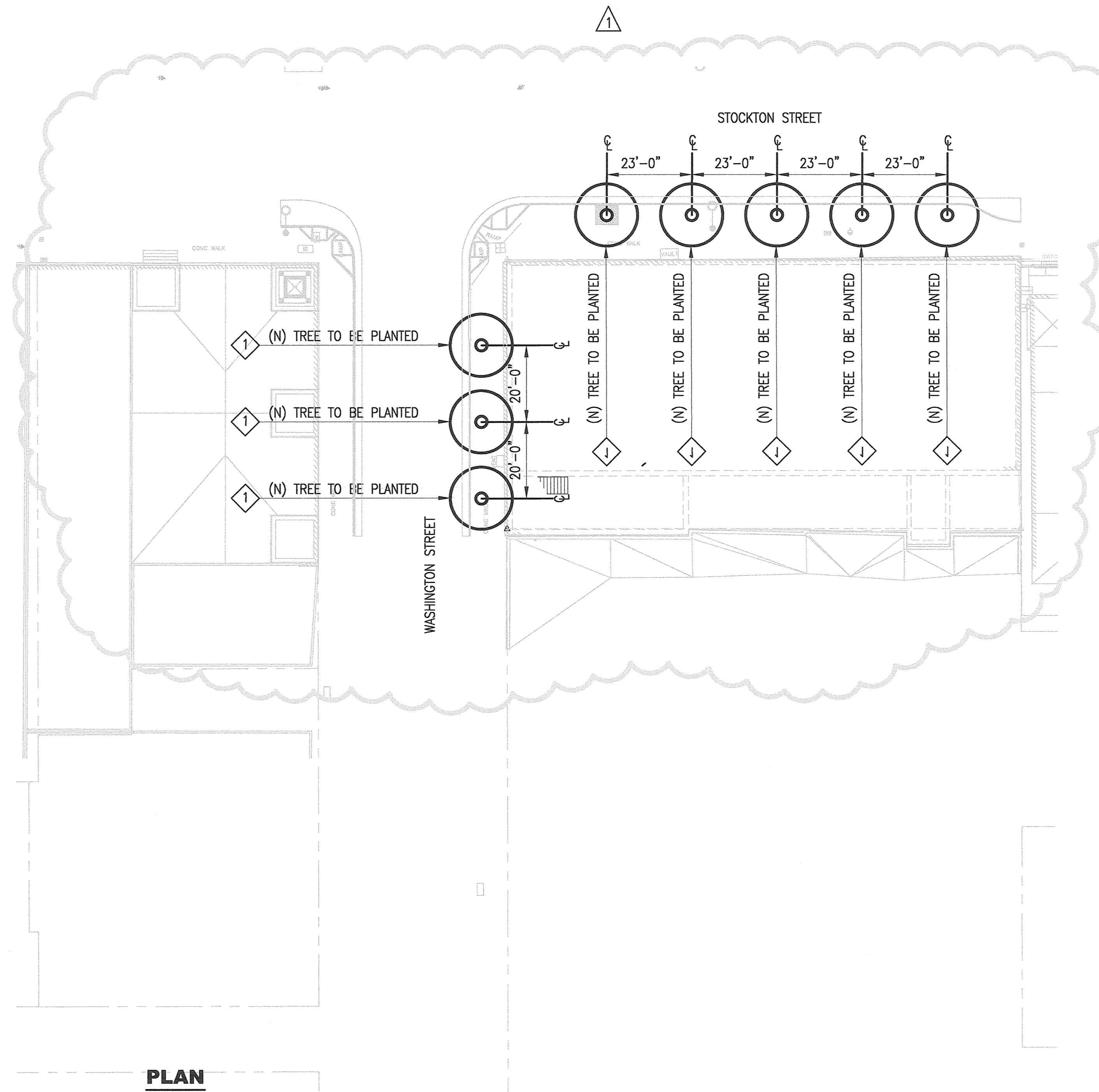
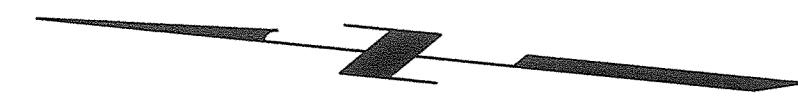
ARCHITECTURE
FLOOR PLAN - SURFACE LEVEL - 4

CONTRACT NO. 1254R
SFMTA CONTROL NO. CL-19901
DRAWING NO. AR-114
SHEET NO. 413
REVISION 3

CONFORMED

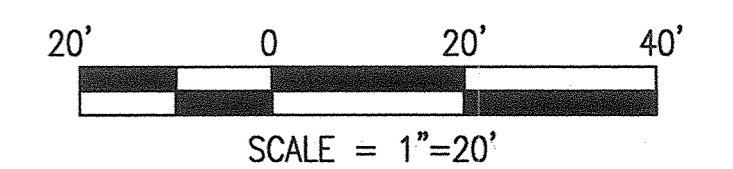
- NOTES:**
- (N) TREE TO BE PLANTED, SEE DETAIL 1, PLANT LEGEND, SHEET LA-200, AND SPECIFICATIONS.
 - FOR TREE WELL DETAIL, SEE DETAIL 2, SHEET LA-200.

- LEGEND**
- (N) TREE TO BE PLANTED, SEE DETAIL 1, PLANT LEGEND, SHEET LA-200, AND SPECIFICATIONS.



PLAN

FOR ORIGINAL SIGNATURES, SEE CL-19572, REV. 0.



\\sv6nas003\m544.1\FinalDesign\DRAWINGS\CSI155-2\1254\SHEET FILES\02_CIVIL\15_LANDSCAPING\12540215LA100.dwg
 c4sharma Fri Jan 11, 2013 3:14 pm LA-100

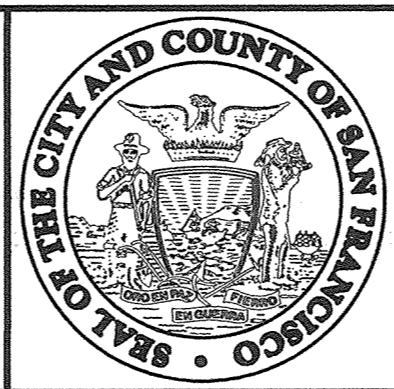
DATE	DESCRIPTION	REV. NO.	BY	CHECKED	APPROVED
01/18/2013	ISSUED FOR CONTRACT 1300 ADDENDUM NO. 3	1	CS	MS	[Signature]
08/20/2012	ISSUED FOR 1254R	0			
12/14/2011	ISSUED FOR BID	0			

central subway design group

STEVENS & ASSOCIATES

DESIGNED
C. SHARMA
DRAWN
C. SHARMA
CHECKED
M. STEVENS
REVIEWED
D. HENMI
RECOMMENDED
A. READ
APPROVED
R. EDWARDS
DATE
12/14/2011

REV. 0
SEALED BY
MYLES STEVENS

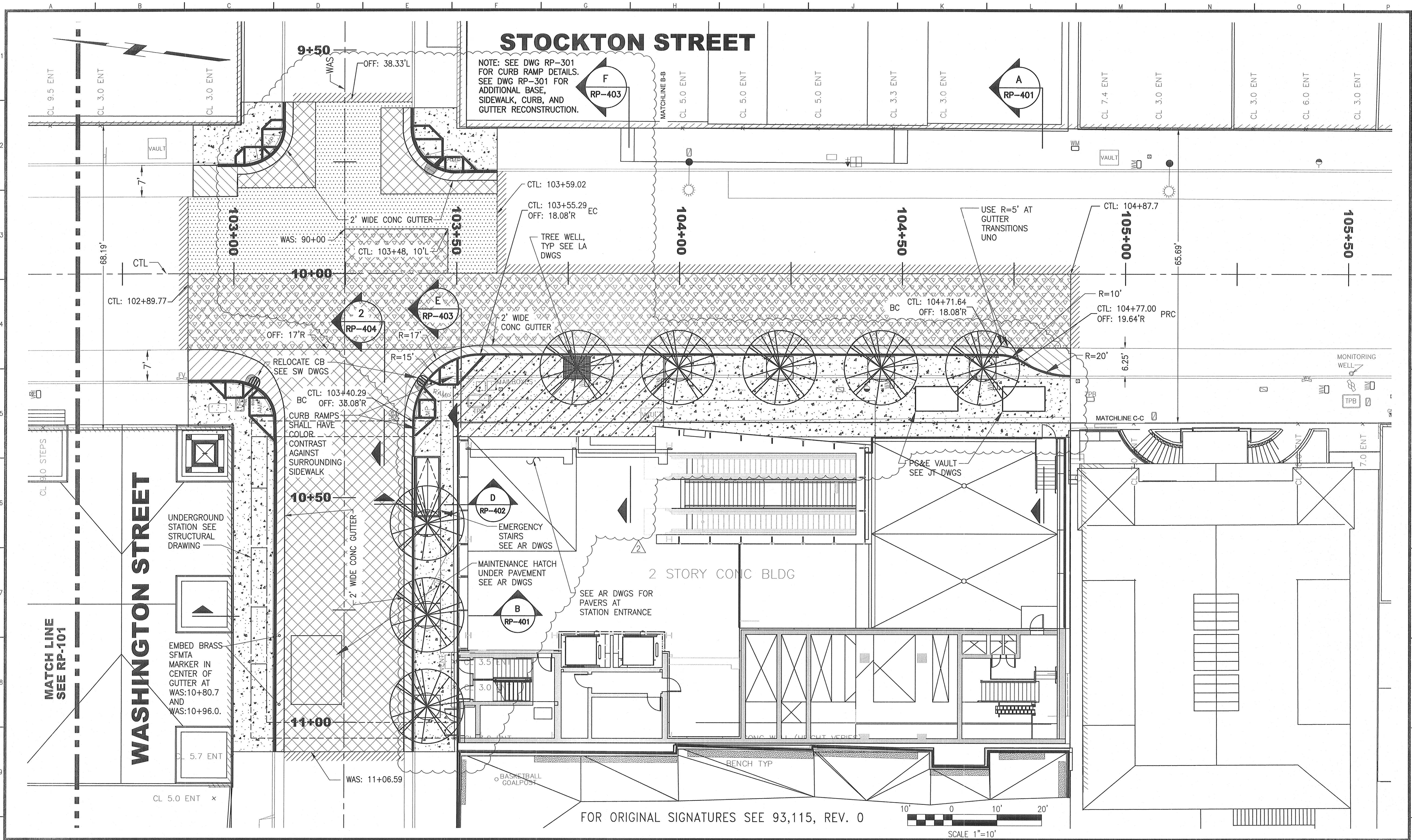


CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
APPROVED
EDWARD D. REISKIN
DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
CHINATOWN STATION
**LANDSCAPING
PLANTING**

CONTRACT NO. 1254R	
SFMTA CONTROL NO. CL-19572	
DRAWING NO. LA-100	REVISION
SHEET NO. 105	1

CONFORMED



STOCKTON STREET

WASHINGTON STREET

NOTE: SEE DWG RP-301 FOR CURB RAMP DETAILS. SEE DWG RP-301 FOR ADDITIONAL BASE, SIDEWALK, CURB, AND GUTTER RECONSTRUCTION.

RP-403

RP-401

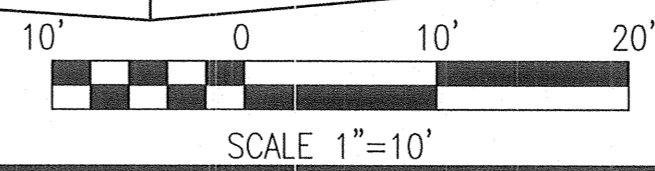
RP-404

RP-403

RP-402

RP-401

FOR ORIGINAL SIGNATURES SEE 93,115, REV. 0



NO.	DATE	DESCRIPTION	BY	APP.
2	2/15/2013	ISSUED FOR CONTRACT 1300 ADDENDUM 45	SJO	CY
1	1/18/2013	ISSUED FOR CONTRACT 1300 ADDENDUM 3	SJO	CY
0	12/14/2011	ISSUED FOR BID	SJO	CY

TABLE OF REVISIONS
CHECK WITH TRACING TO SEE IF YOU HAVE LATEST REVISION

REFERENCE INFORMATION & FILE NO. OF SURVEYS



INFRASTRUCTURE DIVISION

DEPARTMENT OF PUBLIC WORKS

CITY AND COUNTY OF SAN FRANCISCO

REV. 0
SEALED BY
S. O'SULLIVAN

DESIGNED:	DATE:	APPROVED:	DATE:
S.O'Sullivan	9/11	F. CISNEROS	12/15/11
DRAWN:	DATE:	SECTION MANAGER:	DATE:
S.O'Sullivan	9/11	M. DIVINE	12/16/11
CHECKED:	DATE:	DEPUTY DIVISION MANAGER:	DATE:
C.Yu	9/11	M. DIVINE	12/19/11
		DIVISION MANAGER:	DATE:

SCALE:	1" = 10'
SHEET OF SHEETS	

CONTRACT NO.	1254R
DRAWING NO.	RP-102
FILE NO.	93,115
REV. NO.	2

THIRD STREET LIGHT RAIL PROGRAM
PHASE 2 - CENTRAL SUBWAY
CHINATOWN STATION
PAVEMENT RENOVATION
PLAN
CTL 102+65 TO 105+50

CONTRACT NO.	1254R
DRAWING NO.	RP-102
FILE NO.	93,115
REV. NO.	2

CONFORMED

Drawing: Working_Drawings_ESH_CPS_Chinatown_Station_1254_M1254_RP_PAVING_130207.dwg, Login: osullivan

SUBJECT:

Configuration Management Board Meeting # 100– Rev. No. 0

RECORD OF MEETING (*Italicized text indicates status update of open items*)

ITEM #	DISCUSSION	ACTION BY DUE DATE
1-	<p>1254 – R. Edwards and Quon Chin presented for information as a potential- ECP FD-00XX – Elongated Sidewalk Bulb out at Chinatown Station, which would extend the planned pedestrian curb extension at the southwest corner of Washington 130 feet to accommodate a bus stop. The SF Planning Department requested change would bring CTS in close conformance with the City’s General Plan to create a “station plaza”. In addition the SF Planning Department requested the installation of street trees along Stockton Street adjacent to the Chinatown Station and trees along Washington Street. The proposed design change will require design drawing modifications of several disciplines. Construction and design costs need to be developed and vetted. The CMB required that the Engineering Change Proposal be brought back to the CMB at later time when more specific information is available and cost schedule impacts identified.</p>	
2-	<p>1251 - M. Benson and M. Acosta presented for approval Trends related to the</p>	

CMB ACTION ITEM

CTS Bulb-Out has been provided into Contract 1300 and includes tree & bike racks per SF Planning CUA

1254R – Chinatown Station

Sub-Item a: R. Edwards and Quon Chin presented for information as a potential- ECP FD-00XX – Elongated Sidewalk Bulb out at Chinatown Station, which would extend the planned pedestrian curb extension at the southwest corner of Washington 130 feet to accommodate a bus stop. The SF Planning Department requested change would bring CTS in close conformance with the City's General Plan to create a "station plaza".

Sub-Item b: In addition the SF Planning Department requested the installation of street trees along Stockton Street adjacent to the Chinatown Station and trees along Washington Street.

Sub-Item c: The proposed design change will require design drawing modifications of several disciplines. **Construction and design costs need to be developed and vetted.**

Sub-Item d: The CMB required that the Engineering Change Proposal be brought back to the CMB at later time when **more specific information is available and cost schedule impacts identified.**

ARE INCLUDED

- a. SF Planning GPR requirement for **elongated bulb-out on Stockton Street** included in Contract 1300 Addendum No. 3: Drawings AR-114 Rev. 3, RP-102 Rev. 1 and RP-202 Rev. 1.
- b. SF Planning GPR requirement of **trees included** in Contract 1300 Addendum No. 3: Drawing LA-100 (Rev. 1), and AR-114 (Rev. 3)
- c. ROM Construction Cost Estimate for elongated sidewalk, trees, tree wells and bicycle racks = \$36,200
- c. ROM Design Costs:
 - DPW about 97 hrs / \$16,900: Modifies four (4) drawings, coordinate w/LA and Architect plus TASC drawing for beginning sidewalk legislation;
 - CSDG 60 architectural hours: evaluation of bus berthing of different fleet vehicles, bus doors & mirrors to tree locations. Develop sketch.
- d. ECP-038 prepared for routing.

SUBJECT: Configuration Management Board Meeting # 127 – Rev. No. 0

RECORD OF MEETING (*Italicized text indicates status update of open items*)

ITEM #	DISCUSSION	ACTION BY DUE DATE
7- 04/03/13	1252 – R. Edwards presented PCC 012 - Revised Tunnel Alignment for approval of the Re-profile of tunnel to avoid the micropile installed under CN1251. In response to RFI 179 this PCC directs the Contractor to use the revised micropile northbound and southbound profile to avoid conflict with existing micropile. The change is required so that TBM and Tunnel structure is below previously installed micro-piles (see attached). The CMB recommend a request be made to the Contractor to segregate all changes so the information may be included in the documentation regarding the Designers E&O issue. Also the CMB need evidence that the design change has been routed and is signed off by the various disciplines as backup documentation for the files. Notification of the profile change will be submitted to BART and the IRP. Status: <i>05/29 This item was not discussed at this week's meeting.</i>	RE 06/05/13
8	1252 – Design Services During Construction – M. Benson presented for a later discussion	MR

ITEM #	DISCUSSION	ACTION BY DUE DATE
	<i>the Contractor an actual invoice which can be compared to the cost estimate as validation of the rental rate. This item will be brought back to the CMB at a later date.</i>	
5- 05/08/13	<p>1252 – M. Benson requested negotiation position for PCC 06 cost comparison of Additional BART Tunnel Instrumentation (see attached). The CMB expressed concerned that the \$624K price to perform the work was too high and requested Jane Wang to contact the IRP in writing to find out if the additional instrumentation monitoring was still warranted given the new lowered tunnel alignment and the cost to implement the work. The RE will confirm the date for which a final decision by SFMTA is to be made to forward to the Contractor. This item will be brought back to the CMB at a later date. Status: 05/29 This item was not discussed at this week's meeting.</p>	JW 06/05/13

May 20, 2013

Independent Review Panel
American Public Transportation Association
1666 K Street, N.W., 11th Floor
Washington, DC 20006-1215

Attn: Mr. Martin P. Schroeder, P.E.
Chief Engineer
Peer Review Facilitator

Reference: Central Subway BART Undercrossing

Subject: Tunnel Design Changes

Dear Mr. Schroeder:

The purpose of this letter is to advise the IRP of recent changes and developments in the tunneling contract.

Due to the as-built conditions of piles in the vicinity of the tunnel alignment, the tunnel profiles were recently re-designed to be lowered. The revised tunnel profile below the BART tubes is now approximately 8 feet deeper, or a minimum 15.5 feet clearance between the BART tunnels and the new CS tunnels. Increasing the separation between the two tunnels was one of the top recommendations by the IRP.

Secondly, the Program recently received a cost proposal from the contractor for the additional work that was suggested by the IRP: to determine (a) the actual preload forces in the existing tunnel liner bolts, and (b) actual bolt forces along the circumference (the IRP recommended measuring the forces in the tunnel crown but BART requested a more representative sampling along the circumference). The contractor's original cost proposal for performing this work was approximately \$760,000. While we appreciate BART's efforts in agreeing to reduce the scope of work, the revised cost proposal is still significant at \$470,000.

In light of the increased separation between the BART and CS tunnels, we do not believe the planned testing described above are critical or add value relative to the cost of the work. The existing bolts within the zone of influence were intact and in good condition based on the survey performed by our design consultant, PB/Telamon, and verified by the IRP inspection.

At around the same time of the IRP meeting last year, BART had requested additional monitoring of the rails for detecting rail deflection under dynamic train loading. The quote for dynamic polling using strain gauges is approximately \$180,000. Tilt beam sensors are already planned to be installed to detect movement of the rails.

We believe the monitoring systems required as part of the original contract work – 14 tilt beams and 30 prism arrays along the circumference and on the tracks within the 340' assessment zone, are adequate in determining the impacts, if any, on BART facilities. The contract value of the originally required work is approximately \$400,000.

The Program respectfully requests IRP's reconsideration of the peer review suggestions and concurrence on our recommendation to eliminate the three tests: bolt pre-load tension, load cells on the bolts, and the rail deflection under train load.

If you have any questions or require further clarification, please contact Design Oversight Manager Jane Wang at 415.701.4287.

Sincerely,



John Funghi
Program Director

cc: Albert Hoe, SFMTA
Ross Edwards, CSP
Jane Wang, SFMTA
Pepe Vallenias, BART
CS File No. M544.1.3.1252.0092

INVOICE



Hammer & Steel, Inc.
Piling and Pile Driving Equipment

P.O. Box 86936
St. Louis, Missouri 63166-6936
314-895-4600
1-800-325-PILE (7453)

NO. 817-05009

DATE 5/9/2013

S O L D T O

CONDON-JOHNSON & ASSOCIATES
CONDON JOHNSON-MCC JOINT VENTU
480 ROLAND WAY STE 700
OAKLAND CA 94621

S H I P T O

CONDON-JOHNSON & ASSOCIATES
WILL CALL - HAMMER & STEEL, INC.
401 WEST CHANNEL ROAD
BENICIA CA 94510

CUSTOMER ORDER NO.	SHIPPED VIA	TERMS	SALESMAN	F.O.B.	OUR ORDER NO.
JOB #21134	CUSTOMER PICK UP	NET 10 DAYS	DC	BENICIA CA	17554
QTY SHIPPED	DESCRIPTION	UNIT PRICE	AMOUNT		

1.00	COMACCHIO MC602 SN 1987 COMACCHIO MC602 DIESEL POWER PACK SN 1987D MC602 RADIO REMOTE SN 528406	\$18,000.00	\$18,000.00
------	---	-------------	-------------

RENTAL RATE: \$18,000.00/MONTH

FOR 1ST MONTH RENTAL FOR PERIOD 5/6/2013 THRU 6/5/2013

PROJECT: THIRD ST. LIGHT RAIL PROGRAM PHASE 2 CENTRAL SUBWAY TUNNELING,
4TH STREET & STOCKTON STREET, SAN FRANCISCO, CA

RECEIVED
MAY 13 2013
CONDON-JOHNSON



PAYMENT DUE DATE: 5/19/2013

CA 7.625%

Subtotal	\$18,000.00
Tax	\$1,372.50
Freight	\$0.00
Total	\$19,372.50

INTEREST CHARGED AFTER 30 DAYS AT MAXIMUM LEGAL RATE
BUYER TO PAY ALL APPLICABLE FEDERAL, STATE OR OTHER TAXES ON GOODS RENTED OR SOLD

CONDITIONS

- SELLER SHALL NOT BE LIABLE FOR DELAYS OCCASIONED BY FIRE, STRIKES, ACCIDENTS, OTHER CATASTROPHIC EVENTS, DIFFERENCES WITH EMPLOYEES OR OTHER CAUSES BEYOND ITS CONTROL.
- NO CLAIM FOR ALLOWANCE WILL BE CONSIDERED OR RECOGNIZED UNLESS PRESENTED IN WRITING WITHIN TWENTY (20) DAYS AFTER RECEIPT OF MATERIAL. THERE ARE NO WARRANTIES OF SELLER WHICH EXTEND BEYOND THE DESCRIPTION OF MATERIALS ON THE FACE OF THIS INVOICE, AND SELLER WILL NOT PAY FOR LABOR EXPENSE OR DAMAGE RESULTING FROM ITS USE.
- CARRIERS ARE RESPONSIBLE FOR MATERIALS LOST OR DAMAGED IN TRANSIT AND CONSIGNEE MUST IMMEDIATELY NOTIFY THE CARRIER OF SUCH LOSS OR DAMAGE.

Thank You

Hammer & Steel, Inc.

Piling-Pile Driving Equipment-Drilling Equipment
401 West Channel Road
Benicia, CA 94510
Ph: 1-877-224-3356
Fx: 707-746-7821



2/134-CC

Date: May 1, 2013

To: Tom Baddeley, Condon Johnson (CIA-NCC Joint Venture)

RE: Rental of Comacchio CM 602 SN 1987 · Value: \$348,000

Dear Tom,

I have attached a draft of a Rental Agreement for the above mentioned equipment. If you would like to go ahead with the rental, please fill in all the pertinent information on the rental form, fax or email and mail the contract. We would then mail the signed agreement back to you.

Please keep in mind that we would need a certificate of liability insurance in the amount of \$1,000,000, and an equipment floater in the amount of \$ 348,000 as stated on the attached agreement. The certificate must also show any excess umbrella coverage per occurrence that is on the policy. We must be named as the additionally insured as well as the loss payee. The name, insurance value and serial number of the rig must be shown as above on the insurance certificate.

If you have any questions, please feel free to contact me. Thank you very much for trusting us with your order, we appreciate the business and wish you success with your project.

Sincerely,

Darius

Darius Tata
Territory Manager
Cell: 707-758-8086



HAMMER & STEEL, INC.
 Steel & Piling Equipment
 401 West Channel Road
 Benicia, CA 94510
 1-877-224-3356
 707-746-7821 - Fax

EQUIPMENT RENTAL AGREEMENT

DATE: May 1, 2013
 PROJECT:
 LOCATION:
 OWNER:
 GENERAL CONTRACTOR:
 CONTRACT #:
 H & S SHIPMENT#

2134-CC

TO: Condon Johnson (CJA-NCC joint Venture)

ATTN: Tom Baddeley

FAX#

HAMMER & STEEL, INC., the LESSOR, agrees to rent the above captioned customer, the LESSEE, the equipment shown below. This equipment is to be used only for the project described above. Lessor and Lessee agree to all the terms and conditions set forth in this agreement in six pages.

ITEM	VALUE	RENTAL RATE
Cornacchio CM 602 SN 1987	\$348,000	\$18,000/month
Operator Training \$110.00/hour plus travel expenses		\$110.00/hour plus travel expenses
Transport Charge to Be Determined		TBD
Purchase Option Payment Plan Attached		
Sales tax, insurance and transport not included.		

Lessee agrees to rent the subject equipment for a minimum of _____
 Lessee agrees to pay transportation charges from, and back to Lessor's shipping point.

SHIP TO:

RETURN TO:

HAMMER & STEEL, INC.
401 WEST CHANNEL ROAD
BENICIA, CA 94510

If an Option to Purchase has been granted, such Option shall be an integral part of this agreement.

ADDITIONAL TERMS AND CONDITIONS

(The terms "Lease" and "Rental" shall be used interchangeably herein)

1. Rental Period. The rental period shall begin on and include the date of shipment to the Lessee and shall end on and include the date of return to Lessor's warehouse or receiving point. All rentals are subject to the minimum rental period provided for in this agreement. If the Lessee causes any damage to the equipment and Lessor notifies Lessee within ten days after receipt of equipment that damage has occurred, the rental period will not end on date returned but will continue until such damage has been repaired and paid in full by Lessee. If equipment is kept longer than the specified minimum rental period, the Lessee will then be involved for an additional rental period, with credit given on a pro-rate basis for equipment returned prior to the end of the additional rental period.

2. Determination of Rental Rates. Rental rates expressed in this agreement are based on a normal work day of (8) hours and a normal work week of (5) days. Monthly rates are based on (22) days in any one thirty consecutive day period. Usage that exceeds the standards as herein expressed shall be billed to Lessee at 1/40th of the weekly rate of 1/176th of the monthly rate for excess usage over the standard.

3. Non-Working Time. No allowances will be granted due to non-working time caused by factors such as bad weather, holidays, strikes or other delays in the job over which HAMMER & STEEL, INC. has no control. No allowances will be granted due to equipment down-time for routine parts replacement. No "back charges" for job delay or other damages shall apply against Lessor.

4. Payments. Rentals are payable "IN ADVANCE" within (10) days of receipt of Lessor's invoice. Lessee also agrees to pay actual and reasonable cost of collection on past due invoices to include attorney's fees and court costs. Payments should be mailed to Lessor's office at 11912 Missouri Bottom Rd, Hazelwood, MO 63042 or to the address shown on the Lessor's invoice. Lessee agrees that if lease charges are not paid after 30 days after receipt of invoice, interest will be charged at the Lessor of 18% per annum or the maximum rate permitted by law, from the due date until the payment received.

5. Title. Title to the equipment shall at all times vest in the Lessor. If Lessee takes any action that would in any way prejudice HAMMER & STEEL, INC.'S title to the equipment, including but not limited to subjecting the equipment to any lien, Lessee shall immediately be liable to Lessor and will pay Lessor for full value of the equipment.

6. Insurance. Lessee shall carry insurance to cover the full value of the equipment for all risk of loss or damage. In the event of loss or damage, Lessee shall report same to Lessor and immediately pay Lessor for such loss or damage. Lessee shall also maintain liability insurance and shall protect, indemnify and hold Lessor harmless against any claim for death or injury to person or property arising out of the possession, control or use of the equipment. Lessee shall provide appropriate certificates of insurance to Lessor, evidencing insurance coverage and with an insurance company acceptable to Lessor and naming Lessor as an additional insured.

7. Subletting. No equipment shall be rented or subjected by Lessee nor shall Lessee assign or transfer any interest in this Agreement without prior written consent of the Lessor, nor shall said equipment be used in any manner or at any location other than that specified herein.

8. Receipt and Acceptance. Lessee shall notify Lessor in writing within (5) days after delivery in the event the equipment is not in good, safe, serviceable condition. Lessor's sole responsibility shall be to repair or replace equipment within a reasonable time. Failure by Lessee to send proper notice shall constitute full acceptance of the equipment. Acceptance of the equipment by Lessee indicates full performance under this lease by Lessor.

9. Termination of Agreement. Either the Lessor or Lessee may terminate this agreement at any time, by written notice, for failure of the other party to comply with any of its terms and conditions. This agreement may also be terminated by either party upon one month's written notice following fulfillment of all obligations hereunder. Upon termination of this agreement, Lessor shall be entitled to immediate possession of the equipment and shall be permitted to enter Lessee's premises or other location to take possession of the equipment without legal action. Upon termination, Lessor shall also be entitled to recover all rental payments due from Lessee plus interest and attorney's fees. In the event of damage to the equipment by Lessee, Lessor shall be entitled to recover full compensation for such damage including attorney's fees and other legal costs including rental payments during the time it takes to restore equipment to useable condition. Lessee shall be responsible for the full cost of returning the equipment to Lessee in all events.

10. Maintenance and Operation. The Lessee shall not alter, disfigure or modify the equipment and shall see that the equipment is not subject to careless or needlessly rough usage or usage inconsistent with the normal and intended purpose of the equipment; and Lessee shall at Lessee's own expense maintain the equipment and its appurtenances in good repair and operative condition and return it in such condition to the Lessor. Upon return to Lessor, equipment will be inspected by Lessor and the Lessee will be liable for charges for repair necessitated by damage of excessive wear. Any accessory items not returned with equipment will be billed to Lessee.

11. Warranties. Lessee acknowledges that Lessee has selected the equipment leased hereunder based entirely and solely on Lessee's judgment and Lessee understands and agrees that any IMPLIED WARRANTIES OF MERCHANTABILITY OF FITNESS FOR A PARTICULAR PURPOSE ARE EXPRESSLY EXCLUDED. Lessor expressly warrants that the equipment leased hereunder will be in good working order on the date of delivery to Lessee and will conform to the manufacturer's published specifications, instructions and usage procedures.

12. Taxes, Fees, Permits, Licenses. Unless Lessee provides a "Tax Exemption Certificate" with this lease agreement, Lessee shall pay for any or all sales, use and other taxes, fees, permits, licenses, or any other similar type of charges of any kind that might apply or might be assessed now or later in connection with Lessee's use of this equipment.

13. Limitation of Remedies. Lessor's entire liability and Lessee's exclusive remedy is set forth in this section. In all situations involving performance or non performance of the equipment leased hereunder, the Lessee's remedy is (1) the adjustment or repair of the equipment or replacement of it by Lessor's option, or (2) if Lessor, after repeated efforts, is unable to restore the equipment to good working order or to replace it, all as warranted, the Lessee shall be entitled to recover actual damages set forth in this section.

Lessor's liability for damages to the Lessee for any cause whatsoever, except as otherwise stated in this section, and regardless of the form of action, whether in contract or tort including negligence shall be the total lease or rental charges for the specific piece of equipment that caused the damages or that are the subject matter of, or are directly related to, the cause of action. Such charges shall be those in effect for the specific piece of equipment when the cause of action arose.

No action, regardless of form, arising out of this agreement may be brought by either party more than two years after the cause of action has arisen or, in the case of an action of nonpayment, more than two years from the date last payment was due.

In no event will Lessor be liable for any damages caused by Lessee's failure to perform Lessee's responsibilities or for any lost profits, lost savings, incidents, damages or other consequential damages, even if Lessor has been advised of the possibility of such damages, or for any claim by Lessee based on any third party claim.

Upon the occurrence of any event of default, Lessor at its sole option, may exercise any one or more of the following remedies (a) declare all unpaid rent immediately due and payable with respect to any or all of the equipment, without notice or demand to Lessee; (b) sue for and recover all rent and payments then accrued or thereafter accruing, whether under this lease or under any other agreement between the parties executed in conjunction with this lease, with respect to any or all of the equipment and /or without notice or demand, repossesses any or all of the equipment, wherever the same may be located, without any court order or other process of law and without liability for any damages occasioned by such taking of possession (any such taking of possession not constituting a termination of this lease as to any or all of the equipment unless Lessor expressly so notifies Lessee in writing); (c) demand that Lessee return all of the equipment to Lessor; (d) sell or otherwise dispose of any or all of the equipment, whether or not in Lessor's possession, in a commercially reasonable manner at public or private sale and with or without notice to Lessee and apply the net proceeds of such disposition after deducting the estimated fair market value of the equipment plus all costs of such disposition, including, but not limited to costs of transportation, repossession, storage, refurbishing, advertising and broker's fees, to the obligations of Lessee hereunder with Lessee remaining liable for any deficiency and, except as otherwise provided by law with any excess being retained by Lessor; (e) retain any repossessed equipment and credit the reasonable value thereof first, to the estimated fair market value of such equipment as of the end of the rental period, plus all costs incurred in repossession, including but not limited to costs of transportation, refurbishing and reasonable attorney's fees, costs and expenses and then to the obligations of Lessee hereunder with Lessee remaining liable for any deficiency and, except as otherwise provided by law, with Lessor having no obligation to reimburse Lessee on account of any excess of such reasonable value over such obligations; (f) terminate this lease as to any or all of the equipment; or (g) utilize any other remedy available to Lessor at law or in equity.

No right or remedy conferred herein is exclusive of any other right or remedy conferred herein or by law; but all such remedies are cumulative of every other right or remedy conferred hereunder or at law or in equity, by such statute or otherwise, and may be exercised concurrently or separately from time to time.

14. Indemnity. To the fullest extent permitted by law, the Lessee shall indemnify and hold harmless HAMMER & STEEL, INC. and its agents and employees from and against all claims, damages, losses, and expenses, including but not limited to attorney's fees arising out of or resulting from the use or handling of the material and/or equipment sold or rented hereunder, provided that any such claim, damage, loss or expense (1) is attributable to bodily injury, sickness, diseases, or death, or to injury or to destruction of tangible property, including the loss of use resulting therefrom, and (2) is caused in whole or in part by any negligent act or omission of the Lessee, or anyone directly or indirectly employed by the Lessee or anyone for whose acts the Lessee may be liable, regardless of whether or not it is caused in part by a party indemnified hereunder. Such obligation shall not be construed to negate, abridge, or otherwise reduce any other right or obligation of indemnity which would otherwise exist as to any party or person described in this paragraph.

15. Lessee's Warranty. Lessee warrants that Lessee is familiar with and competent to use the equipment rented hereunder. Lessee represents that Lessee is not relying on the Lessor regarding proper use of equipment or installation or removal techniques required in the utilization of the equipment covered by this agreement.

16. Binding Effect/Entire Agreement/Choice of Law. This lease shall be binding upon and inure to the benefit of the parties, their successors, legal representatives, and assigns. This is the entire agreement between the parties. There are no other oral or written promises, terms, conditions, representations of quality of fitness for any purpose, or warranties express or implied concerning the equipment other than those contained herein. This agreement, and any disputes arising from or related to this agreement, will be governed by the laws of the State of California.

18. Litigation Venue/Choice of Forum. It is hereby agreed that the place of the making of this Rental Agreement shall be deemed to be Solano County, California and that any legal proceedings brought to determine the validity, construction, breach, interpretation or enforcement of the Rental Agreement shall be instituted and maintained in only the Circuit Court for Solano County, California, or, if Federal Court jurisdiction is applicable, any action may be brought in the United States District Court for the Eastern District of Missouri and not elsewhere.

19. Attorneys' Fees and Expenses. Lessee agrees to pay Lessor's reasonable attorneys' fees and expenses incurred by Lessor in enforcing this agreement or in collecting any amounts due from Lessee.

20. Interest. Customer/Lessee will pay HAMMER & STEEL, INC. interest, calculated at the lesser of 18% per annum, or the maximum rate permitted by law, from the date any amount is due until the date payment is received.

CUSTOMER:

Customer Name: CJA-NCC A JV.
 Cardea Johnson-Melton HAMMER & STEEL, INC.

Authorized Signature: Kozl Authorized Signature: _____

Print or Type Name: KORIN BIRZACK Print or Type Name: _____

Date: 5-3-13 Date: _____

Lessee is to sign and return to HAMMER & STEEL, INC. upon acceptance, HAMMER & STEEL, INC. will sign and return one signed copy to Lessee. NOTE: If tax exempt, exemption certificate must accompany signed Rental Agreement. See Paragraph 12.

Please complete the following and provide us with your company information:

Lessee: _____ Billing Address: _____

Project: _____

Phone #: _____ Fax #: _____

Accounts Payable Contact: _____ Direct Phone #: _____

PRELIMINARY INFORMATION

Purchase Order Number: _____ Job/Project Name: _____

Job Site Address _____

Job County: _____

OWNER: _____

Address: _____

Phone: _____ Fax: _____

LENDER: _____

Address: _____

Phone: _____ Fax: _____

CONTRACTOR: _____

Address: _____

Phone: _____ Fax: _____

SUB-CONTRATOR: _____

Address: _____

Phone: _____ Fax: _____

CJA- CONTRACT / C.O. REVIEW

JOB # 21134

Project Central Subway

P.M. T.B

OWNER _____ or G.C. _____ CONTRACT _____ SUBCONTRACTOR - Subconsultant

HAMMER & STEEL

Contract _____ (or) Change Order # _____

Subcontract _____ (or) Change Order # _____

PROJECT MANAGER

Approved "as is" for CJA signature and transmitting _____ Sign _____ Date _____

REVIS AS FOLLOWS: _____ Sign _____ Date _____

Purchase Order _____

Subconsultant _____

Equipment Rental Agreement

REVIEW PRIOR TRANSMITTING TO SUB

PM Approved _____ Sign _____ Date _____

KB TO REVIEW YES _____ NO Yes 5/3/12 Sign _____ Date _____

KB REVIEW

Approved as per Project Mgr. _____ Sign _____ Date _____

REVIS AS FOLLOWS: _____ Sign _____ Date _____

JC / JJ APPROVED _____ Sign _____ Date _____

Subcontract returned signed but revised

as follows:

(1) _____

(2) _____

(3) _____

(4) _____

JV NAME - YES

JC/JJ Review

_____ Sign _____ Date _____

Approved & Signed _____ Sign _____ Date _____

Comments and / or Revised as noted: _____

PM COMMENTS TO REVISIONS:

All Revisions Approved _____ Sign _____ Date _____

NOT ACCEPTED: _____ Sign _____ Date _____

Restore As Noted: _____

Fully executed contract returned to CJA

CJA revisions not accepted _____
Owner/GC revised contract _____

JC / JJ REVIEW

Comments: _____ Sign _____ Date _____

WHO IS TRANSPORTING ?

CJA -

ATTN: CERTIFICATE UNIT FAX: (415) 391-1882

CERTIFICATE OF INSURANCE REQUEST FORM

DATE: <u>5-3-13</u>	REQUESTED BY: <u>Linda Erickson</u>	PAGE 1 OF	<input type="checkbox"/> RUSH
INSURED: <u>CONDON-JOHNSON & ASSOCIATES, INC.</u>		<u>(CONDON 1)</u>	PHONE: 206-575-8248
<u>P.O. Box 12368, Oakland, CA 94604</u>			FAX: 206-575-8354

PROJECT SPECIFIC
 FOR BIDDING PURPOSES ONLY
 FOR LICENSES, PERMITS, ETC.
 REVISION/RENEWAL/RE-ISSUANCE - Please enter date of previous certificate:

CERTIFICATE HOLDER: Hammer & Steel, Inc.

ADDRESS: _____ ATTN: _____
 _____ FAX: _____

DESCRIPTION/NAME OF PROJECT/OPERATIONS/LOCATIONS/VEHICLES/PROPERTY/EQUIPMENT

Job #: 21134 Client's Job #: _____ Contract #: _____
 Residential Project Commercial Property OCIP/CCIP/Offsite Project Contract Amount: _____

CERTIFICATE OF LIABILITY INSURANCE

COPY OF INSURANCE REQUIREMENTS FROM THE CONTRACT/AGREEMENT ATTACHED

<input type="checkbox"/> GENERAL LIABILITY	<input type="checkbox"/> Additional Insured	<input type="checkbox"/> Primary Insurance	<input type="checkbox"/> Waiver of Subrogation
<input type="checkbox"/> AUTOMOBILE LIABILITY	<input type="checkbox"/> Additional Insured	<input type="checkbox"/> Waiver of Subrogation	<input type="checkbox"/> Loss Payee
<input type="checkbox"/> WORKERS' COMPENSATION	<input type="checkbox"/> Waiver of Subrogation		

Name(s) of Person(s) or Organization(s) as Additional Insured(s): _____

UMBRELLA/EXCESS LIABILITY Limit Required: _____ Million OTHER COVERAGE(S):
 NOTICE OF CANCELLATION: 30 _____ DAYS CROSS OUT "ENDEAVOR TO" and "BUT FAILURE TO..."

CERTIFICATE OF PROPERTY INSURANCE

COPY OF INSURANCE REQUIREMENTS FROM THE CONTRACT/AGREEMENT ATTACHED

<input type="checkbox"/> BUSINESS PERSONAL PROPERTY	<input type="checkbox"/> REAL PROPERTY/BUILDING(S)	<input type="checkbox"/> INSTALLATION FLOATER
CONTRACTOR'S EQUIPMENT: <input type="checkbox"/> Leased/Rented	<input type="checkbox"/> Scheduled/Owned	<input type="checkbox"/> BUILDER'S RISK
NOTICE OF CANCELLATION: 30 _____ DAYS		<input type="checkbox"/> CROSS OUT "ENDEAVOR TO" and "BUT FAILURE TO..."

SPECIAL INSTRUCTIONS

THIS IS A JV JOB



COMACCHIO

DRILLING HI-TECH

PERFORATRICE
IDRAULICA

HYDRAULIC
DRILL RIG

FOREUSE
HYDRAULIQUE

HYDRAULISCHE
BOHRMASCHINE

MC 602



MC 602

Perforatrice Idraulica Hydraulic drill rig



Perforatrice idraulica inglobata di taglia medio-piccola, estremamente versatile e potente, ideale per l'esecuzione di opere specializzate di ingegneria civile quali monopoli, ancoraggi e consolidamenti, in genere tramite l'impiego di tutti i sistemi di perforazione di tipo a rotazione e/o rotazione oscillante.

La **MC 602** rappresenta un'innovazione nel campo delle perforatrici idrauliche inglobate per piccole e medie perforazioni. Infatti, grazie allo speciale sistema scorrevole di cui è dotata, può essere utilizzata sia nella versione con centrale idraulica a bordo, che nella versione con centrale separata, al secondo degli aspetti di compattezza e delle emissioni in termini di emissioni e rumorosità. Le dimensioni compatte e la grande varietà di articolazioni di cui è dotata la rendono la soluzione ideale per l'esecuzione di lavori di fondazioni in spazi angusti e di difficile accesso, afflusso di aria e per i consolidamenti di gallerie di piccole dimensioni.

Lo speciale mast telescopico progettato da **COMACON** consente l'impiego di tutta la perforazione di diverse lunghezze evitando l'applicazione di prolunga, con conseguenti vantaggi in termini di tempi e costi.

Fruito della costante attenzione di **COMACON** per i temi della sicurezza e del suo impegno nell'affrontare le effettive necessità di cantiere, la **MC 602** nasce con un nuovo tipo di radiocomando one, oltre ai comandi tradizionali le funzioni di movimentazione dei bracci, gestisce il traino delle operazioni di posizionamento e perforazione, compresa la regolazione telescopica del mast e i sistemi di montaggio e smontaggio della centrale di potenza.



Small-medium size hydraulic crawler mounted drill rig, extremely versatile and powerful, suitable for specialized civil engineer works such as monopoles, anchors, ground consolidation by using all rotary and/or rotary percussive drilling systems.

The **MC 602** represents an innovation in the field of hydraulic crawler mounted drill rigs for small and medium drilling. Thanks to the special roll-off system included in basic equipment, the **MC 602** can be used both in the version with on-board power pack and in the version with separated power pack, depending on the job site conditions and exhaust gas and noise emission limits. The small dimensions and the great variety of articulations that characterize the machine make it the perfect solution for foundation works in narrow spaces and difficult access areas, inside buildings and for the consolidation of small diameter tunnels. The special telescopic mast designed by **COMACON** allows the use of drilling rods with different lengths, avoiding the application of mast extensions, with consequent advantages in reduced cost, time and effort.

As a result of the constant attention of **COMACON** for safety issues and of its commitment in meeting the actual jobsite needs, the **MC 602** is designed with a new type of radio control which manages synchronically not only the track movements, but also all the positioning and drilling operations, including the telescopic mast regulation and the power pack roll-off system.



MC 602

Foreuse Hydraulique Hydraulische Bohrmaschine



Foreuse hydraulique sur chenilles de petite moyenne-portée, extrêmement polyvalente et puissante, idéale pour l'exécution des travaux spéciaux de génie civil comme micropieux, ancrages et consolidations en tous terrains grâce à l'utilisation de tous les systèmes de perforation de type rotation et/ou rotation-percussion.

La **MC 602** représente une innovation dans le domaine des foreuses hydrauliques sur chenilles pour petits et moyens forages. En fait, grâce au système rotatif interchangeable dont elle est équipée, elle peut être utilisée aussi bien en version avec contrôle hydraulique embarqué, qu'en version avec centrale séparée, selon les tailles des chantiers et des forages en termes de dimension et de bruit. Les dimensions compactes et le grand encombrement sont elle est équipée à rendre la solution idéale pour l'exécution des travaux de forage en espace étroit, et difficile d'accès, à travers un pur solifère et pour la consolidation des gisements de petites dimensions.

Le mât télescopique spécialement étudié par **CONRADOM** permet l'utilisation de tiges de forage de longueurs différentes évitant le montage de rebords de mât, d'où des avantages considérables en termes de temps et de coûts. Pour une constante attention de **CONRADOM** dans le domaine de la sécurité et de son engagement pour affronter les réels besoins des chantiers, la **MC 602** est avec un nouveau type de radiocommande qui, en plus de contrôler électriquement les fonctions de déplacement des chenilles, grâce à la totale des opérations de positionnement et de perforation, inclut le réglage du télescopage du mât et le système de montage et démontage de la centrale de puissance.



Hydraulische Klein- bei Mittel-Flächenbohrgeräte, äußerst vielseitig und leistungsstark; idealisiert für Spezialtieflösungen wie Mikropfähle, Verankerungen und Bodenverbesserungen in allen erdigen mittel-schweren Dreh- und/oder Dreh-schlag-Verfahren.

Die **MC 602** stellt die Innovation im Bereich der hydraulischen Flächenbohrgeräte für Klein- und mittlere Bohrarbeiten dar. Dank ihrer speziellen Abtriebsweise (einzel- oder zweiphasig) kann die Maschine sowohl in der Version mit integrierter Zentrale als auch in der Version mit getrennter Hochdruck-Zentrale benutzt werden, was sich nach den Platzverhältnissen, Arbeitsbedingungen und den Abtrieb- und Leistungsanforderungen etc. Aufgrund der kompakten Abmessungen und der Vielseitigkeit der Bohrverfahren das Gerät ist die perfekte Lösung für den Einsatz für Gründungsarbeiten bei beschränkten Platzverhältnissen und an schwer zugänglichen Orten, in Kellerkellern und für kleine Tunnel. Das von **CONRADOM** speziell entwickelte teleskopierbare Mast ermöglicht den Einsatz von Bohrstäben unterschiedlicher Längen ohne das An- und Abbau von Mastverlängerungen mit der daraus resultierenden Zeit- und Kostensparnis.

Entsprechend den Anforderungen von **CONRADOM** in Bezug auf Sicherheit und die Praxis bedingten Baukastenprobleme, ist die **MC 602** mit einer neuen Funktionskonzeption ausgestattet. Mit dieser Funktionsausstattung werden sämtliche Funktionen bedient, das Gerät verfahren, gesteuert und Mast in die Bohrposition eingestellt und abgefahren. Auch das Absteigen (Aussteigen) des Hydrauliksystems wird mit der Fernbedienung ausgeführt.



Caratteristiche	Unità di misura	Quantità min./max.	Valori max.			
Rasoio angolo	Wheel base	Spaziamento	Rahwerk	1.800 mm	5,58 ft.	
Larghezza max.	Max width	Larghezza max.	Gesamtbreite	1.200 - 1.800 mm	4,26 - 5,91 ft.	
Larghezza ruota	Rad width	Larghezza ruota	Radsbreite	250 mm	9,84 in.	
Rendita max.	Max. clearing ability	Rendita max.	Breuchfähigkeit max.	38%		
Velocità max.	Max. speed	Velocità max.	Fahrtgeschwindigkeit	1 km/h	0,62 mph	
Caratteristiche motore	Motor power	Motor max. power	Hydraulikaggregate			
Motore diesel	Diesel engine	Motore diesel	Dieismotor	OBLTZ TCO 80 1 1 L04 W		
Potenza motore	Engi. power	Potenza motore	Antriebsleistung	71,6 kW (98,8 HP) / 9.500 rpm		
Pompa principale	Main pump	Pompa principale	Hydraulische Leistung	3 x 180 l/min	8 x 48,2 gal/min	
Serbatoio olio	Oil tank	Riservorio olio	Öltank	500 l	132 gal.	
Serbatoio gasolio	Fuel tank	Riservorio diesel	Dieseltank	90 l	24 gal.	
Altezza	Height	Altezza	Laufhöhe			
Dimensione	Feed stroke	Course utile	Vorschub	880 - 2.000 mm	3,12 - 6,57 ft.	
Lunghezza totale	Total length	Longueur totale	Gesamtlänge	8.880 - 9.050 mm	29,14 - 29,71 ft.	
Forza d'azione	Feed force	Pression	Vorschubkraft	6.000 daN	13,446 lbs	
Forza di tiro	Retract force	Traction	Rückzugkraft	8.000 daN	18,000 lbs	
Tensione nominale	Nominal voltage	Tensione nominale	Spannung	A	B	C
Voltaggio	Voltage	Voltaggio	Schleifspannung	2	3	3
Coppia max.	Max torque	Coppia max.	Drumtorque max.	780 daNm	690 daNm	6270 daNm
				6.200 lb-ft	5.882 lb-ft	47.480 lb-ft
Velocità	Max speed	Velocità max.	Drum speed max.	300 rpm	660 rpm	60 rpm
Rendimento	B.W.M. passage	Passaggio post-tourant	Seilrührführung	11%	11%	2%
Altezza	Height	Altezza	Laufhöhe und Seilrührführung			
Dimensione min. di presa	Min. handling	Tipe/diametro min.	Durchmesser min.	40 mm		1,6 in.
Dimensione max. di presa	Max. handling	Tipe/diametro max.	Durchmesser max.	220 mm		8,66 in.
Forza d'azione	Clamping force	Force de serrage	Klemmkraft	300 daN		67,200 lbs
Coppia avvolgimento	Wrapping torque	Couple de serrage	Seilrührführung	3.000 daNm		18,000 lb-ft
Altezza	Height	Altezza	Laufhöhe			
Trazione	Max. haul	Rendita di trazione	max. Zugkraft	1.000 daN		2,248 lb
Capacità fuso	Drum capacity	Capacité tambour	Beladung	88 m ³		12,7 ft ³
Velocità fuso	Feed speed	Velocità di trazione	Seilrührerleistung	90 m ³ /min		1,22 ft ³ /min
Dimensione fuso	Feed diameter	Diametro di uscita	Seilrührmesser	8 - 10		0,31 ft
Altezza	Height	Altezza	Laufhöhe und Seilrührer			
Lunghezza	Length	Lingueur	Laufe	8.880 mm		29,14 ft
Larghezza	Width	Largueur	Breite	1.200 mm		3,94 ft
Altezza	Height	Hauteur	Höhe	3.280 mm		10,76 ft
Peso	Weight	Peso	Netto	7.000 - 9.400 kg		15.400 - 20.700 lbs



	<i>meeting.</i>	
2- 05/08/13	<p>1252 – M. Benson requested approval for negotiation position for COR79 Compensation Grout – Old Navy additional cost associated with inefficiencies of installing the compensation grout tubes. Justification was based on the premise that the larger piece of equipment could have performed the work if the in place utilities had been found as shown in the Bid documents. Because of the actual configuration of underground utilities was much different than that shown on the Bid documents, payment is warranted for the inefficiencies caused by utilizing the smaller piece of equipment. The CMB raised several questions about the graphical depiction of the As Bid condition and requested that the graphical representation be properly updated to show the As Bid condition. Similarly, the actual condition graphic could be improved to better show actual conditions found. Detailed information demonstrating the cost of the inefficiency appear in order, however the requested NTE amount was not properly justified compared to the computed cost by the Engineer. The CMB was unable to provide approval due to the still open question that the utility obstructions potentially indicate the Contractor should have brought in a smaller rig to begin with. CMB suggested improved graphics and supporting information be developed to justify the requested negotiating position. This item will be brought back to the CMB next week. 05/15 A revised graphic was shown depicting the actual condition shown found incorporating the second PG&E 12” electrical line. Further determination from the BP dwgs indicates the Contractor could have actually accomplished the work by using the original larger Klemm machine as listed in the bid proposal. AGREE – CMB 0117 Not to exceed value of \$152,000 to be negotiated as a lump sum for inefficiencies associated with having to use a smaller drill rig to work around the existing utilities. In addition the CM Team will obtain the invoice from the Contractor to identify the actual cost for the rental of the smaller Comacchio drill rig. Status: 05/29 Documentation was presented to showing the monthly rental rate for the Comacchio rig. The CMB took exception to the documentation presented stating the information describes an Equipment agreement value and not a monthly rental invoice. The CMB requested the RE obtain from</p>	ML 06/05/13



ITEM #	DISCUSSION	ACTION BY DUE DATE
	<i>the Contractor an actual invoice which can be compared to the cost estimate as validation of the rental rate. This item will be brought back to the CMB at a later date.</i>	

CENTRAL SUBWAY PROJECT
Project Trend/Change Control Log

Trend Log

Current Cost Estimate and Allocated Contingencies									
Item #	Date Initiated	CMB No.	Change Description	Change Type	Change Status		Projected Cost Impact +Exposure/(Benefit)	Comments	Contract Modification Number
					Approval Action	Approval Date			
CONSTRUCTION TRENDS									
Contract 1250 - Utilities Relocation #1 (Portal & MOS)									
1250-0001	04/15/10	081-0001	Delete Option MF-OPT1 "Fire Protection Sprinkler System"	1	N/A		(50,000)	Cleared City Attorney's Office; to Shahnam for execution. [In "Current Forecast" - Sep2010 Cost Report.] COMPLETE, NO FURTHER ACTION; CMOD #1	CMOD #1 Approved
1250-0002	05/07/10	081-0002	Changes to Sewer on 4th between Howard and Folsom	2	CMB-0001	Agree	07/12/10	+107,174 Differing conditions as AWSS is directly over existing sewer; unable to install sewer manhole per plan. [In "Current Forecast" - Sep2010 Cost Report.] No reimbursement from SFPUC Sewer. COR #2-\$34,135; COR #5-\$73,039 COMPLETE, NO FURTHER ACTION; CMOD #2	CMOD #2 Approved
1250-0003	05/28/10	081-0003	Quantity Adjustment for JT-6 and JT-7	4	CMB-0002	Agree	08/04/10	+192,420 In April 2010 pay app, JT-6 bid qty exceeded by 344% (230 vs. 1021) and JT-7 bid qty exceeded by 112% (500 vs. 1060) [In "Current Forecast" - Sep2010 Cost Report.] COMPLETE, NO FURTHER ACTION; CMOD #3	CMOD #3 Approved
1250-0004	07/02/10	081-0004	Demolition of existing brick and concrete footing on 4th between Howard and Folsom	2	CMB-0003	Agree	07/12/10	+170,000 Unforeseen bricks and concrete footing discovered on east side of 4th Street between Folsom and Howard. [In "Current Forecast" - Sep2010 Cost Report.] COR #3-RFI #76 COMPLETE, NO FURTHER ACTION; CMOD #4	CMOD #4 Approved
1250-0005	04/20/10	081-0005	Modify AWSS at 4th/Bryant and 4th/Harrison	3	CMB-0004	Agree	08/04/10	+586,000 At 4th/Bryant, AWSS conflict with new 48" sewer and AT&T duct bank. At 4th/Harrison, AWSS conflict with 18" sewer. [In "Current Forecast" - Sep2010 Cost Report.] No reimbursement from SFPUC AWSS. PCC #2: RFI #s 34, 49 & 51 COMPLETE, NO FURTHER ACTION; CMOD #5	CMOD #5 Approved
1250-0006	04/29/10	081-0006	Install four additional piles and reinforce existing foundation at 401 4th Street	2	CMB-0005	Agree	08/25/10	+130,000 Existing foundation was discovered to be part brick and part concrete, which is different from plan. Also, foundation was unstable and required additional reinforcement. Refer to RFIs #85R1, 88.1, 100, 101, 102 COMPLETE, NO FURTHER ACTION; CMOD #6	CMOD #6 Approved
1250-0007	10/06/10	081-0007	Additional work to install 48" sewer due to various utility conflicts at 4th/Bryant	2	N/A		+32,964 48" RCP sewer in conflict with existing AT&T duct bank that needs to remain in service until new joint trench is installed to enable switchover. This conflict forced contractor into a more expensive means to install sewer. Also, 48" RCP sewer in conflict with existing 30" force main and 24" steel pipes. No reimbursement from SFPUC Sewer. COMPLETE, NO FURTHER ACTION; CMOD #9	CMOD #9 Approved	
1250-0008	10/06/10	081-0008	Relocate TPC vault on 4th Street between Howard and Folsom	6	N/A		+19,500 Contractor uncovered existing TPC conduits on top of AT&T duct bank on 4th near Howard, a differing site condition. An AT&T intercept vault is to be installed, however, TPC conduits can not reside inside AT&T intercept vault. Joint Trench utilities participation cost TBD. Executed on 10/14/2010. Refer to RFI #62 (\$19,500) Expected Reimbursement from TPC. RE has provided documentation of notification to TPC, July-August '10. See final 1250 Form B actual costs COMPLETE, NO FURTHER ACTION; CMOD #8	CMOD #8 Approved	

- 1 - Owner Directed Change in Scope
- 2 - Unforeseen Conditions
- 3 - Errors and Omissions
- 4 - Quantity Variation
- 5 - Value Engineering
- 6 - Private Utilities
- 7 - Other

CENTRAL SUBWAY PROJECT
Project Trend/Change Control Log

Item #	Date Initiated	Change Description	Change Type	CMB No.	Change Status		Projected Cost Impact +Exposure/(Benefit)	Comments	Contract Modification Number
					Approval Action	Approval Date			
1250-0009	10/06/10	081-0009 Install additional sewer and provide temporary connections at 4th/Stillman	3	N/A			+47,000	Location of existing sewer to be intercepted differs from where it's shown in the plan, hence additional sewer to be installed. Also, due to optional sewer MH not buildable until (E) AT&T DB is removed, additional sewer is necessary to tie into (E) main as interim. New sewer is supposed to tie into optional MH. No reimbursement from SFPUC Sewer. RFI #91 COMPLETE, NO FURTHER ACTION; CMOD #7	CMOD #7 Approved
1250-0010	10/06/10	081-0010 Perform AT&T wye cast connection and chipping of existing duct bank at various locations	6	N/A			+48,181	AT&T was supposed to tie-in its own facility. However, AT&T's contractor, who is hired to install vaults, stated it is not in their scope of work. Refer to RFI #82.1. AT&T intercept vault at 4th/Harrison can not be installed at design location due to utility conflicts. AT&T vault to be shifted a few feet instead of vault being on top of existing duct bank. This will cause additional excavation, joint trench and modification to existing duct bank. Joint Trench utilities participation cost TBD. SFMTA and AT&T are negotiating with Contractor. (\$48,181) Expected Reimbursement from AT&T. RE has provided documentation of notification to AT&T, Sept-Oct '10. See final 1250 Form B actual costs. COMPLETE, NO FURTHER ACTION; CMOD #10	CMOD #10 Approved
1250-0011	10/06/10	081-0011 Remove existing piles and shoring at 801 Howard	2				-0-	This change order has been incorporated in CMOD #16 (Trend #30) COMPLETE, NO FURTHER ACTION; CMOD #16	CMOD #16 Approved
1250-0012	10/06/10	081-0012 Additional work related to AT&T facilities due to unforeseen conditions.	6	CMB-0047	Agree	08/03/11	-0-	This is the total of all force account work related to AT&T facility. SFMTA to seek reimbursement from AT&T through the Form B process. (\$67,798) Expected Reimbursement from AT&T. Cost captured in Trend #15 COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
1250-0013	10/06/10	081-0013 Additional work related to PG&E facilities due to unforeseen conditions.	6	CMB-0047	Agree	08/03/11	-0-	This is the total of all force account work related to PG&E facility. SFMTA to seek reimbursement from PG&E through the Form B process. (\$30,547) Expected Reimbursement from AT&T. Cost captured in Trend #15 COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
1250-0014	10/06/10	081-0014 Remove brick wall at sewer sta 152+94 (west side) on 4th between Howard and Folsom.	2				-0-	This change order has been incorporated in CMOD #16 (Trend #30) COMPLETE, NO FURTHER ACTION; CMOD #16	CMOD #16 Approved
1250-0015	10/06/10	081-0015 Archaeological Findings - Charges for rental of steel plates, triton barriers, shoring and labor for maintenance.	2				+784,771	Midden deposits were discovered on 4th Street between Howard and Folsom. Contractor stopped work. Contractor requesting payment for rental charges of trench plates, barricades, and shoring, and cost of maintenance. Total cost exposure is projected through end of November 2010. The \$290,703.00 is expected to be the final estimate for the rental of trench plates, barriers and shoring. \$597,000 Arch, \$179,000 DIFF, \$8,500 Traffic COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20
1250-0016	10/06/10	081-0016 Additional AWSS modification at 4th/Harrison	1	CMB-0016	Agree	11/10/10	+156,418	New alignment is proposed by Design team. No reimbursement from SFPUC AWSS. COR #33 Supplemental COMPLETE, NO FURTHER ACTION; CMOD #12	CMOD #12 Approved
1250-0017	10/06/10	081-0017 Protection Work at PG&E Vault 1611	6				-0-	Expected Reimbursement from PG&E or work to be deferred \$6,400. RE has notified PG&E via email dated 10/29/10 that SFMTA expects full reimbursement for this work if implemented. Follow-up pending. Work was not performed and will need to be addressed in a future contract.	
1250-0018	10/06/10	081-0018 Demolition and Support Work at PG&E Vault 1611	6				-0-	Expected Reimbursement from PG&E or work to be deferred \$25,000. RE has notified PG&E via email dated 10/29/10 that SFMTA expects full reimbursement for this work if implemented. Follow-up pending. Work was not performed and will need to be addressed in a future contract.	
1250-0019	10/06/10	081-0019 Extended overhead delay claim due to Archaeological discoveries	2					Old Note: Potential delay claim if issue extends project duration. Amount is estimated at \$3,000.00 per day for 60 days (\$180K). New Note: By exercising all option work, additional 84 calendar days is added to the contract time. In addition, midden mitigation will start on 10/7/10. As a result, the potential for extended overhead cost is averted. This trend has been superseded by Trend #37. COMPLETE, NO FURTHER ACTION	

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1250-0020	10/06/10	081-0020	Sewer modification under I-80 freeway at 2 locations	3			+39,062	Inverts of existing sewer where new sewer will tie into are found to be different than shown in plan and would create a reverse slope. Additional pipe and manhole are required to fix slope. Refer to RFI Nos. 2S01 and 147. No reimbursement from SFPUC Sewer. COMPLETE, NO FURTHER ACTION; CMOD #11	CMOD #11 Approved	
1250-0021	10/06/10	081-0021	Install new AWSS lateral including hydrant at NW 4th/Harrison	2				Existing AWSS lateral at SW 4th/Harrison is in conflict with new 18" sewer. Instead of modifying the lateral to avoid the sewer, a new lateral at NW corner is a preferred solution. CM awaiting design revision from Design team. Trend No. 21 is combined with Trend No. 22.		
1250-0022	10/06/10	081-0022	Install additional 30 feet of DIP and new AWSS at 4th/Harrison	2	CMB-0026	Agree	01/12/11	+160,908	AWSS Point of connection on 4th Street was found to be cast iron instead of ductile iron as shown in DPW as-built. Due to this differing condition, additional length of pipe needs to be replaced and lead joint to be installed in order to install a ductile iron tee. Also, existing AWSS hydrant lateral is in conflict with new 18" sewer. The preferred solution was to install a new ductile iron lateral at a different location rather than installing vertical offsets on an existing cast iron lateral. No reimbursement from SFPUC AWSS. COMPLETE, NO FURTHER ACTION; CMOD #15	CMOD #15 Approved
1250-0023	10/06/10	081-0023	Steel plates and shoring standby for 2 months due to PG&E delay at Clementina	3				+24,981	Sewer at Clementina was delayed by 2 months due to PG&E vault conflict. PG&E will not pay cost of steel plates and shoring standby because PG&E drawings were sent to MTA during design phase and PG&E was not made aware of the conflict then. COMPLETE, NO FURTHER ACTION; CMOD #13	CMOD #13 Approved
1250-0024	10/06/10	081-0024	Utility support and work around for AT&T facilities	6	CMB-0048	Agree	08/24/11	+95,311	Contract specs specified AT&T to compensate contractor directly for workaround & support. AT&T refused to pay Synergy. Synergy considers AT&T's refusal as a changed condition to CN 1250 contract. (\$95,311) Reimbursement from AT&T. See Final 1250 Form B actual costs. COMPLETE, NO FURTHER ACTION; CMOD #19	CMOD #19 Approved
1250-0025	10/06/10	081-0025	Utility support and work around for PG&E facilities	6					Contract specs specified PG&E to compensate contractor directly for workaround & support. PG&E negotiated \$100,000 directly with Synergy and will pay synergy directly as well.	
1250-0026	10/06/10	081-0026	Utility support and work around for SFWD facilities	3	CMB-0028	Agree	02/09/11	+66,510	Total cost known to date (10/5/10) of all SFWD support and workaround. See COR #12, 20, 32 and 35. Agreed amount is total of COR #12, 20 and 32. Total exposure is equal to agreed amount plus COR #35. No reimbursement from SFWD. COMPLETE, NO FURTHER ACTION; CMOD #14	CMOD #20 Approved
1250-0027	10/06/10	081-0027	Trucking and labor support for Archaeological mitigation	2				-0-	Synergy is asked to provide trucking for delivery of midden soil to Sonoma State University lab. Other support work includes traffic control, plating, loading and unloading of midden soil. The amount shown are charges up to end of November 2010 only. Additional charges are expected which will be tracked as Trend No. 28. COMPLETE, NO FURTHER ACTION	CMOD #20 Approved
1250-0028	12/08/10	081-0028	Trucking and labor support for Archaeological mitigation, Part II	2				-0-	Synergy is asked to provide trucking for delivery of midden soil to Sonoma State University lab. Other support work includes traffic control, plating, loading and unloading of midden soil. The amount shown are expected charges beyond November 2010. COMPLETE, NO FURTHER ACTION	CMOD #14 Approved
1250-0029	12/08/10	081-0029	Cost overrun for bid item UD-10 "Additional excavation and backfill"	4	CMB-0036	Agree	05/04/11	+112,500	Bid item UD-10 is to pay for excavation of connection hole and kill hole for SFWD line. Contract only included 200 cubic yard for this work. The size of excavations is dictated by field conditions and per the direction of SFWD inspector. RE's estimate of the total volume of dirt to be excavated is 630 cy. Examples of differing conditions encountered are: location of existing water line is different than where it's shown in the plan, existing waterline where shown in the plan to be connected to is inactive, differing condition at 5th/Clementina, connection holes and kill holes need to be expanded due to other utilities in the way. COMPLETE, NO FURTHER ACTION; CMOD #17	CMOD #17 Approved
1250-0030	12/08/10	081-0030	Supplement bid item GE-4 "Allowance for differing site conditions" related to the installation of publicly owned facilities only (sewer, water, AWSS, Joint trench)	4	CMB-0030	Agree	04/13/11	+235,595	This change order is to supplement GE-4 for cost overrun related to the installation of publicly owned infrastructure only. GE-4 cost overrun for installation of privately owned infrastructure is being tracked separately and a separate change order will be issued. COMPLETE, NO FURTHER ACTION; CMOD #16	CMOD #16 Approved

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1250-0031	12/08/10	081-0031 Unused allowance for bid item GE-8 "Allowance for cast-in-place utility vaults"	4				-0-	Bid item GE-8 is an allowance to pay contractor for designing and constructing cast-in-place manholes as required for installation of new systems. Only one CIP vault was installed. Credit amount included in Trend #43	
1250-0032	12/08/10	081-0032 Unused allowance for bid item UD-5 "Handling and disposal of Class I Hazardous Waste"	4				-0-	Bid item UD-5 is an allowance to pay contractor for handling and disposal of Class I Hazardous Waste. No class I hazardous waste was off hauled away from the job site. Class I soil found under the I-80 freeway was put back in the trench as backfill. This bid item is expected to remain unused. However, contractor is claiming to recover "General Conditions" cost. Credit amount included in Trend #43	
1250-0033	12/08/10	081-0033 Unused allowance for bid item UD-6 "Transportation of Class I Hazardous Waste"	4				-0-	Bid item UD-6 is an allowance to pay contractor for transporting of Class I Hazardous Waste. No class I hazardous waste was off hauled away from the job site. Class I soil found under the I-80 freeway was put back in the trench as backfill. This bid item is expected to remain unused. However, contractor is claiming to recover "General Conditions" cost. Credit amount included in Trend #43	
1250-0034	12/08/10	081-0034 Cost overrun for bid item TR-6 "Allowance for manual traffic control"	4	CMB-0037	Agree	05/04/11	+68,820	Bid item TR-6 is an allowance to pay for off-duty police officers and parking control officers. Contract allowance amount is \$50,000.00. Contract specs calls for police officers at 4 intersections (4th/Howard, 4th/Folsom, 4th/Harrison and 4th/Bryant). The average cost of 1 police officer is about \$800.00 per day (\$100.00 per hour). The \$50,000.00 allowance is only good for 60 days for 1 police officer. Hence, additional money was needed for traffic control support. Final amount paid police officers is \$87,500.00. Additional billings for parking control officers (from Al Herce of DPT) is forthcoming; RE estimated this bill to be about \$60,000.00. COMPLETE, NO FURTHER ACTION; CMOD #18	CMOD #18 Approved
1250-0035	12/08/10	081-0035 Premium cost for requesting contractor to accelerate work on 4th Street between Howard and Folsom	2				-0-	Due to archaeological discoveries, many trenches remained open and the project would not have enough time to complete installation of underground utilities and restore the roadway before the Holiday season. Without accelerating the work, the project would have had to pay rentals for the barriers, street plates and shoring for the unfinished trenches that would have remained. By accelerating the work and DPT allowing the contractor to work through the beginning of the Holiday Moratorium, the project avoided these rental costs, which is estimated to be greater than the premium cost of accelerating the work. COR Not submitted by Contractor CLOSED COMPLETE, NO FURTHER ACTION	
1250-0036	12/08/10	081-0036 Rental/Maintenance costs for support of Archeological Trenches (Part 1 of 4) \$255,840; Support Work During Archeologist Mitigation Efforts (Part 2 of 4) \$199,741; Mobilization and Equipment Standby Costs (Part 3 of 4) \$67,728; Additional Overhead/Indirect Costs During Archeological Delay Period (Part 4 of 4) \$124,186	2	CMB-0041	Agree	07/13/11 02/15/12 07/25/12 09/12/12	-0-	Due to archaeological discoveries, contractor may potentially claim for compensation for his equipment that were idle for the duration of the waiting period. RE's estimate is based on Caltrans rate for equipment standby for 5 months (May to October). RE has not received any change order request related to this item. RE to verify if his equipment were indeed idle during this time period. 2/15: Rental/Maintenance Costs for Support (Part 2 of 4); Add Work as Result of Archeological Shutdown (3 of 4). 07/25: Revised mod - compensation for 1 of 3) Rental/Maintenance, 2 of 3) Addl Work result of Archeological support, 3 of 3) Addl OH/Indirect cost. RE to add Equipment Standby time to revised mod. 09/12: RE to Prepare Modification 10/29: CMod 20 is being issued as a unilaterally change, by the SFMTA that represents the SFMTA's estimate of a fair and reasonable final compensation amount for the additional work. COMPLETE, NO FURTHER ACTION	CMOD #20 Approved
1250-0037	12/08/10	081-0037 Compensation for loss of production, inefficiency and disruption of work due to archaeological discovery	2				-0-	Due to archaeological discoveries, contractor may potentially claim for compensation for loss of production, inefficiency and disruption of work. RE has not received any change order request related to this item. Amount shown is a ROM cost by the RE. RE expects a COR from the contractor but change justification is very unlikely. Initial draft COR was \$898,453. RE has adjusted to \$661,559 removing the month of October and option work. This was then revised to 800,000. There has been no official submittal from Synergy. SFMTA has directed Synergy to submit anything related to the Architectural delays through the claims process. This will be carried in the Potential Claim Log. COMPLETE, NO FURTHER ACTION	

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1250-0038	03/18/11	081-0038	Project Delay due to archaeological discovery and PG&E Issues.	2				-0-	Due to archaeological discoveries compounded with PG&E's ability to perform service switchover to Olivet University and de-energize a live duct bank in conflict with 48" sewer. The project will realize an estimated 6 weeks of non-compensable delay to the substantial completion date of 4/6/11. 47 days estimated by RE. Contractor submitted COR 51 in the amount of \$1,144,776.74 on September 1st, 2011. RE responded requesting additional backup on October 19th, 2011. Subsequently, no additional information has been provided by the contractor, therefore, SFMTA has directed Synergy to submit anything related to the Architectural delays through the claims process. This will be carried in the Potential Claim Log.	
1250-0039	05/18/11	081-0039	Remove BP-4 (Waterproofing) from Contract	4				-0-	Bid item was not used. Contractor requesting compensation to recover for General Conditions cost. Bid item amount total is \$40,000. NO FURTHER ACTION; Superseded by COR #58. (See 1250-0043 below)	N/A
1250-0040	05/18/11	081-0040	Delete SW-8 (24" Sewer) from Contract	4				-0-	Bid item was not used. Contractor requesting compensation to recover for General Conditions cost. Bid item amount total is \$73,117. NO FURTHER ACTION; Superseded by COR #58. (See 1250-0043 below)	N/A
1250-0041	05/18/11	081-0041	Delete UD-5 and UD-6 (Class I Off haul and Handling) from Contract	4				-0-	Bid items were not used. Contractor requesting compensation to recover for General Conditions cost. Bid items total amount is \$150,000. NO FURTHER ACTION; Superseded by COR #58. (See 1250-0043 below)	N/A
1250-0042	05/25/11	081-0042	Delete GE-6 (Hazmat) from Contract	4				-0-	Bid item was not used. Contractor requesting compensation to recover for General Conditions cost. Bid item amount total is \$100,000. NO FURTHER ACTION; Superseded by COR #58. (See 1250-0043 below)	N/A
1250-9001	10/06/10	081-9001	Utility Companies refusal to contribute to utility support costs and work-around. (Voided. Recorded individually in TR-0059-TR-0061.)	7						VOIDED
1250-0043	08/03/11	081-0043	Unit Rate Adjustment for Entire 1250 Bid Items	4				(333,904)	Pursuant to General Provision GP-11, COR is to adjust contract value to account for fix overhead costs for bid items not performed or where the bid item performed varied by 25%. COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
Contract 1250 Totals								+2,694,211	Total Changes to the Contract in the amount of \$2,694,211, represents a \$740,834 OVER RUN of the original allocated contingency of \$1,953,377. Changes that will be Utility Reimbursement credits through the Form B process in the amount of \$162,992 will offset some of this OVER RUN. Final Contract Closeout Total \$11,968,150 will be reduce by \$2,275,419 Utility Reimbursement credits for a Final Program cost of \$9,692,731 for CN 1250	
Contract 1251 - Utilities Relocation #2 (UMS)										
1251-0001	03/18/11	082-0001	Modify traffic control plan by establishing a single travel lane on Stockton St. in lieu of time-based multiple lane requirements as stipulated in contract plans TR-001 to 004.	7				-0-	After discussions with the Contractor and the Union Square Business Improvement District (BID), the SFMTA determined that modifying the lane requirements along Stockton St. and implementing the alternate pedestrian walkway design would be beneficial to the execution of CN 1251 in the following ways: increased public safety, increase construction production, and create a more pleasing environment to the public and merchant. COR #1 COMPLETE, NO FURTHER ACTION; CMOD #4	CMOD #4 Approved

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1251-0002	03/18/11	082-0002	1&3	CMB-0035	Agree	04/20/11	+67,634	Pursuant to a commitment between Barney's and SFMTA, Emergency Stair #4 (to be constructed in CN 1253) must avoid existing subsidewalk basement. It was determined that moving the conflicting stairs would require the Joint Trench to be constructed in a manner that would enable it to span the excavated area. The addition of the invert slab allowed for the needed span thereby allowing for a relocation of Emergency Stair #4. COR #4; PCC #1 COMPLETE, NO FURTHER ACTION; CMOD #2	CMOD #2 Approved
1251-0003	03/18/11	082-0003	1	CMB-0029	Agree	02/23/11	+148,919	During future station construction, the design team anticipates that excavation related settlement may cause damage to the brick sewers along Geary and O'Farrell Streets. This proposed modification will replace the vulnerable brick sewer within the influence zone with HDPE pipe that will not be as susceptible to settlement-induced damage. COR #9, PCC #2 COMPLETE, NO FURTHER ACTION; CMOD #1	CMOD #1 Approved
1251-0004	04/06/11	082-0004	2	CMB-0031	Agree	04/13/11	+189,584	Due to various differing site conditions as described in RFIs 15, 37, 38 and 44, changes to the secondary enclosure wall are required. The existing footing were found to be inconsistent, i.e. missing footing in one area, footing sticks out beyond existing wall, overpour on existing footing, existing wall to be 6" into private property. Contract plan SR-302 shows a consistent footing. COR #8 COMPLETE, NO FURTHER ACTION; CMOD #3	CMOD #3 Approved
1251-0005	04/06/11	082-0005	6				+66,084	Contract plans AW-501.1, 501.2, 501.4 and 501.4 show the existing AWSS to be replaced in place. However, existing PG&E conduits were found to be on top and parallel to the existing AWSS resulting in extra work in working around and/or shifting the existing electrical conduits. Contract plans JT-308 and 309 showed the existing electrical and AWSS to be separate. Estimate includes \$77,892.53 (Synergy FA Request) + \$25,000 (first digging west side) + \$10,000 (temp backfill during moratorium) + \$15,000 (plate rental west side at \$5k per month). Estimate does not include Synergy's cost to sequence his work. Updated projected cost impact from \$50,000 to \$127,893 based on compilation of force accounts received. RE negotiating COMPLETE, NO FURTHER ACTION	CMOD #24 Approved
1251-0006	04/20/11	082-0006	1	CMB-0039	Agree	06/08/11	+398,624	Due to changes to the limits of the UMS Station on Stockton Street between Post and Geary, the original utility design has changed. COR #24, PCC #4 COMPLETE, NO FURTHER ACTION; CMOD #5	CMOD #5 Approved
1251-0007	04/20/11	082-0007	1	CMB-0049	Agree	08/24/11	(434,957)	Due to changes to the limits of the UMS Station on Ellis street, the original utility design has changed. PCC #5 COMPLETE, NO FURTHER ACTION; CMOD #5	CMOD #5 Approved
1251-0008	04/20/11	082-0008	1					Based on field verification, it was determined that underpinning is no longer needed. Contract bid item SR-9 is for a total amount of \$116,000 measured by LF. However, CM team does not anticipate to recover the full amount because contractor may be entitled to retain general conditions cost and some excavation cost that was already performed but encumbered in Bid Item SR-9. Agreed credit of (\$58,000) to be included in Trend #60. CLOSED, NO FURTHER ACTION	
1251-0009	04/20/11	082-0009	4	CMB-0038	Agree	06/01/11	+261,584	Contract bid item TR-7 for manual traffic control in the amount of \$120,000 is insufficient to control traffic for the duration of the contract. The \$120,000 allowance only equates to one police officer for a duration of 7.5 months. At a minimum, it is anticipated that one police officer or PCO will be needed at Stockton/Post for the entire contract duration of 16 months. Additionally, one PCO is required for each intersection at Geary/Stockton and O'Farrell/Stockton. COMPLETE, NO FURTHER ACTION; CMOD #4	CMOD #4 Approved
1251-0010	05/25/11	082-0010	1				+27,409	Contractor needed to rent additional excavators to work concurrently with the other block on Stockton between O'Farrell and Market. Amount: \$27,409; COR #25 COMPLETE, NO FURTHER ACTION; CMOD #3	CMOD #3 Approved

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1251-0011	06/22/11	082-0011	Potholing at 4th/Howard for PUC sewer design	1				Request from MOS utility design team to pothole for the intercept of a 36" force main sewer and 96" sewer. PUC requested that this be done in order to ensure that we have accurate utility information to avoid costly change orders during construction. Per direction from CMB on January 18, 2012, this request is denied. COR #50, PCC #6 CLOSED, NO FURTHER ACTION: Per direction from CMB on January 18, 2012, this request is denied.		
1251-0012	07/06/11	082-0012	Additional cost to lower the AWSS at O'Farrell, east of Stockton, due to numerous existing utility conflicts.	2	CMB-0042, CMB-0050	Agree	8/3/2011, 9/21/2011	+278,351	The AWSS is in conflict with AT&T, PG&E and several other utilities and needs to be installed under all these utilities at about 11' deep (instead of 6' depth as noted in Plan AW-501.4, sheet note #5). During negotiations with the Contractor several items of work were found to be missing for the original cost estimates which increase the amount to \$278,351. RFI #76, COR #31 COMPLETE, NO FURTHER ACTION; CMOD #6	CMOD #6 Approved
1251-0013	07/06/11	082-0013	Accelerate start of construction in front of Macys West by trenchless construction method	7				-0-	anticipated. To mitigate this potential delay, contractor requested to start construction in front of Macys West. COR 32 (CMB 0055 approval - see also Trend CN1251 #'s 04 & 10) COMPLETE, NO FURTHER ACTION; CMOD #3 (No Cost Change)	CMOD #3 Approved
1251-0014	07/21/11	082-0014	Increase in Bid Item WD-10, Allowance for Additional Excavation and Backfill.	3	CMB-0055	Agree	09/14/11	+191,779	This allowance is for the contractor to perform additional excavation, backfilling and restoration associated with the water tie-ins and capping for the distribution piping system. The contractor has performed two water service tie-ins at an average cost of \$14,000 per tie-in. The current number of tie-ins and capping is 35 separate points. This amounts to a total cost of \$490,000. The current contract allowance amount is \$100,000, therefore an additional \$390,000 will be needed to augment is bid item. CMB made a partial approve of up to \$200K. The initial Contract Modification to supplement this allowance is in the amount of \$191,778.62. RE's cost analysis of this trend confirms that the initial CMod amount of \$191,778.62 is sufficient to cover for all water tie-in/capping work. COMPLETE, NO FURTHER ACTION; CMOD #9	CMOD #9 Approved
1251-0015	08/03/11	082-0015	Additional traffic signal work at Post/Stockton	1				+17,530	Modification of the traffic signal at Post/Stockton is required to implement the detour shown in contract plan TR-010. There are no bid item to bill this work. COR #19 COMPLETE, NO FURTHER ACTION; CMOD #8	CMOD #8 Approved
1251-0016	08/03/11	082-0016	Additional traffic signal work at Sutter/Mason	2				+32,054	Existing conduits shown in Plan ET-101 differs from what is actually in the field. City's response to RFI 73 directs contractor to install new conduits and conductors. RFI #73; COR #33 COMPLETE, NO FURTHER ACTION; CMOD #8	CMOD #8 Approved
1251-0017	08/03/11	082-0017	Furnish and install AT&T Manhole 5830 on O'Farrell Street	6	CMB-0051	Agree	09/21/11	+25,000	AT&T was supposed to hire its own subcontractor to furnish and install all AT&T vaults in CN 1251. AT&T requested SFMTA for Synergy to perform this work as change order to CN 1251. AT&T agreed to reimburse SFMTA (\$25,000) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #7	CMOD #7 Approved
1251-0018	08/03/11	082-0018	Furnish and install AT&T Manhole 5829 on Geary Blvd.	6	CMB-0051	Agree	09/21/11	+31,000	AT&T was supposed to hire its own subcontractor to furnish and install all AT&T vaults in CN 1251. AT&T requested SFMTA for Synergy to perform this work as change order to CN 1251. AT&T agreed to reimburse SFMTA (\$31,000) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #7	CMOD #7 Approved

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1251-0019	08/03/11	082-0019 Extend joint trench and modify existing AT&T Manhole 403 on Ellis Street.	6	CMB-0057	Agree	10/26/11	+31,000	AT&T was supposed to furnish and install AT&T vault #5833 on Ellis Street. The current design was to intercept the existing AT&T conduit in the middle of Ellis Street. It was discovered that the existing AT&T conduit actually run adjacent to the southern curb line along Ellis Street. Therefore, AT&T requested SFMTA for Synergy to extend the joint trench approximately 200 feet to the West and tie into their existing AT&T vault #403. This work will also require Synergy to enlarge vault #403 to accept the additional conduit. . This additional work for AT&T will need to be reimbursed to SFMTA through the Form B process. Note: The agreed amount (\$31,000) is for enlargement of vault #403 only; the additional 200 feet of trench to be captured in the JT bid items. (See Trend 60) COR #43 COMPLETE, NO FURTHER ACTION; CMOD #11	CMOD #11 Approved
1251-0020	09/07/11	082-0020 Change PG&E conduit material from PVC to GRS on west side of Stockton between Geary and Post	6	CMB-0058	Agree	11/02/11	+19,537	PG&E requested to use GRS material in lieu of PVC in areas where PG&E conduits were expected to be exposed and temporary supported during future UMS station construction. PG&E agreed to reimburse SFMTA (\$19,537) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #21	CMOD #21 Approved
1251-0021	09/07/11	082-0021 Revise PG&E service points and layout of primary lines at various locations, and delete Muni Vault 1850 A at Stockton/Ellis.	6					PG&E made changes to the locations of the service points and layout of some of the primary lines. In addition, due to changes to the limits of the UMS Station on Ellis Street, it was determined that Muni Vault 1850A is no longer needed. Cost for this trend in captured in Trend #31 or CMod #12. CLOSED; NO FURTHER ACTION. Cost for this trend is captured in Trend #31/CMod #12	
1251-0022	09/07/11	082-0022 Change OCS supports at SF Hilton Hotel on Mason Street from embedded eyebolts to poles w/foundations.	7					During the design phase the representatives of the Hilton Hotel agreed to and signed a license agreement with the SFMTA to allow embedded eyebolt supports. Hilton's Dir of Property Ops now wants to have OCS poles rather than the embedded building eyebolt supports. However, the OCS designer was able to revise the OCS design to eliminate the need for either eyebolts or poles in front of Hilton Hotel, resulting in a \$2,000 credit. CLOSED; NO FURTHER ACTION. Applicable bid items to be adjusted accordingly, See Trend #60.	
1251-0023	09/07/11	082-0023 4th Street & Folsom Trolley Bypass	1	CMB-0065	Agree	1/25/2012, 2/29/12	+405,440	SFMTA Operations has requested a bypass be installed at 4th Street and Folsom Street. \$242,619 in labor costs for OCS installation has not been negotiated yet. 1/25: Condition agreed upon verification material from Contractor to be procured is available. (Verified 02/01/12). 02/29: CMod negotiated cost has been revised to incorporate original three items and the add'l OCS work on Folsom St. COR #52; PCC #7 Updated projected cost impact from \$490,000 to \$405,440. CMod #12 (Formally CMod #15) COMPLETE, NO FURTHER ACTION; CMOD #12	CMOD #12 Approved
1251-0024	09/07/11	082-0024 Install a 5" slurry wall between the gas line and wet utilities if the separation is less than 3 feet	6	CMB-0059	Agree	11/16/2011, 05/02/12	+201,411	As a new requirement, PG&E had requested that a 5" slurry wall be installed between the gas line and wet utilities where the separation between the 2 utilities is less than 3 feet. PG&E agreed to reimburse SFMTA (\$201,410) through the Form B process. COR #58 COMPLETE, NO FURTHER ACTION; CMOD #19	CMOD #19 Approved
1251-0025	09/07/11	082-0025 Additional PG&E conduit crossings at Geary and O'Farrell to minimize service interruption during future UMS Station construction	6					PG&E requested to install additional conduits crossing Stockton Street at Geary and O'Farrell to minimize service interruption during future UMS Station construction. PG&E agreed to reimburse SFMTA through the Form B process. This item is captured under applicable JT Bid item, see Trend 60. CLOSED; NO FURTHER ACTION. Item is captured under applicable JT Bid Item, See Trend 60.	
1251-0026	09/14/11	082-0026 Install cantilever footing in lieu of regular OCS foundation at 2 locations on Mason Street between Geary and O'Farrell	2					The sub-basement in the public parking garage at O'Farrell/Mason extends beyond the property line up to the curb line. Therefore, regular foundations as per original design could not be installed. CLOSED; NO FURTHER ACTION. Item is captured under applicable OCS Bid Item, See Trend 60.	

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1251-0027	09/20/11	082-0027	Accelerate installation of AT&T facilities in order for AT&T to start cut-over sooner	1	CMB-0056	Agree	11/30/11	+281,435	Acceleration is necessary to avoid potential delay impacts to Central Subway follow-on contracts. COR #40 COMPLETE, NO FURTHER ACTION; CMOD #10	CMOD #10 Approved
1251-0028	10/18/11	082-0028	Furnish and install AT&T Manholes 5828 (Stockton), 5831 (Geary), 5832 (O'Farrell) and 113 (O'Farrell)	6	CMB-0057	Agree	10/26/11	+157,000	AT&T was supposed to hire its own subcontractor to furnish and install all AT&T vaults in CN 1251. AT&T requested SFMTA for Synergy to perform this work as change order to CN 1251. AT&T agreed to reimburse SFMTA (\$157,000) through the Form B process. COR #43 COMPLETE, NO FURTHER ACTION; CMOD #11	CMOD #11 Approved
1251-0029	10/26/11	082-0029	Rebuild existing AT&T vault 133 at Post/Stockton and demolish existing AT&T vaults 403 (Ellis) and 113 (O'Farrell)	6					Vault 133 rebuild is superseded by Trend 40. Demolition of existing vaults 403 and 113 is superseded by Trend 43 CLOSED; NO FURTHER ACTION. Demo of exiting vaults 403 and 113 are superseded by Trend 43. (See CMOD #23)	
1251-0030	10/26/11	082-0030	Enlarge existing PG&E vault 873 at SW Geary/Stockton	6	CMB-0058	Agree	11/02/11	+46,366	PG&E asked SFMTA to enlarge vault 873. PG&E agreed to reimburse SFMTA (\$46,366) through the Form B process. CMB approved a NTE amount of \$40K COMPLETE, NO FURTHER ACTION; CMOD #21	CMOD #21 Approved
1251-0031	10/26/11	082-0031	Additional PG&E conduits crossing O'Farrell on east side of Stockton (Task Order #32)	6	CMB-0058	Agree	11/02/11	+17,193	PG&E asked SFMTA to install additional conduits. PG&E agreed to reimburse SFMTA (\$17,193) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #21	CMOD #21 Approved
1251-0032	11/08/11	082-0032	Excavation and restoration cost for PG&E gas tie-in and kill holes.	6	CMB-0059	Agree	11/16/2011, 05/02/12	+92,292	PG&E gas drawings were not included in the bid package and there was no bid item to capture this work. Gas drawings was issued by PG&E in June 2011. PG&E agreed to reimburse SFMTA (\$92,292) through the Form B process. Updated projected cost impact from \$202,400 to \$169,087 due to reduction in number of kill holes from 18 to 15. 04/11: Discovery of actual number of Kill/Tie locations being 26 instead of 15 will require confirmation from PG&E of the locations and the new cost of scope of work. COMPLETE, NO FURTHER ACTION; CMOD #19	CMOD #19 Approved
1251-0033	11/23/11	082-0033	Install secondary enclosure wall at One Stockton (Apple Store)	1	CMB-0068	Agree	03/07/12	+408,700	Additional enclosure wall is design initiated change. COR #47, PCC #9 Updated projected cost impact from \$262,484 to \$408,700. 39 Calendar Day Time Extension COMPLETE, NO FURTHER ACTION; CMOD #13 (formally CMOD #14)	CMOD #13 Approved
1251-0034	01/10/12	082-0034	Install temporary support as required to protect existing live utilities that are encroaching into the City right-of-way in front of 17-25 Stockton Street.	2	CMB-0069	Agree	04/04/12	+337,548	Existing utilities that are in active service were discovered in the City public right-of-way and are obstructing the performance of Contract work. Worst case scenario - "Not to Exceed" value) CM team will report actual amount value at a later date COR #71 CMB agreed to a Not-to-Exceed amount of \$346,456 on 4/4/12. All physical work was scheduled to complete by 4/13/12 COMPLETE, NO FURTHER ACTION; CMOD #17	CMOD #17 Approved
1251-0035	01/11/12	082-0035	Potholing on Stockton between Post and Geary for UMS Station design	1					UMS Design Team requested 1251 to pothole (6'Wx12'Lx8'D) on Stockton between Post and Geary to locate existing piles and tie-backs at Union Square. Cost for this scope of work from the Contractor is \$19,870.73. Per direction from the CMB on Jan 18th this request is denied and the DP2 designer will cover this potential future obstruction with a note on the drawings. COR #53 CLOSED; NO FURTHER ACTION. Per direction of CMB on 1/18/12, this request is denied and DP2 Designer will cover this potential future obstruction with a note on the drawings.	

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1251-0036	01/11/12	082-0036	6					CN 1252 Contract is requesting 1251 to install PG&E conduits in order to bring power in advance of the tunnel equipment arrival. The scope of work, which involves trenching at the busy 4th/Bryant intersection, was part of the 1252 contract, hence, a credit of same amount (\$54,892.07) to be due back to 1252. Based upon a field meeting and discussions with BIH on Jan 17th this work will be done under CN 1252 as originally planned. COR #55 CLOSED; NO FURTHER ACTION. Based on field meeting with BIH on 1/17/12; this work will be completed under CN 1252 as planned.	
1251-0037	01/17/12	082-0037	3	CMB-0078	Agree	09/12/12	+27,428	After field investigation, the existing sewer lateral was found to connect to the Stockton main near Geary. However, the location where the lateral is connected to the main is inside the footprint of the UMS station, in which the main is shown in the contract plan to be slurry filled. The contract plan shows the lateral to be connected to the main on Maiden Lane but is not possible due to invert problem. At the CMB meeting on 1/18/12, Albert Hoe reported that he had instructed the design team during the design phase to investigate this lateral and make the connection to the sewer main at Maiden Lane. RE was asked to investigate if this could be a possible error and omission by the sewer design. (See Trends #44 and 45) COMPLETE, NO FURTHER ACTION; CMOD #18	CMOD #18 Approved
1251-0038	01/04/12	082-0038	1	CMB-0064, CMB 0080	Agree	1/04/2012, 10/24/12	+29,273	JCDecaux refused to remove the remaining (2EA) kiosks without being paid for work already completed. The CMB approved on 1/04/12 Task Order No. 35 (Removal of 3EA sidewalk kiosks) as requested for the work already completed. On 10/24/12 the CMB approved Trend #38 removal of the remaining 2EA kiosks; work already completed and paid for as part of the approved CMB 0064 dated 01/04/12 for a total of 5EA kiosks removed. Total work priced at \$29,268. COMPLETE, NO FURTHER ACTION; CMOD #22	CMod #22 Approved
1251-0039	01/17/12	082-0039	1				+4,752	Requested by Muni Operation. COMPLETE, NO FURTHER ACTION; CMOD #22	CMod #22 Approved
1251-0040	02/15/12	082-0040	6	CMB-0070	Agree	04/11/12	+45,000	AT&T requested SFMTA for Synergy to rebuild manhole 133 at Post/Stockton. COR #64 AT&T agreed to reimburse SFMTA (\$45,000) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #14	CMOD #14 Approved
1251-0041	02/15/12	082-0041	6	CMB-0070	Agree	04/11/12	+23,704	AT&T requested SFMTA for Synergy to extend their conduit from vault to vault instead of wye-casting into exiting duct bank. COR #63 AT&T agreed to reimburse SFMTA (\$23,704) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #14	CMOD #14 Approved
1251-0042	02/15/12	082-0042	6	CMB-0070	Agree	04/11/12	+31,525	AT&T requested SFMTA for Synergy to extend their conduit from vault to vault instead of wye-casting into exiting duct bank. COR #62 AT&T agreed to reimburse SFMTA (\$31,525) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #14	CMOD #14 Approved
1251-0043	02/15/12	082-0043	6	CMB-0076	Agree Conditionally	10/03/12	+52,607	Agreed cost shown in Trends 29 and 40 is for installing vaults only. Cost to demo existing vaults is tracked on force account (estimated cost at \$15k each vault). AT&T agreed to reimburse SFMTA (\$52,607) through the Form B process. CMB Agreed contingent on receipt of confirmation letter from AT&T for costs associated with these three trends (CMB 0076 approval - see Trend CN1251 #'s 72 & 80) COMPLETE, NO FURTHER ACTION; CMOD #23	CMOD #23 Approved
1251-0044	02/15/12	082-0044	3	CMB-0078	Agree	09/12/12	+11,438	Contract plan SW-4 shows an existing lateral to be connected to new main. However, the 8" lateral is too big to be connected directly to the new 14" main necessitating a new connection to the manhole. However, numerous existing utilities were in the way and required installation of a new lateral at a higher elevation. Due to the raised lateral invert, modification to the building is also needed. COMPLETE, NO FURTHER ACTION; CMOD #18	CMOD #18 Approved

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1251-0045	02/15/12	082-0045	Install new 2 each sewer laterals on Stockton between Post and Geary	3	CMB-0078	Agree	09/12/12	+30,260	Contract plan SW-3 shows 2 each existing laterals coming from the Union Square garage to be connected to the new main at approximate sta 128+55. However, the laterals were found to be connected to the downstream end of the existing main (toward Geary), which is called for to be abandoned in 1251 to make room for the future station. (See Trends #37 and 44) COMPLETE, NO FURTHER ACTION; CMOD #18	CMOD #18 Approved
1251-0046	03/06/12	082-0046	Additional cost for enlarging PG&E vault 584 on O'Farrell east of Stockton	6				+80,000	The two (2) existing PG&E duct banks that were supposed to be intercepted by vault 584 were wider than shown on plan. Vault 584 was needed to be enlarged in order to capture the two (2) ductbanks. PG&E agreed to reimburse SFMTA (\$80,000) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved
1251-0047	04/10/12	082-0047	Enlarge Muni Vault 900A on Stockton between Post and Geary	2	CMB-0082	Agree	10/31/12	+15,357	Contract plan JT-301 calls for an intercept vault to capture 2 (E) Muni ductbanks (DB). These DBs were found to be farther apart than shown on plan. The intercept vault needs to be widened. (CMB 0082 approval - see Trend CN1251 #'s 47, 58 & 78) COMPLETE, NO FURTHER ACTION; CMOD #26	CMOD #26 Approved
1251-0048	04/10/12	082-0048	Additional cost and time for stuck auger during micropile drilling	2					Contractor's hollow stem auger got stuck during drilling of micropile #27C in front of 17 Stockton. Contractor claims differing condition as cause for the stuck auger. Contractor is asking for 1 day compensable time extension. SFMTA is reviewing claim for entitlement. COR #67 RE Denied COR (City Letter 26); however Contractor will most likely send rebuttal. CLOSED; NO FURTHER ACTION.	
1251-0049	04/10/12	082-0049	Additional cost and time for added rebar and coordination with 17-19 Stockton Owner's contractor	2	CMB-0083	Agree	11/07/12	+9,058	Existing footing of One Stockton was found to be recessed from property line. City's response to RFI 171 called for additional rebar to compensate for the gap between back of new enclosure wall and existing footing. Also, the Owner's contractor of 17-19 Stockton asked that Synergy clear his work area so he can pour the primary wall and bring in his new switchgear. Synergy is asking 2 days compensable time extension. (CMB 0083 approval - see Trend CN1251 #'s 53, 59, and 64) COMPLETE, NO FURTHER ACTION	CMOD #27 Approved
1251-0050	04/10/12	082-0050	Additional cost to install a Type 770 pole foundation at a different location, install 2 each DPT mast arms and demo existing wall in conflict with OCS footing	2	CMB-0074	Agree	08/15/12	+48,898	Type 770 pole foundation at SW 5th/Folsom could not be installed per plan due to a conflict with a cistern. Only solution is to remove an existing pole foundation (recently installed in 1251 as contract work) to make room for the 770 pole foundation. At 5th/Folsom, 2 DPT mast arms (not shown in original plan) need to be installed. At Pole #811, an existing concrete wall is in conflict with pole foundation. (CMB 0074 approval - see Trend CN1251 #'s 67, 74 & 75) COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
1251-0051	04/10/12	082-0051	Intercept existing PG&E duct bank and terminate into vault 5464 at Market/Ellis	6				+23,633	PG&E requested to reroute existing duct bank running north out of MH 1803 at 4th/Pioneer Place into MH 5464 at Market/Ellis PG&E agreed to reimburse SFMTA (\$23,633) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved
1251-0052	04/10/12	082-0052	Reroute unforeseen existing waterline on Geary east of Stockton	2	CMB-079	Agree	10/10/12	+27,234	An unforeseen existing waterline was found to be in conflict with PG&E vault 573 and needs to be rerouted per City's response to RFI #182. (CMB 0079 approval - see Trend CN1251 #'s 65 & 70). This is potentially a E&O issue due to unforeseen existing waterline on discovered along Geary Street (east of Stockton Street) not shown in the contract drawings. COMPLETE, NO FURTHER ACTION; CMOD #28	CMOD #28 Approved

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1251-0053	04/10/12	082-0053	Additional cost for premium to accelerate contract work	1	CMB-083	Agree	11/07/12	+10,591	SFMTA directed Synergy to accelerate work to advance PG&E and AT&T cut-over and allow CN 1252 Contractor to start work at UMS COR #74 PG&E agreed to reimburse SFMTA (\$10,590) through the Form B process. 11/07/12 (CMB 0083 approval - see Trend CN1251#s 49, 59, and 64) COMPLETE, NO FURTHER ACTION; CMOD #27	CMOD #27 Approved
1251-0054	04/17/12	082-0054	Additional waterproofing at FIDM and north end of Crate & Barrel	2				+8,151	Sidewalk in front of FIDM and north end of Crate & Barrel did not have a sub-sidewalk basement, hence the original plan did not call for waterproofing of the primary wall. While excavating for the joint trench, contractor found the existing waterproofing to be spalling and peeling off from the primary wall. COR #72 COMPLETE, NO FURTHER ACTION; CMOD #22	CMOD #22 Approved
1251-0055	04/17/12	082-0055	Install Swiveloc lids on all PG&E vaults	6					PG&E requested SFMTA to add this scope to the 1251 contract for safety reason. COR #72 SFMTA decided to defer this work to the 1252 contract. COMPLETE, NO FURTHER ACTION;	
1251-0056	05/31/12	082-0056	Additional work on force account related to sewer installation	2				+45,156	Additional work due to unforeseen and differing conditions during installation of sewer. COMPLETE, NO FURTHER ACTION; CMOD #15	CMOD #15 Approved
1251-0057	05/31/12	082-0057	Additional work on force account related to water installation and NRG vault demolition	2				+39,599	Additional work due to unforeseen and differing conditions during installation of water and demolish existing NRG vaults that are in conflict with utility installation. COMPLETE, NO FURTHER ACTION; CMOD #16	CMOD #16 Approved
1251-0058	05/31/12	082-0058	Additional streetlight conduit on 5th Street between Harrison and Bryant	2	CMB-0083	Agree	10/31/12	+17,754	The existing streetlight pullboxes new cables were supposed to be connected to per contract plan did not have power. New conduit needs to be install from the power source. COR #75 (CMB 0082 approval - see Trend CN1251 #s 47, 76 & 78) COMPLETE, NO FURTHER ACTION; CMOD #25	CMOD #25 Approved
1251-0059	06/26/12	082-0059	Accelerate contract work at Barney's front	1	CMB-0083	Agree	11/07/12	+16,948	As a result of numerous meetings and complaints by Barneys, it was decided to accelerate contract work in order to restore Barneys sidewalk earlier (CMB 0083 approval - see Trend CN1251 #s 53, 49, and 64) COMPLETE, NO FURTHER ACTION; CMOD #27	CMOD #27 Approved
1251-0060	06/26/12	082-0060	Final Bid Item Reconciliation	4				(508,961)	Bid item analysis for entire contract as of 6/12/12.	CMOD #30 Approved
1251-0061	06/26/12	082-0061	Excavate for PG&E cutover at 177 Stillman	6				+7,424	PG&E requested Synergy to excavate for PG&E cutover at 177 Stillman. Reference document: Email from Michael Lightstone dated 12/9/11 PG&E agreed to reimburse SFMTA (\$7,424) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved
1251-0062		082-0062	Additional waterproofing at Macys Men's wall	2				+24,774	Contract did not call for waterproofing at Macys Men's wall. However, when contractor dug for the joint trench, it was found that the existing waterproofing has deteriorated and needed to be replaced. COR# 20 COMPLETE, NO FURTHER ACTION; CMOD #22	CMOD #22 Approved
1251-0063		082-0063	Excavation and restoration cost for PG&E gas tie-in and kill holes. Part II	6				+56,458	After completion of Part I (see trend 32), PG&E had offered to self-perform the excavation and restoration of the gas tie-in/kill holes because PG&E had crews available to perform the work. However, PG&E no longer have available crews, hence Synergy had to continue and finish excavating and restoring the last tie-in/kill holes. PG&E agreed to reimburse SFMTA (\$56,458) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved
1251-0064		082-0064	Accelerate finish and detail work of AT&T and PG&E vaults	1	CMB-0083	Agree	11/07/12	+4,704	In order to expedite turn-over of AT&T and PG&E vaults, SFMTA directed Synergy to accelerate the detailing and finish work of these vaults. SFMTA will pay Synergy premium cost for OT hours. 11/07/12 - (CMB 0083 approval - see Trend CN1251 #s 53, 59, and 49) COMPLETE, NO FURTHER ACTION; CMod #27	CMOD #27 Approved

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1251-0065		082-0065 Additional fittings for AWSS lateral and chipping of pole foundation at Market/Ellis	2	CMB-0079	Agree	10/10/12	+20,412	An existing Muni pole foundation is in the way of the AWSS lateral. Additional fittings are required to complete contract work. (CMB 0079 approval - see Trend CN1251 #'s 52 & 70) This is potentially an E & O issue due to an existing Muni pole foundation is in the way of the AWSS lateral. COMPLETE, NO FURTHER ACTION; CMOD #28	CMOD #28 Approved
1251-0066		082-0066 Intercept PG&E conduits at Vault 573 at Geary east of Stockton	6				+6,466	During installation of PG&E intercept vault 573, PG&E had incorrectly identified the conduits to be intercepted. This error was discovered during cut-over process by PG&E. PG&E agreed to reimburse SFMTA (\$6,466) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved
1251-0067		082-0067 Additional feeder span, no-bo insulator and adjustment of guywire for the Folsom OCS bypass	3	CMB-0074	Agree	08/15/12	+32,149	SFMTA Electrical Designer had pointed out the new Folsom Bypass does not have enough guy capacity to power the new OCS necessitating installation of feeder spans. Also, adjustments to the guywire are necessary to mitigate conflict with new signal mast arms. COR #76 7/25/12 - RE presented COR to CMB; direction given to develop one CMod with all remaining items to be purchased for the new OCS bypass. (CMB 0074 approval - see Trend CN1251 #'s 50, 74 & 75) COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
1251-0068		082-0068 Extended Overhead cost from July 19, 2012 to August 16, 2012	1	CMB-0075	Agree Conditionally	10/03/12	+106,967	Contract is expected to be extended beyond the approved July 18, 2012 completion due to the following reasons: contractor was slowed due to accommodation of AT&T and PG&E cut-over, for example, contractor has to provide windows of time for AT&T to occupy its work space to provide continuity and efficiency for AT&T cut-over, contractor has to advance cleaning and detailing of vaults to turnover to AT&T and PG&E, these work are typically done as punchlist, contractor now has to sequence work and reallocate resources; SFWD cut-over is taking longer than expected; PG&E gas cut-over was delayed due to inability to access Armani building, this delay held up completion of sewer MH, SFWD connection and restoration on O'Farrell east; added change order work, i.e. additional PG&E conduit at vault 573 and additional excavation for AT&T cut-over at 240 Stockton CMB Agreed contingent on receipt of letter confirming e-mail as presented in the meeting and content viewed related to agreement and remaining actions and release of funds. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #29 Approved
1251-0069		082-0069 Removal of abandoned PG&E gas valve (8each) and install anodes	6				+4,737	PG&E asked Synergy to remove all abandoned gas valves (8each) PG&E agreed to reimburse SFMTA (\$4,737) through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved
1251-0070		082-0070 Core drill primary wall and install interior plumbing at Armani and Disney for the fire service water cutover	2	CMB-0079	Agree	10/10/12	+14,034	Existing fire service inside the building is a thin-wall steel pipe that extends out to the previously existing sub-sidewalk basement. However, 1251 had backfilled the subsidewalk basement per contract, which now made the steel pipe susceptible to corrosion. SFWD does not want to tie-in to the steel pipe due to corrosion issue plus the fact that the pipe is thin walled, which makes welding almost impossible. A ductile iron pipe needs to be installed from the main and into the building. COR #77 (CMB 0079 approval - see Trend CN1251 #'s 52 & 65) COMPLETE, NO FURTHER ACTION; CMOD #28	CMOD #28 Approved
1251-0071		082-0071 Install a new 8" backflow preventer for the fire service inside Macys Men's building	6					The new water main (installed in the 1251 Contract) is closer to the property line than the existing backflow preventer. State law stipulates that a backflow preventer be installed in between the property's fire suppression system and the water main. The SFWD refuses to make this connection until a proper backflow preventer is installed. SFMTA will insist on Macys to install its own backflow preventer. CLOSED; NO FURTHER ACTION.	

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1251-0072	08/08/12	082-0072	6	CMB-0076	Agree	10/03/12	+23,993	AT&T requested new conduits to maintain continuity between the 2 vaults. AT&T agreed to reimburse SFMTA (\$23,993) through the Form B process. RE Tracking work on FAR CMB Agreed contingent on receipt of confirmation letter from AT&T for costs associated with these three trends (CMB 0076 approval - see Trend CN1251 #'s 43 & 80) COMPLETE, NO FURTHER ACTION; CMOD #14	CMOD #14 Approved
1251-0073	08/08/12	082-0073	1				+17,966	Contract bid allowance for providing utility services for flower stands (UD-14) had been exhausted during relocation of 2 flower stands. A third and final flower stand is still needed to be relocated. COMPLETE, NO FURTHER ACTION; CMOD #22	CMOD #22 Approved
1251-0074	08/08/12	082-0074	1	CMB-0074	Agree	08/15/12	+16,429	Due to installation of OCS bypass on Folsom between 4th and 5th Streets, a left turn signal is required at 5th/Folsom. Additional signal heads and modification to the existing traffic signal is necessary. RE Tracking work on FAR (CMB 0074 approval - see Trend CN1251 #'s 50, 67, & 75 also). COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
1251-0075	08/08/12	082-0075	2	CMB-0074	Agree	08/15/12	+12,568	Contractor encountered unforeseen concrete and brick substructures and abandoned utility lines. RE Tracking work on FAR (CMB 0082 approval - see Trend CN1251#'s 47, 58 & 78) COMPLETE, NO FURTHER ACTION; CMOD #20	CMOD #20 Approved
1251-0076	08/08/12	082-0076	2/3	CMB-0082	Agree	10/31/12	+14,988	Modify bracket arm at existing Pole #511 to alleviate tension load and bending of pole. Realign trolley wire on Mason between Geary and Eddy to smoothen transition when shifting from right lane to left lane. Also, add intermediate guywire to prevent excessive sagging of bracket arms at various locations. RE Tracking work on FAR (CMB 0082 approval - see Trend CN1251#'s 47, 58 & 78) COMPLETE, NO FURTHER ACTION; CMOD #26	CMOD #26 Approved
1251-0077	09/12/12	082-0077	2	CMB-0081	Agree Conditionally	10/31/12	+37,181	Qwest, TCG and UCCO requested Synergy to tie-in their existing conduits into the new vaults that Synergy installed per contract. The utility companies are responsible for connecting existing conduits into new vaults, therefore, this request is considered extra work. Private Utilities companies agreed to reimburse SFMTA (\$37,181) through the Form B process. RE Tracking work on FAR CMB agreement contingent upon CMod package to include evidence from all utilities that cost are reimbursable to SFMTA through the Form B process. COMPLETE, NO FURTHER ACTION; CMOD #25	CMOD #25 Approved
1251-0078	09/12/12	082-0078	2	CMB-0082	Agree	10/31/12	+27,645	Several obstructions, i.e. buried concrete, boulders, brick, abandoned utilities were discovered during excavation and installation of OCS poles along Mason and 5th Street. RE Tracking work on FAR (CMB 0082 approval - see Trend CN1251 #'s 47, 58 & 76) COMPLETE, NO FURTHER ACTION; CMOD #26	CMOD #26 Approved
1251-0079	09/12/12	082-0079	2				+44,530	Unforeseen conditions were encountered during installation of select PG&E vaults including joint trenches leading to these vaults. PG&E agreed to reimburse SFMTA (\$44,530) through the Form B process. RE Tracking work on FAR COMPLETE, NO FURTHER ACTION; CMOD #24	CMOD #24 Approved

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1251-0080	09/12/12		082-0080	Additional work related to installation of AT&T Vaults 5828, 5829, 5830, 5831, 5832 and 5833 including joint trench leading to these vaults	2	CMB-0076	Agree	10/03/12	+56,806	Unforeseen conditions were encountered during installation of select AT&T vaults including joint trenches leading to these vaults. AT&T agreed to reimburse SFMTA (\$56,805) through the Form B process. CMB Agreed contingent on receipt of confirmation letter from AT&T for costs associated with these three trends (CMB 0076 approval - see Trend CN1251 #'s 43 & 72) COMPLETE, NO FURTHER ACTION; CMOD #23	CMOD #23 Approved
1251-0081	09/13/12		082-0081	PG&E concrete cover versus CDF on electrical duct bank	6	CMB-0107	Agree	02/13/13	+43,882	Contract Plan JT-014 calls for CDF cover on PG&E duct bank. However, PG&E requested to use regular concrete due to heat dissipation problem with CDF. PG&E agreed to reimburse SFMTA (\$43,881) through the Form B process. 12/19/12 - Presented to CMB, direction given to finalize cost and obtain a Form B agreement with PG&E 02/13/13 - Revised CMod presented to CMB. PG&E agreed with negotiated reimbursement amount. COMPLETE, NO FURTHER ACTION	CMOD #24 Approved
Contract 1251 Totals									+3,962,032	Total Changes to the contract in the amount of 3,962,032 represents an UNDERRUN of allocated contingency in the amount of \$1,405,265.32. Changes that will be Utility Reimbursement credits through the Form B process in the amount of \$1,224.828 will increase the UNDERRUN. Final Contract Closeout Total \$20,794,581 will be reduce by \$7,618,112 Utility Reimbursement credits for a Final Program cost of \$13,176,469 for CN 1251	
Contract 1252		COR No.	PCC No.	1252 Change Description	Change Type	CMB Change No.	Configuration Management Board Agree/Disagree	Decision Date	Actual/Forecast	Comments	Contract Modification No.
1252-0001	04/23/12	1		AWSS Conflict w/ Low Pressure Water	6	CMB-0091	Agree to Merit Agree to Cost	10/10/12 1/16/13	+16,907	AWSS contract work was made more difficult by the proximity of a water line which was closer than indicated on UT-501. See COR 008 for TIA request. 10/10/12 - Merit info presented to CMB - Agreed 01/16/13 - CMB - NOT TO EXCEED Amount \$16,900	
1252-0002	04/23/12	2		AWSS Add. Tie-Rods 4th-Harrison	6	CMB-0086	Agree to Merit Agree to Cost	10/10/12 12/19/12	+58,000	The AWSS line must be restrained a certain distance beyond the 90 degree elbow we are installing under contract 1252. The 1252 contract work includes excavating on both sides of this 12-foot portion of pipe. Additional work scope (per Michael Smith @ DPW): Station 162+80 to 162+90 - Restrain the existing 12-inch tee to the existing pipe north of the tee. Restrain any other joints that are not restrained on the existing pipe up to the new pipe to be installed. 10/10/12 - Merit info presented to CMB - Agreed 12/19/12 - CMB - NOT TO EXCEED Amount \$58,000	
1252-0003	04/23/12	3		AT&T Vault Conflict-4th & Harrison	6	CMB-0092	Agree to Merit Agree to Cost	10/10/12 1/16/13	+16,015	After excavation of the roadway surface and subgrade at Fourth and Harrison streets an AT&T vault was discovered to be in conflict with the current alignment of the 42" RCP sewer main. Verbal authorization was given by an AT&T representative to make modifications to the vault. COR 003 has been submitted by BIH in response to this issue. Work is to be charged via SFMTA Form B agreement to AT&T. 10/10/12 - Merit info presented to CMB - Agreed 01/16/13 - CMB - NOT TO EXCEED Amount \$21,089 04/01/13 - CM team must obtain in writing from AT&T a utility agreement for cost reimbursement.	

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1252-0004	05/18/12	6	PG&E live electrical delays	6	CMB-0088	Agree to Merit Agree to Cost	11/28/12 1/9/2013	+7,402	PG&E lines indicated on contract drawing UD-502 thru 504 were to be abandoned yet were not yet abandoned when BIH started work. They were abandoned on 5/21. 11/28/12 - Merit info presented to CMB - Agreed 01/09/13 - CMB - NOT TO EXCEED Amount \$7,600 04/01/13 - CM team must obtain in writing from PG&E a utility agreement or cost reimbursement. COMPLETE, NO FURTHER ACTION: CMOD #10	CMOD #10 Approved
1252-0005	06/06/12	7	Oil filled pipe @ Launch Box	2	CMB-0093	Agree to Merit Agree to Cost	1/16/13 1/16/13	+9,980	between Harrison and Bryant. 01/16/132 - Merit info presented to CMB - Agreed 01/16/13 - CMB - NOT TO EXCEED Amount \$10,000 COMPLETE, NO FURTHER ACTION; CMOD #05	CMOD #5 Approved
1252-0006	04/23/12	8	TIA - Associated w/ COR 001, COR 002, and COR 003	6		Agree to Merit	10/10/12	+269,904	TIA and Overhead Costs related to COR 001, 002 and 003. 10/10/12 - Merit info associated w/COR's 001, 002 & 003 presented to CMB - Agreed. 01/30/13 Elevation Ladder documentation presented for review of unresolved issue of payment of Indirect costs for TIA submittals- COR 008	
1252-0007	06/12/12	9	MOS - Live Utilities: PGE, AT&T, and SFWD 8" Water Line extra kill hole location	2		Agree to Merit	11/14/12	+49,007	Live PG&E Electrical Lines Conflict: BIH was notified that PG&E had live electrical lines running through each headwall work zone @ Moscone station. 8 inch Low Pressure Water Line Conflict: Contract drawings show this utility to be abandoned and capped prior to work. Synergy struck and damaged the water line. SFWD directed Synergy to excavate a kill hole so they could properly abandon the line. 11/04/12 - COR Evaluation for Merit presented to CMB.	
1252-0008	06/19/12	10	MOS - Demo PGE duct bank / SF City 911 fiber optic line	2	CMB-0094	Agree to Merit Agree to Cost	10/10/12 1/16/13	+4,916	Upon excavation of the MOS north headwall, BIH's subcontractor Synergy Project Management (SPM) uncovered a PG&E duct bank that contained a live San Francisco 911 fiber optic line. 11/14/12 - Presented to CMB. The narrative will be rewritten CMB agrees with partial merit labor work involved in supporting the utility agencies. This item will be brought back to the CMB for approval. 01/09/13 - CMB - NOT TO EXCEED Amount 4,919.54 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. COMPLETE, NO FURTHER ACTION: CMOD #03	CMOD #3 Approved
1252-0009	07/02/12	11	MOS - Traffic Signal line re-route south headwall	6	CMB-0095	Agree to Merit Agree to Cost	1/16/13	+37,500	After potholing Synergy discovered several traffic lines which were in the proposed location of the south MOS headwall. Traffic Lines re-routed. 01/16/13 - CMB - NOT TO EXCEED Amount \$37,500	
1252-0010	01/00/00	12	MOS - Archaeological Standby North Headwall	2	CMB-0096	Agree to Merit Agree to Cost	12/11/12 1/16/13	+16,892	uncovered a layer of "Midden" or Native American debris. 12/11/12 - Merit info presented to CMB - Agreed 01/16/13 - CMB - NOT TO EXCEED Amount \$7,600 COMPLETE, NO FURTHER ACTION: CMOD #06	CMOD #6 Approved
1252-0011	07/06/12	13	PCC No. 14 - MOS - SL conduit from PG&E vault @ north headwall	6		Agree to Merit	11/28/12	+10,903	Permanent power is required to supply an existing Street Light box north of the MOS N headwall. 11/28/12 - Merit info presented to CMB - Agreed 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement.	
1252-0012	06/26/12	15	LB - Pre-Excavation for Slurry Walls	2	CMB-0087	Agree to Merit Agree to Cost	11/28/12 12/19/12	+96,000	Pre-excavation of slurry wall panels due to CDF encased sewer line. The concrete encasement was found by the Contractor to extend to a depth of 9'-11' below the top of guide wall or street surface. The 8" VCP sewer main and concrete encasement was incorrectly shown in the contract drawings to be approximately 4'-5' below the street surface. 11/28/12 - Merit info presented to CMB - Agreed 12/19/12 - CMB - NOT TO EXCEED Amount \$96,000	
1252-0013	07/02/12	17	MOS - Standby Time @ south headwall due to live TS lines	6		Agree to Merit	11/21/12	+79,219	Standby delays related to the discovery of traffic lines within the proposed location of the south MOS headwall. 11/14/12 - COR presented to CMB. CMB requested additional revisions to the evaluation. CMB agrees with partial merit. The COR will be brought back to the CMB for approval. 11/21/12 - COR revisited at mtg for clarification the issue is related to CMB action from item 3 -7/25/12 (Mtg #88) specifically acted on 08/01/12 (CMB Mtg. 89) referred originally as Trend #15.	

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1252-0014	Closed	19	UMS - Grant street 2 way conversion (e- & striping)	7					In order to implement a full street closure for Stockton Street at the UMS location the DPT recommended that Grant Street be converted from an existing one-way street to a two way street for vehicular traffic. Closed - Work is done. 07/18/12 - This potential change was brought before the CMB. Board's decision to have DPT perform the work. CLOSED; NO FUTHER ACTION	
1252-0015	06/22/12	20	PROJECT WIDE - Utility Demo limit reduction credit	5					BIH's subcontractor Synergy Project Management (SPM) would like to minimize the limits of utility demo at the Launch Box, Moscone station, UMS station and TBM Retrieval Shaft locations. Closed - SFMTA to BIH/SPM "Build per Plan".	
1252-0016	04/02/12	21	UMS - Reduced Duration	5	CMB-0073	Agree Cost (Conditionally)	8/01/12	-0-	No cost change for acceleration of utility relocation, ground improvements (jet grouting), and headwall construction at Union Square/Market Street Station. 08/01/12 - CMB approval condition upon incorporating correctness to backup documentation as required by & documented in CMB mtg. RE to prepare modification. COMPLETE, NO FURTHER ACTION: CMOD #08	CMOD #8 Approved
1252-0017	07/10/12	22	MOS - Asbestos Pipe at north headwall	2	CMB-0090	Agree to Merit Agree to Cost	12/12/12 1/09/13	+5,705	Upon excavation of the MOS north headwall, BIH's subcontractor Synergy Project Management (SPM) uncovered friable asbestos-laden transite pipe. 12/12/12 - Merit info presented to CMB - Agreed 01/09/13 - CMB - NOT TO EXCEED Amount \$6,000 COMPLETE, NO FURTHER ACTION: CMOD #5	CMOD #5 Approved
1252-0018	07/10/12	23	MOS - Brick Wall #2 discovered @ north headwall	2		Agree to Merit	12/6/12	+16,025	Upon excavation of the MOS north headwall, BIH's subcontractor Synergy Project Management (SPM) uncovered a buried wall. 11/14/12 COR presented to CMB. Evaluation for merit will be rewritten and brought back to CMB for approval. 11/21/12 - Merit info presented to CMB - Agreed	
1252-0019	08/10/12	26	MOS - Asbestos Pipe at south headwall	2	CMB-0085	Agree to Merit Agree to Cost	12/12/12 1/09/13	+27,630	Transite pipe was discovered during MOS south headwall excavation. 12/12/12 - Merit info presented to CMB - Agreed 01/09/13 CMB - NOT TO EXCEED Amount \$30,000 COMPLETE, NO FURTHER ACTION: CMOD#15	CMOD #15 Approved
1252-0020	08/10/12	27	MOS - Oil filled pipe at south headwall	2	CMB-0089	Agree to Merit Agree to Cost	12/12/12 1/09/13	+8,226	A pipe containing oil was discovered during MOS south headwall excavation. 12/12/12 - Merit info presented to CMB - Agreed 01/09/13 CMB - NOT TO EXCEED Amount \$8,600 COMPLETE, NO FURTHER ACTION: CMOD #05	CMOD #5 Approved
1252-0021	06/19/12	29	MOS - Todco scaffolding reimbursement	2	CMB-0097	Agree to Merit Agree to Cost	12/12/12 1/16/13	+1,718	1252 contract work requires the removal of scaffolding erected by Fine Line Construction, the contractor performing renovations on the Woolf House. 12/12/12 - Merit info presented to CMB - Agreed 01/09/13 - CMB - NOT TO EXCEED Amount \$1,717 COMPLETE, NO FURTHER ACTION: CMOD #03	CMOD #3 Approved
1252-0022	Closed	closed	Disputed Work Items - NTP2 vs. NTP3	7				-0-	BIH interpretation of Special Provisions SP-3 NTP2 description includes activities that SFMTA interprets to be within NTP3. These activities include: Ellis Street and Green Street shafts and associated compensation grouting, MOS headwalls and jet grouting, jet grouting at cross passage 5, UMS head walls and jet grouting, OCS removal along Stockton Street from Geary to Ellis, and the TBM retrieval shaft. On April 13, SFMTA released the MOS headwalls and jet grouting, jet grouting at cross passage 5, UMS jet grouting, OCS removal along Stockton Street from Geary to Ellis, and preparatory and utility work necessary for the construction of the retrieval shaft.	
1252-0023	05/30/12	4	Manhole in east guidewall footprint	2				-0-	While performing excavation for the east side guidewalls at the TBM Launch Box, BIH's subcontractor CJA-NCC encountered a manhole near the bottom of the excavation which was unexpected. BIH has stated that removal of the manhole prior to the installation of the guidewalls was necessary to ensure the successful installation of the slurry walls.	
1252-0024	Closed	closed	2" gas line inside 16" casing	2				-0-	While performing excavation for the Sewer and AWSS trench at the intersection of Fourth and Harrison streets, BIHJV's subcontractor Synergy encountered what was identified to be a 16" depressurized gas main. This gas main contained a 2" gas line which is shown on contract drawing UD-502 to be abandoned/removed.	

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1252-0025	04/23/12	5	CDF Encasement on 42" RCP	2				+140,596	While performing excavation for the east side guidewalls at the TBM Launch Box, BIH's subcontractor CJA/NCC encountered a CDF backfill at the existing 42" RCP sewer which reportedly had a significantly higher compressive strength than the 50-150 psi. which was anticipated. BIH claims that the CDF conflicted with the construction of the guidewalls and it could not be excavated by conventional means.	
1252-0026	Closed	closed	Damage to Live LPW main at 4th and Folsom	2				-0-	Synergy Demo Crew hit a blow-off valve on the 8" water line at the Southwest corner of the MOS headwalls. SFWD is to install a permanent cap per contract drawing WD-404.	
1252-0027	Closed	14	MOS - 76 Gas Station fencing	1					SFMTA requested BIH to install fencing around the perimeter of the 76 gas station at 4th and Folsom St. Issue closed, cost of fence paid by BIH in return for use of gas station lot. CMB - Temporary use of 800 Folsom Street site for construction staging. Use of this site would be a tradeoff between SFMTA and BIH. The Contractor would maintain the sites security and up-keep in exchange for not billing.	
1252-0028	Closed	16	LB - SPTC delays due to concrete encased 8" sewer	2					07/25/12 - Verbal information presented to CMB as a potential change. CLOSED NO FURTHER ACTION	
1252-0029	08/06/12	18	MOS - South Headwall re-design	1				+145,000	Costs related to leave end stops in place and a "T-Section" at the Moscone headwalls return walls. 11/14/12 - Presented to CMB. RE is recommending a possible compensation value of \$135K. CMB agrees with partial merit for labor and material cost of this COR. RE will proceed with scoping the work. 04/17/13 - Elevation Ladder of unresolved issue of End Stops presented to CMB - COR 018	
1252-0030	08/02/12	24	Ellis - Utility demolition	3		Agree to Merit	12/6/12	+38,678	BIH says that no utilities were shown at Ellis; however, Reference Drawing UE-126 shows utilities. 12/06/12 - Merit info presented to CMB - Agreed	
1252-0031	Closed	25	Crack Gauges	4				-0-	BIH disagrees with SFMTA on the total number of crack gauges to be installed on the project. Per the contract drawings a total of 926 crack gauges are to be installed. BIH claims that a total of 50 are to be installed.	
1252-0032	08/07/12	28	Ellis asbestos abatement	2		Agree to Merit	12/6/12	+35,386	A steam line containing asbestos was discovered during potholing at the Ellis Street Shaft. 12/06/12 - Merit info presented to CMB - Agreed	
1252-0033	-	30	LB - Class 1 Hazardous Material Haul off	2				+198,276	BIH trucking costs for Class 1 haul off generated at staging yard. Also see COR 054 regarding Class 1 Haz material haul off for Phase 2.	
1252-0034	-	70	LB - Jet Grout Quantity Overrun	4	CMB-0115	Agree to Merit Agree to Cost	3/20/13 04/03/13	+768,600	The contract drawings call for the contractor to determine the extent of jet grouting at the launch box. Bid Item ES-3 has an estimated quantity of 2,831 CY of in-situ treated ground. The Contractor has currently installed 3,929 CY of Soilcrete (Jet Grout) for ground treatment. This translates to an additional quantity of 1,098 CY of ground treatment beyond the quantity listed for Bid Item ES-3. By subtracting 241 CY for the "scallop" portion of the columns the total is 857 CY. 03/20/13 - Merit info presented to CMB - Agreed 04/03/13 - CMB - NOT TO EXCEED Amount \$599,900	
1252-0035	01/00/00	unsubmitted	Utility Removal UD-304 and UD-503	6				TBD	RFI 00134 asks whether certain utilities indicated on drawings UD-304 (On Stockton inside limits of future UMS Station) and on UD-503 (On Fourth and Stillman Streets outside limits of Launch Box) may be left in place. Answer: "Confirmed, the utilities in question can be abandoned in place and are not required to be removed. The Engineer will seek an adjustment to Bid Item GB-3 for the elimination of this work."	
1252-0036	05/17/12	35	Additional Road Base on Fourth Street	2			12/6/12	+7,000	BIH's subcontractor Synergy Project Management (SPM) discovered what they believe to be an additional layer of road base under the existing asphalt and road base layer while performing roadway demo work along the west side of Fourth street between Harrison and Bryant St. The additional layer is estimated by BIH/SPM to be approximately 8" - 12" thick. 12/06/12 - Evaluation of merit presented to CMB - Agreed to No Merit for this change.	
1252-0037	01/00/00	unsubmitted	DPW permitting potential impacts	7				TBD	DPW Permitting issue to cause potential impacts. DPW is not issuing BIH permits for excavating the MOS headwalls, jet grouting at the LB, and excavating support at the LB, as they claim to have not been able to conduct a thorough review of the project's design documents and were not included in the design process. BIH may be required to re-sequence and stand many other potential impacts w/o said permits.	

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1252-0038	05/30/12	38	Manhole in West Guidewall Footprint	3	CMB-0101	Agree to Merit Agree to Cost	12/05/12 1/23/13	+3,821	While performing excavation for the west side guidewalls at the TBM Launch Box, BIH's subcontractor CJA-NCC unexpectedly encountered a manhole foundation within the excavation at approximate station 165+70. It was necessary to chip away a portion of the existing manhole to gain the necessary clearance to construct the west slurry wall. 12/05/12 - CMB agreed with Evaluation of Merit 01/23/13 - CMB - NOT TO EXCEED Amount \$4,052.17 COMPLETE, NO FURTHER ACTION: CMOD #12	CMOD #12 Approved
1252-0039	01/00/00	unsubmitted	Panel W-33 Obstruction	2				TBD	While performing excavation for the slurry walls at the TBM Launch Box, BIH's subcontractor CJA-NCC unexpectedly encountered buried (wood pile?) debris located approximately 12 feet below the guide wall within Launch Box Diaphragm Wall Panel W-33. This trend is being tracked separately from Trend 20 because we believe it may be a different structure. BIH has not provided adequate information substantiating the merging of the two trends.	
1252-0040	08/23/12		PCC No.1 - Revisions to Moscone North Headwall Elevation (Top)	1	CMB-0099	Agree to Cost	1/16/2013	+20,358	SFMTA initiated change to revise headwall elevations to facilitate a change in the roof elevation of MOS station. 01/16/13 CMB - NOT TO EXCEED Amount \$20,500	CMOD #7 Approved
1252-0041	08/23/12		PCC No. 2 - Revisions to UMS Headwall Concrete and Reinforcement Limits	1				-0-	SFMTA initiated change to revise UMS headwall concrete and reinforcement limits to provide a cost savings. 08/29/12 - Revised PCC presented to CMB	
1252-0042	08/24/12	31	Retrieval Shaft - Unmarked 12" Steel Pipe	2	CMB-0108	Agree to Merit Agree to Cost	1/2/2013 2/27/13	+1,200	While performing excavation for the joint trench at the retrieval shaft, BIH's subcontractor Synergy Project Management (SPM) discovered an abandoned 12" dia. steel pipe which appears to run continuously under Columbus Ave. 01/02/13 - Merit info presented to CMB - Agreed 2/27/13 - Cost presented to CMB - agreed to NTE \$1,250 04/01/13 - No reimbursement from SFWD. COMPLETE, NO FURTHER ACTION: CMOD #13	CMOD #13 Approved
1252-0043	08/31/12	32	LB - Panel W29 Wood Pile	2		Agree to Merit	11/28/12	+81,000	While performing excavation for the slurry walls at the TBM Launch Box, BIH's subcontractor CJA-NCC unexpectedly encountered what appeared to be a wood pile within Launch Box Diaphragm Wall Panel W-29. 11/28/12 - Merit info presented to CMB - Agreed	
1252-0044	09/04/12	33	Ret Shaft - 20" Water Line Conflict	2		Agree to Merit	11/14/12	-0-	During excavation for the joint trench at the retrieval shaft SPM discovered that the MRY Duct Bank is directly above an active 20" low pressure waterline. As a result the work at the Retrieval Shaft must reportedly be altered. 11/14/12 - Presented to CMB, the CMB agrees with some merit of this COR, element of change not define clearly. RE will proceed with scoping the work.	
1252-0045	09/11/12	34	LB - Panel W28 Wood Pile	2				+81,000	While performing excavation for the slurry walls at the TBM Launch Box, BIH's subcontractor CJA-NCC unexpectedly encountered what appeared to be a wood pile within Launch Box Diaphragm Wall Panel W-28.	
1252-0046	01/00/00	36	MOS - Catch Basin @ south headwall	3				-0-	A catch basin on the NW corner of 4th and Folsom interfered with the construction of the MOS Headwalls. Removal of the catch basin was needed in order for the headwalls to be built. BIH claims the catch basin was not shown in the contract drawings. Closed due to improper notification.	
1252-0047	01/00/00	37	UMS - 12" steel pipe removal	3		Agree to Merit	6/5/13	+3,500	A 12" steel pipe at the north UMS headwall location interfered with the demolition of other existing UMS utilities. BIH claims the steel pipe was not shown in the contract drawings. Closed due to improper notification. 06/05/13 - Merit info presented to CMB - Agreed	
1252-0048	09/07/12	39	MOS - 16" Steel Pipe removal @ northeast headwall	3	CMB-0102	Agree to Cost	1/23/13	+2,952	SPM removed an unmarked 16" steel pipe from the excavation of the NE MOS Headwall. Construction of the MOS headwall could not commence without this utility being removed. BIH claims the 16" steel pipe was not shown in the contract drawings. 01/23/13 - CMB - NOT TO EXCEED Amount \$2,951.99 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. COMPLETE, NO FURTHER ACTION; CMOD #14	CMOD #14 Approved
1252-0049	01/00/00	40	MOS - Extra Road Base @ northeast headwall	2				-0-	BIH's subcontractor Synergy Project Management (SPM) discovered what they believe to be an additional layer of road base under the existing asphalt at the NE MOS headwall location. The road base in this location was approx. 20" thick on a patch that was roughly 25' x 25'. Closed due to improper notification.	

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1252-0050	05/18/12	41	LB - Impacts due to live PG&E electrical lines (Synergy)	6	CMB-0098	Agree to Merit Agree to Cost	1/16/13	+1,217	Based on direction from the SFMTA, Synergy provided a full time flagger for one night shift of work @ the intersection of I-80 off-ramp near 4th/Bryant streets in order to assist PGE while they performed the abandoning of the existing live PGE electrical at the Launch Box location which was shown to be dead in the contract drawings. 01/13/13 - CMB agreed with Evaluation of Merit 01/16/13 - CMB - NOT TO EXCEED Amount \$1,245 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. COMPLETE, NO FURTHER ACTION: CMOD #10	CMOD #10 Approved
1252-0051	01/00/00	executed	Contract Amendment No. 2	1				(3,040,713)	COMPLETE, NO FURTHER ACTION: CMOD #2	CMOD #2 Approved
1252-0052	08/17/12	42	LB - Panel W25 Wood Pile	2		Agree to Merit	11/28/12	+116,785	While performing excavation for the slurry walls at the TBM Launch Box, BIH's subcontractor CJA-NCC unexpectedly encountered an obstruction within Launch Box Diaphragm Wall Panel W-25. 11/28/12 - Evaluation of merit presented - CMB agreed	
1252-0053	08/22/12	43	LB - Panel W30 Wood Pile	2		Agree to Merit	11/28/12	+47,185	While performing excavation for the slurry walls at the TBM Launch Box, BIH's subcontractor CJA-NCC unexpectedly encountered an obstruction within Launch Box Diaphragm Wall Panel W-30. 11/28/12 - CMB agreed with Evaluation of Merit	
1252-0054	09/19/12	44	MOS - 16" steel pipe removal @ southeast headwall	3	CMB-0109	Agree to Merit Agree to Cost	12/06/12 2/27/13	+1,600	SPM removed an unmarked 16" steel pipe from the excavation of the NE MOS Headwall. Construction of the MOS headwall could not commence without this utility being removed. BIH claims the 16" steel pipe was not shown in the contract drawings. 12/06/12 - CMB agreed with Evaluation of Merit 02/27/13 - Cost presented to CMB - agreed to NTE \$1,800 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. COMPLETE, NO FURTHER ACTION: CMOD #14	CMOD #14 Approved
1252-0055	06/09/12	45	Ret Shaft - Credit for reduced AT&T conduits	6				+331	Credit to install 2 less conduits in the AT&T infrastructure at the Retrieval Shaft Joint Trench. The conduit notes on page JT-701 of the contract documents require 8 EA 4" conduits as laid out on the AT&T line. The reduced materials would result in 6 EA 4" conduits as a result. 04/01/13 - CM team must obtain in writing from AT&T a utility agreement for cost reimbursement.	
1252-0056	09/25/12	46	UMS - Unmarked steel pipes (6", 12", 14") / Duct Bank	6	CMB-0103	Agree to Merit Agree to Cost	12/06/12 1/23/13	+7,475	While performing excavation for the utility demolition at the south UMS headwall BIH/Synergy discovered a 6" dia. steel pipe, 16" dia. steel pipe, 18" dia. steel pipe, and a concrete duct bank. BIH claims the utilities were not shown in the contract drawings. 12/206/12 - CMB agreed with Evaluation of Merit 01/23/13 - CMB - NOT TO EXCEED Amount \$7,461	
1252-0057	09/25/12	47	Ret Shaft - unmarked 10" steel pipe/duct bank	6	CMB-0104	Agree to Merit Agree to Cost	12/06/12 1/23/13	+2,108	Synergy Project Management (SPM), discovered an abandoned duct bank and 10" steel pipe which traveled underneath Columbus Avenue between Powell and Union Streets. Removal of the duct bank was necessary to complete the construction of the 36" RCP sewer main relocation work in this area. The status of the removal of the steel pipe is uncertain and may have not been necessary for the sewer main relocation. 12/06/12 - CMB agreed with Evaluation of Merit 01/23/13 - CMB - Not to Exceed \$2,201 04/01/13 - No reimbursement from SFWD COMPLETE, NO FURTHER ACTION: CMOD #13	CMOD #13 Approved
1252-0058	09/28/12	48	UMS - 3'x5' brick sewer at south headwall	6	CMB-0114	Agree to Merit Agree to Cost	12/06/12 03/13/13	+11,675	Synergy Project Management (SPM), discovered a brick sewer within the utility demolition limits at the UMS South headwall. The sewer encountered was larger than the 12" pipe which was shown in the contract drawings. Removal and disposal of the sewer commenced on September 26th and was completed by September 28th. Removal of the sewer was necessary for headwall construction. 12/06/12 - CMB agreed with Evaluation of Merit 03/13/13 - CMB - NOT TO EXCEED Amount \$12,474.65	
1252-0059	01/00/00	49	Ret Shaft - Tree trimming debris haul off	7				-0-	Synergy Project Management (SPM), was requested by the SFMTA to remove and dispose of debris near the Northeast corner of Columbus Avenue and Union Street. The debris resulted from tree trimming activities performed by MUNI operations. CLOSED; NO FURTHER ACTION.	

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1252-0060	10/10/12	50	Ret Shaft - 12" steel pipe removal at Union/Columbus	6	CMB-0110	Agree to Merit Agree to Cost	12/12/12 02/27/13	+2,200	On October 4th 2012, the Contractor discovered and removed a 12" Steel pipe at the intersection of Union Street and Columbus Avenue. Removal and disposal of the steel pipe was necessary in order to complete the proposed sewer re-location work near the Retrieval Shaft. Sewer work included the construction of a manhole which had been positioned in the field to avoid an existing MUNI duct bank. 12/12/12 - CMB agreed with Evaluation of Merit 02/27/13 - Cost presented to CMB - Agreed to NTE \$2,600 04/01/13 - No reimbursement from SFWD COMPLETE, NO FURTHER ACTION: CMOD #13	CMOD #13 Approved
1252-0061	10/11/12	51	Ret Shaft - Elevation design conflict (30" low psi water / sewer)	3		Agree to Merit	3/13/13	+24,075	03/13/13 - Merit info presented to CMB - Agreed	
1252-0062	09/07/12	53	Chinatown - Night drilling for instrumentation	7				-0-	Closed.	
1252-0063	-	54	LB Class 2 Hazardous Material Haul Off (Phase 2)	2				+110,967	BIH trucking costs for Class 1 haul off generated at staging yard. Also see COR 030 regarding Class 1 Haz material haul off for Phase 1.	
1252-0064	10/10/12		PCC No. 4 - Ret shaft sewer pipe material change	1				-0-	Change approximately 65 ft. of pipe for sewer bypass around the TBM Retrieval Shaft from 3' X 5' oval glass fiber reinforced pipe to a 48-inch (OD) HDPE SD-17 pipe with casing insulator placed inside a 54-inch diameter, ½-inch thick steel casing pipe. In addition, delete permanent sheet pile wall that was to have been placed along the west side of the trench to facilitate future maintenance access to the sewer along the shaft.	
1252-0065	12/18/12	57	PCC No. 8 - UMS - Tree Removal	1				-0-	SFMTA is directing removal of a tree at the northeast corner of Stockton and O'Farrell St. CLOSED; NO FURTHER ACTION.	
1252-0066	10/10/12		PCC No. 5 - Ret Shaft Water Dist. Pipe Material Change	1				-0-	Change material type of approximately 188 linear feet of 20-inch water line and approximately 125 linear feet of 30-inch water line from Welded Steel Pipe (WSP) to Ductile Iron Pipe (DIP) with TR-Flex joints. Install 6-inch thick concrete cap over 20-inch water line where cover is less than 24 inches.	
1252-0067	10/26/12	58	Ret Shaft - Additional 10" steel pipe removal	6	CMB-0111	Agree to Merit Agree to Cost	12/12/12 02/27/13	+700	12/12/12 - CMB agreed with Evaluation of Merit 02/27/13 - Cost presented to CMB - Agreed to NTE \$860.85 COMPLETE, NO FURTHER ACTION: CMOD #13	CMOD #13 Approved
1252-0068	10/10/12		PCC No. 3 - General Detour Signage	1	CMB-0113	Agree to Cost	02/27/13	-0-	Full closure of Stockton Street at Union Square was not anticipated under the 1252 Contract. The implementation of detour signage associated with the full closure of Stockton Street was to occur under the 1300 Contract. After the award of the 1252 Contract it was decided that a full closure of Stockton could be implemented to help reduce the headwall construction durations. As a result of the full closure the City Traffic Engineer has requested that a portion of the 1300 detour signs be installed now under the 1252 Contract to help alleviate traffic congestion and provide better detour signage around Union Square. 10/03/12 - PCC presented to CMB. CMB directed the RE to get a price quote from DPT do the work as a price comparison. This item will be brought back to the CMB at a later date. 11/14/12 - PCC presented to CMB. The CMB did not agree that Central Subway needed to take the lead on implementing detour signage, but suggested that the RE study the traffic activity and at the end of November report findings. 02/27/13 - Cost presented to CMB - Agreed to NTE \$6,152.60	
1252-0069	11/08/12		PCC No. 6 - Supplemental instrumentation of BART tunnel lining.	1				+657,884	Implement additional instrumentation of BART tunnel lining: (A) Existing bolt preload force – Determine the actual load in 12 bolts. (B) Bolt force sensor – Install 12 bolt force sensors at bolts for a total of 24 bolts. (C) Rail movement under dynamic train loading – Install dynamic strain gauges (2 per rail, 8 per tunnel). 10/31/12 - PCC presented to CMB for merit. CMB directed RE to proceed with obtaining a price quote from Contractor to perform the work.	
1252-0070	01/00/00	unsubmitted	Full Closure of 4th Street	1				TBD	SFMTA allowed full closure of 4th Street in order to facilitate construction of Launch Box Slurry Wall Construction Week Ending 11/04/12 - 11/11/12.	
1252-0071	11/07/12	61	LB - Panel P-17 Buried Obstruction	2		Agree to Merit	1/2/13	+1,473	CJA-NCC discovered an unmarked buried water main at Panel P-17, approximately 6' below the guidewall at CTL Station 165+52, while performing slurry diaphragm wall excavation. 01/02/13 - CMB agreed with Evaluation of Merit.	

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1252-0072	11/14/12	62	LB – Panel P-9 Buried Obstruction	2		Agree to Merit	1/23/13	+150,000	CJA-NCC discovered a buried obstruction while excavating slurry diaphragm wall panel P-9 at the Launch Box location. 01/02/13 - Presented to CMB for merit, CMB did not agree. CMB suggested inspector's daily tags as backup or reference the tag numbers in the write-up when force account work is involved. 01/23/13 - Revised write up presented CMB agreed with Evaluation of Merit.	
1252-0073	11/08/12	63	UMS - 48" Steel Pipe @ southwest headwall	2		Agree to Merit	1/23/13		BIH/Synergy encountered a buried 48" Diameter steel pipe during work at the UMS S. Headwall. The pipe is located 6 feet from the south UMS headwall and 5 feet from the face of the curb. 01/02/13 - Presented to CMB for merit, CMB did not agree. CMB suggested inspector's daily tags as backup or reference the tag numbers in the write-up when force account work is involved. 01/23/13 - Revised write up presented CMB agreed with Evaluation of Merit COR has been changed to PCC09.	
1252-0074	11/16/12	65	LB – Panel P-12 Buried Obstruction	2		Agree to Merit	1/23/13	+150,000	CJA-NCC discovered a buried obstruction (water main) while excavating slurry diaphragm wall panel P-12 at the Launch Box location. 01/02/13 - Presented to CMB for merit, CMB did not agree. CMB suggested inspector's daily tags as backup or reference the tag numbers in the write-up when force account work is involved. 01/23/13 - Revised write up presented; CMB agreed with Evaluation of Merit.	
1252-0075	11/19/12	66	Retrieval Shaft - Unmarked 12" Steel Pipe	2	CMB-0112	Agree to Merit Agree to Cost	1/23/13 02/27/13	+3,700	While performing excavation for the joint trench at the retrieval shaft BIH's subcontractor Synergy Project Management (SPM) discovered an abandoned 12" dia. steel pipe which appears to be a Low Pressure Water Main. 01/23/13 - Merit info presented to CMB - Agreed 2/27/13 - Cost presented to CMB - Agreed to NTE \$3,800 04/01/13 - No reimbursement from SFWD COMPLETE; NO FURTHER ACTION: CMOD #13	CMOD #13 Approved
1252-0076	11/19/12	67	LB - Panel P-10 Buried Obstruction	2		Agree to Merit	1/23/13	+18,000	CJA-NCC discovered a buried obstruction or concrete encased clay sewer pipe, approximately 7' below the top of the guide wall while excavating slurry diaphragm wall panel P-10 at the Launch Box location. 01/02/13 - Presented to CMB for merit, CMB did not agree. CMB suggested inspector's daily tags as backup or reference the tag numbers in the write-up when force account work is involved. 01/23/13 - Merit info presented to CMB - Agreed	
1252-0077	12/21/12		PCC No. 7 - BART Temporary Annex Wall	1	CMB-0106	Agree to Cost	1/30/13	+15,500	SFMTA is requesting Contractor construct temporary wall for Bart Annex for SFMTA use. 01/30/13 - NOT TO EXCEED Amount \$15,380 COMPLETE; NO FURTHER ACTION: CMOD #11	CMOD #11 Approved
1252-0078	12/28/12		PCC No. 9 - UMS Headwall - Unknown Buried Pipe	1	CMB-0100	Agree to Merit Agree to Cost	1/16/13 1/16/13	+53,563	Related to COR 063. SFMTA instructed BIH to perform investigation into the 48" diameter steel pipe as it is a Differing Site Condition. 01/30/13 01/16/13 - CMB - NOT TO EXCEED Amount \$75,000 04/01/13 - CM team must obtain in writing from AT&T a utility agreement for cost reimbursement.	CMOD #4 Approved

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1252-0079	01/09/13		PCC No. 10 - Relocation of TBM Retrieval Shaft	1					Modify Bid Item ST-4 Construction of Base Bid Permanent Retrieval Shaft to relocate the TBM retrieval Shaft to 1731-1741 Powell Street - 01/09/13 - Draft PCC10 presented to CMB for Contractor Design Activity to be paid on Force Account. NTE value \$20K presented to CMB as a draft copy, final copy distributed to the Contractor increased to \$50K. 02/20/13 - COST IS NOT REFLECTED HERE - SCOPE IS FUNDED BY OTHER CPT 690 \$\$\$\$ 03/06/13 - Cost proposal received from BIH. SFMTA will develop a detail estimate of what is believed to be the scope of work. 03/13/13 - PCC 10 presented for CMB change approval of \$50K NTE amount to perform design activities. CMB requested CM team return with a more detailed estimate, Contractor's cost proposal of the bulleted items as listed on the PCC and backup documentation as evidence of work completed by the Contractor.	
1252-0080	12/19/12	0	Segment Measuring Frequency	7				(1,300,000)	RFI 203.1 - In light of gauging frequency, and the difficulties of erecting a ring with cast in gaskets, BIH JV alternately proposes to assemble a ring on the Master ring at an interval of every 500th ring cast. The assembled ring would be comprised of randomly selected segments, or segments chosen by the Engineer.	
1252-0081	09/09/12	0	F21 and Old Navy Instrumentation Proposal	7				-0.	Wang Technology is proposing instrumentation changes to the Liquid Level Sensors and Tilt Beams in the basements of Old Navy and Forever 21.	
1252-0082	01/14/13	68	UMS – AT&T Vault Walkway @ N Headwall	2		Agree to Merit	1/16/13	+2,000	SPM discovered an walkway at the UMS north headwall. This walkway is located at the UMS north headwall and connects the south AT&T vault, located within the utility demolition limits, to an north AT&T, located outside of the utility demolition limits. SPM proposes to construct 3'x5' wall to the extents necessary, inside this walkway. The wall will be constructed of plywood and necessary reinforcement in order to create a bulkhead that will isolate the walkway and north vault from the south vault. 01/16/13 - CMB recommendation Contractor/Sub-contractor proceed on Force Account with a "Not to Exceed Amount" \$2K. 04/01/13 - CM team must obtain in writing from AT&T a utility agreement for cost reimbursement.	
1252-0083	11/08/12	69	UMS - Shoring Impacts due to 48" ATT Pipe at SW Headwall	2				+45,715	On or about 10-25-13, BIH/Synergy were installing sheet piles at the UMS SW head wall to demolish the existing basement wall in conflict with the head wall construction per contract requirements. During the driving of the sheet piles, Synergy struck a buried object which was later identified as a 48" Diameter steel pipe. See PCC 9 and 11. Synergy thought they were hitting a buried tie-back which was shown on the contract drawings. Synergy did not know at the time that they were hitting a buried 48" pipe. After hitting the buried object, Synergy installed additional sheet piles and enlarged the excavation area in order to clear the buried object and drive the sheet piles to the minimum embedment depth. Due to the larger excavation, Synergy is claiming impacts as a result of encountering this DSC including shoring revisions and for installing additional sheets. 06/05/13 - CM TEAM NOTES: COR 069 reopened by BIH to cover cost for enlarging excavation due to	
1252-0084	01/24/13		PCC No. 11 - UMS – Cap and Remove 48" Steel pipe	1	CMB-0105	Agree to Cost	1/23/13	+70,999	BIH/Synergy encountered a buried 48" Diameter steel pipe during work at the UMS S. Headwall. The pipe is located 6 feet from the south UMS headwall and 5 feet from the face of the curb. This PCC is to complete removal of pipe @ S. Headwall and Investigate and remove pipe if necessary @ the North headwall. 01/30/13 - CMB - NOT TO EXCEED Amount \$150,000 04/01/13 - CM team must obtain in writing from AT&T a utility agreement for cost reimbursement. COMPLETE NO FURTHER ACTION: CMOD #9	
1252-0085	01/24/13	71	LB – Ramp Excavation Obstructions	2		Agree to Merit	4/3/13	+10,000	BIHJV encountered numerous groups of wood piles while performing excavation of the Launch Box ramp. The wood piles are approximately 16"+ in diameter and are located along the north and south perimeter walls of the Launch Box ramp excavation area. 01/30/13 - CMB did not agree with merit of change suggestion the RE negotiate w/Contractor for another concession. 04/03/13 - COR merit evaluation presented to CMB again due to a greater number of obstruction encountered. CMB - Agreed to Partial Merit	
1252-0086	02/08/13	72	Ellis Shaft Jet Grout Obstruction	2				-0.	On February 6th, 2013, CJA/NCC was drilling column R3 of the Ellis Shaft and reported hitting an obstruction 52ft below existing grade. The obstruction reportedly caused the jet grout monitor to shear off. At this time it is not known what the obstruction is or if it caused the monitor to be sheared off.	

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1252-0087	02/14/13	73	MOS - 16" Pipe removal @ NE Headwall	2	CMB-0118	Agree to Merit Agree to Cost	3/13/2013 05/08/13	+940	BIH-JV's subcontractor, Synergy, encountered an abandoned 16" pipe while installing the 12" waterline at the north headwall of Moscone. The pipe was at the same elevation as the new 12" waterline and therefore needed to be removed to facilitate the installation of the new 12" waterline. 03/13/13 - Merit info presented to CMB - Agreed 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. 05/08/13 - CMB agreed to cost of reconciliation of change on FA - NTE \$989.00	
1252-0088	NA	74	Whole Foods - Unknown brick obstruction in Comp Grout Trench	0				-0-	BIH has decided not to submit a COR as production was not significantly impacted.	
1252-0089	03/11/13	75	Hazardous Waste Disposal Denial - Spoils Containment Cost Impacts	7				+326,971	BIH submitted change order request (COR) 030 seeking reimbursement for the demonstrated additional costs of excavating, handling and disposing of excavated material found to contain hazardous material to a permitted landfill over that which can be disposed of at an unregulated dump site. The SFMTA refused reimbursement for such costs under the hazardous waste bid item allowance. As a result of this determination, BIH is modifying its approach to the construction and handling operations of tunnel spoils generated during tunneling operations.	
1252-0090	03/08/13	76	MOS - 16" Pipe removal @ SE Headwall	2	CMB-0119	Agree to Merit Agree to Cost	3/20/2013 05/08/13	+1,116	BIH-JV's subcontractor, Synergy, encountered an abandoned 16" pipe while installing the 12" waterline at the south headwall of Moscone. The pipe was at the same elevation as the new 12" waterline and therefore needed to be removed to facilitate the installation of the new 12" waterline. 03/20/13 - Merit info presented to CMB - Agreed 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. 05/08/13 - CMB agreed to cost of reconciliation of change on FA - NTE \$1,116.00	
1252-0091	03/06/13	77	Comp Grout - WFB-5 Drilling Obstruction	2		Agree to Merit	4/3/13	+10,000	On March 06,2013, BIHJV encountered a potential Differing Site Condition (DSC) while drilling the compensation grouting hole WFB-5 located at Whole Foods and more precisely at CTL Station 160+38. This potential DSC consists of an obstruction located at a depth of 9' below grade which prevented CJA-NCC from drilling any further. 04/03/13 - Merit info presented to CMB - Agreed	
1252-0092	03/07/13	78	Comp Grout - WF & ATT	2			3/20/13	-0-	BIHJV encountered multiple potential Differing Site Conditions (DSC's) of varying dimensions while excavating the compensation grouting trench for the drill holes located at Whole Foods and AT&T. 03/20/13 - Merit info presented to CMB - Agreed to No Merit for this change. 05/24/13 - CM Team update - Contractor no longer pursuing due to failure to provide proper notification.	
1252-0093	03/07/13	79	Comp Grout - Old Navy	2	CMB-116	Agree to Merit Agree to Merit (addl) Agree to Cost	3/20/2013 04/24/13 04/26/13	+269,720	BIHJV encountered a potential Differing Site Condition (DSC) while sawcutting the pavement for the compensation grouting holes located at Old Navy. The original design was to drill the compensation grout holes between two existing PG&E lines. It was discovered in the field that there is not enough clearance between the existing PG&E lines for the original design to be possible and multiple other potential DSCs were also encountered including, but not limited to, tile duct banks, concrete of unknown contents, and steel pipes. 03/20/13 - Merit info presented to CMB - Agreed 04/01/13 - CM team must obtain in writing from PG&E a utility agreement for cost reimbursement. 04/24/13 - Addl info presented for rationale for Contractor to demob and remob drill rig due to insufficient clearance between existing PG&E lines to drill the grout holes - Agreed 04/26/13 - COR presented at a special mtg to notify CMB of work needed to have Contractor perform work on FA4 for a NTE amt of \$70K.	
1252-0094	03/12/13	80	MOS - Asbestos Wrapped 20" water line @ Headwalls	2		Agree to Merit	4/3/13	+43,000	BIHJV's subcontractor, Synergy Project Management was performing the installation of low pressure water distribution piping at the Moscone north headwall and discovered an unknown material wrapping around the existing 20" waterline which appeared to contain asbestos material. 04/03/13 - Merit info presented to CMB - Agreed	
1252-0095	03/11/13	81	12" Waterline Tee Connection Location (MOS South)	2		Agree to Merit	4/3/13	+3,001	BIHJV's subcontractor, Synergy Project Management, encountered a potential Differing Site Condition (DSC) upon exposing the tie-in point for the 12" waterline at the south headwall of Moscone. The potential DSC consists of two 45 degree elbows in the existing 12" waterline at the point of the tee connection. SFWD has surveyed the field conditions and indicated that the tee connection will need to be moved to the west. 04/03/13 - Merit info presented to CMB - Agreed	

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1252-0096	03/13/13	82	MOS 20" Conflict with 16" AWSS @ South Headwall	2				+50,000	Upon exposing the existing 20" waterline at the south headwall of MOS, in preparation for installing the new 20" waterline shown on WD-403 and amended by RFI 0155, BIHJV's subcontractor Synergy Project Management encountered a potential DSC. The potential DSC consisted of a crossing conflict between the new 16" AWSS, the new 20" waterline, and the existing 6" PE gas. These three utilities cross at multiple locations however the existing depths are roughly the same.	
1252-0097	03/19/13	83	Ellis Shaft Concrete Obstruction	2		Agree to Merit	4/17/13	+4,000	During excavation for the Ellis Street Shaft, BIHJV's subcontractor, CJA-NCC, encountered a concrete obstruction at approximately 10 feet below grade. CJA-NCC spent 4 hours removing the obstruction. 04/17/13 - Merit info presented to CMB - Agreed	
1252-0098	03/16/13	84	Crate & Barrel Water Intrusion	2				+50,000	Water was discovered seeping into the Crate & Barrel basement in concert with secant pile installation at Pile #SRV-A. At the time, pile excavation was approximately 90 feet deep. BIHJV's subcontractor, CJA-NCC, stopped pile installation and placed lean concrete within the pile excavation. This allowed them to continue secant pile construction on other piles until which time SFMTA could provide appropriate mitigation for the water leak.	
1252-0099	04/05/13		PCC No. 12 - Modifications to Tunnel Alignment at Market Street	1			4/3/13	+292,000	IFB Tunnel Profile conflicts with micro-piles installed under Contract 1251. Tunnel Profile needs to be revised (lowered) such that TBM and Tunnel structure is below previously installed micro-piles. Contractor shall revise compensation grouting design to conform to Contract requirements given the revised tunnel alignment. 04/03/13 - CMB agreed to Micropile profile change being forwarded to the Contractor. 04/10/13 - Updated alignment was presented superseding 04/03/10 version - (lowered 4.9%) 04/10/13 - Original Contractor submittal needs to be evaluated to verify the new alignment has been incorporated.	
1252-0100	03/26/13	85	SFWD Impacts at MOS North	2				+345,600	BIHJV's subcontractor CJA-NCC is in the process of constructing the Moscone Station headwalls, the sequence of construction is such that it requires the 12" and 20" waterlines on the eastside of 4th street to be rerouted prior to completing the headwalls. In accordance with the Contract the San Francisco Water Department (SFWD) is responsible for completing the waterline reroutes (chlorination and tie-in) in a specified duration. BIHJV claims the SFWD failed to complete this work in the contractual durations. As a result, BIHJV claims the headwall construction operation has been incurring stand by costs. 04/10/13 - Viewed at CMB during review of Trend/Change Log COR's. CM team contends this is not a valid change and will reject the Contractor's claim of merit.	
1252-0101	01/28/13	87	Cross Passage 5 - Value Engineering Change Proposal	5				(2,674)	As indicated in Contract Drawing No. ES-144, ground treatment for the excavation of CP5 was anticipated to be performed using jet grouting methods. BIHJV proposes to perform ground freezing from within the tunnels as an alternative ground treatment methodology, thereby avoiding the need to disrupt and impact the public by occupying 4th Street for an extended period. 04/24/13 - Viewed at CMB during review of Trend/Change Log COR's. CMB suggested a credit from the Contractor should be received for Bid Item ES-5. In addition suggested a minimal review of the BIH's proposed ground freezing method be done by the PB.	
1252-0102	04/09/13	86	UMS - Secant Pile SRV-J Obstruction DSC	2				-0-	BIHJV's subcontractor, CJA-NCC, encountered a potential Differing Site Condition (DSC) while drilling for UMS Secant Pile #SRV-J. The DSC consists of what appears to be an abandoned utility pipe located approximately 10 feet deep. 04/24/13 - Information presented to CMB during review of Trend Log. CM Team states COR merit will be rejected due to notification of change not met in timely manner per the contract docs. 05/24/13 - CM Team update - Contractor no longer pursuing due to untimely notification of event.	
1252-0103	04/18/13	88	SFWD Impacts at MOS South	2				+383,800	BIHJV's subcontractor CJA-NCC is in the process of constructing the Moscone Station headwalls, the sequence of construction is such that it requires the 12" and 20" waterlines on the eastside of 4th street to be rerouted prior to completing the headwalls. In accordance with the Contract the San Francisco Water Department (SFWD) is responsible for completing the waterline reroutes (chlorination and tie-in) in a specified duration. BIHJV claims the SFWD failed to complete this work in the contractual durations. As a result, BIHJV claims the headwall construction operation has been incurring stand by costs. 05/08/13 - Information presented to CMB during review of Trend Log. CM Team states COR will be rejected due to no merit. This item is not on the critical path.	

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1252-0104	06/06/13	PCC 13	SFWD Excavation, Support and Backfilling	1				+10,000	work by the San Francisco Water Department. Installation of valves are to facilitate fire service connection cutover and cutting off of the existing water main. The existing water main is in the footprint of the UMS station piles and is to be abandoned. 06/05/13 - PCC #13 Presented at CMB Mtg #128. CMB agreed w/change. Wrk must be completed by	
Contract 1252 Totals								+1,211,227	Total Changes and Potential Change (trends) to the contract are currently \$1,211,227 Out of Scope Changes to the Contract for Relocation of TBM (Trend 79, PCC 10) to be paid out of CPT690, funded separately. Not include in total above and being tracked separately from original Program Costs of \$1.5783B	
CONSTRUCTION TRENDS (Based on #1252 Trending Log and CMod) - SUBTOTALS:								+1,211,227	The \$1,211,227 represents an UNDERRUN of allocated contingency. Current Balance = (33,658,463 - 1,211,227 = \$32,447,236) This does not include potential reimbursements from utilities	
DESIGN TRENDS										
FD-001	07/31/10	084-0001	Narrowing of Platform at UMS	1	CMB-0006	Agree	08/04/10	(800,000)	[In "Current Forecast" - Sep2010 Cost Report.]	
FD-002	07/12/10	084-0002	Redux of Station Emergency Ventilation Fans (UMS)	1	CMB-0009	Agree (Conditionally)	08/25/10	(2,000,000)	Further actions: Designer required to receive Fire/Life/Safety approval before proceeding with this change. Designer to allocate cost impacts by Station (Current figures are "place-holders.". [In "Current Forecast" - Sep2010 Cost Report.]	
FD-002	07/12/10	085-0001	Redux of Station Emergency Ventilation Fans (CTS)	1	CMB-0009	Agree (Conditionally)	08/25/10	(500,000)	Further actions: Designer required to receive Fire/Life/Safety approval before proceeding with this change. Designer to allocate cost impacts by Station (Current figures are "place-holders.". [In "Current Forecast" - Sep2010 Cost Report.]	
FD-002	07/12/10	086-0001	Redux of Station Emergency Ventilation Fans (MOS)	1	CMB-0009	Agree (Conditionally)	08/25/10	(500,000)	Further actions: Designer required to receive Fire/Life/Safety approval before proceeding with this change. Designer to allocate cost impacts by Station (Current figures are "place-holders.". [In "Current Forecast" - Sep2010 Cost Report.]	
FD-003	07/31/10	085-0002	Lowering of CTS	1	To be verified	Info Only	08/25/10	+7,000,000	Presented to CMB for information/consideration.	
FD-004	07/29/10	084-0003	UMS Structural Configuration	1	CMB-0007	Agree	08/18/10	(8,000,000)	[In "Current Forecast" - Sep2010 Cost Report.]	
FD-005	08/22/10	084-0004	UMS Alternate Station Access/Vent Shaft @ Union Square	1	CMB-0010	Agree	12/29/10	(22,500,000)	Documentation to be filed.	
FD-006	08/03/10	084-0005	Station Headwalls - UMS	1	CMB-0008	Agree	08/18/10	-0-		
FD-006	08/03/10	086-0002	Station Headwalls - MOS	1	CMB-0008	Agree	08/18/10	-0-		
FD-007	08/03/10		Cross-passages	1	CMB-0018	Agree	09/15/10	-0-		
FD-008	07/30/10		Trolley Re-route @ Columbus & Powell	1	CMB-0017	Agree	09/15/10	+2,000,000		
FD-009	10/06/10	082-0003	ECP to Modify Sidewalk Vault Demolition and Construction Sequence to Facilitate Construction of Joint Utilities Trench and Future UMS Station	1	CMB-0019	Agree	12/15/10	+3,000,000		

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FD-010	10/06/10	082-0004	Redesign Stockton Street Sanitary Sewer to conform to SFPUC Sewer Criteria that preclude placement of relocated sanitary sewer under sidewalks	1	CMB-0020	Agree	11/17/10	+2,000,000		
FD-011	10/06/10	082-0005	Reconfigure Routing of Water, Sewer and Gas laterals from buildings on north side of Ellis Street	1	CMB-0021	Agree	11/17/10	+500,000		
FD-012	10/06/10	082-0006	Recognize cost transfers from design allowances allocated to contingency to explicit line items in final design estimates	1	CMB-0022	Disagree	11/17/10	-0-		
FD-013	10/06/10	082-0007	Increase Extent of Sub-sidewalks Vault Secondary Closure Walls and Waterproofing	1	CMB-0023	Agree	11/17/10	+4,100,000		
FD-014	10/06/10	082-0008	Include OCS system for rerouting trolley buses to Fifth Street	1	CMB-0024	Agree	12/15/10	+3,100,000		
FD-015	10/06/10	082-0009	Subsidewalk Vault Secondary Closure Walls for Buildings at 800 Market and 838 Market	1	CMB-0025	Agree	11/17/10	+1,000,000		
FD-016	10/06/10	085-0003	Underpinning of Mandarin Tower	1	CMB-0011	over taken by FD-021	01/19/11	+5,000,000	Additional Information Pending.	
FD-017	10/06/10	085-0004	CTS Ground Improvement	1	CMB-0012	over taken by FD-021	01/19/11	+10,300,000	Additional Information Pending.	
FD-018	10/06/10	084-0006	UMS Apple Store Entrance	1	CMB-0013	Disagree	11/10/10	-0-	Additional Information Pending.	
FD-019	10/06/10	086-0003	MOS TOD Configuration	1	CMB-0014	Agree	11/10/10	TBD		
FD-020	10/06/10	084-0007	UMS Emergency Stair #4 Relocation	1	CMB-0015	Agree	11/10/10	TBD		
FD-021	01/14/11	085-0005	CTS Lowering and Stations Reconfiguration	1	CMB-0027	Agree	01/19/11	(18,000,000.00)	Agreement with changes to project configuration only. Additional Information required related to cost and schedule impacts. No agreement on Design cost impact, Design costs directly related to this change to be tracked separately	
FD-022	02/16/11	084-0008	Sewer Replacement along Geary & O'Farrell	1	CMB-0029	Agree	02/23/11	(134,000)	This trend has both an EPC number as well as a Construction Trend Number. See also Construction Trend No. 1251-0003	
FD-023	11/17/10	087-0001	Emergency walkways through crossover cavern changed from outside the main tracks to between the main tracks.	1	CMB-0032	Agree	07/27/11			
FD-024	11/17/10	087-0002	Diamond crossover on surface segment to a tandem (universal) crossover.	1	CMB-0033	Agree	08/24/11	(350,000)		
FD-025	04/13/11	083-0001	Bid Option for TBM Retrieval Shaft		CMB-0034	Agree	04/13/11	(10,000,000)	Will require changes to Bid Schedule to incorporate Traffic, Utilities and Pavement Demo/Restoration as incidental costs to the Bid Option. Estimated DP1 cost impact is approx. 80-100 hrs. of engineering time.	
FD-026	05/31/11	084,085,086	Air Replenishment System for Stations	1	CMB-0040	Agree	07/13/11	+1,245,000	Install air replenishment system at MOS, CTS and UMS to address the requirements of the SFFD Bulletin 5.07. The air replenishment system will be used to fill firefighter's self-contained breathing apparatus during firefighting operations in the three subway stations.	
FD-027	07/21/11	082-0010	1 Stockton street (Apple Store) Secondary Closure Wall		CMB-0043	Agree	08/03/11	+500,000	1) Add secondary closure wall to supplement recently completed primary closure wall built by owner to isolate the sub-sidewalk basement of 1 Stockton Street (Apple Store). 2) Modify position of joint trench (and all associated conduits, ducts etc.) for secondary closure wall to be constructed.	
FD-028	08/03/11	086-0004	MOS Revisions to Emergency Ventilation Requirements		CMB-0052	Agree	09/07/11	+500,000	Changes will extend the date of the 90% (pre-final) submittal by 20 working days and the 100% (final) submittal by 40 working days. CMB did not approve a time extension for 90% or 100% submittal delivery. Project Controls estimated \$321,645.	
FD-029	08/08/11	085-0006	CTS Revisions to Emergency Ventilation Requirements		CMB-0053	Agree	09/07/11	+1,000,000	Changes will extend the date of the 90% (pre-final) submittal by 20 working days and the 100% (final) submittal by 40 working days. CMB did not approve a time extension for 90% or 100% submittal delivery. Project Controls estimated \$411,895.	
FD-030	08/08/11	084-0009	UMS Revisions to Emergency Ventilation Requirements		CMB-0054	Agree	09/07/11	+1,000,000	Changes will extend the date of the 90% (pre-final) submittal by 20 working days and the 100% (final) submittal by 40 working days. CMB did not approve a time extension for 90% or 100% submittal delivery. Project Controls estimated 733,420.	

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FD-031	10/31/11	Accessibility Improvements at the SE corner of Union Square (UMS)							
FD-032	10/31/11	Union Square Entrance Reconfiguration (UMS)							
FD-033	10/31/11	Additional Compensation Grouting due to Tunnel Settlement Alert and Trigger Levels (All Stations)					+21,040,000	ECP dated 11/17/11. ECP being routed for approval.	
FD-034	12/09/11	Operational Performance and Safety Enhancement at 4th and King		CMB-0062	Agree	12/14/11	+450,000	SFMTA Operations requested that the design of the 4th & King tie-in work be expanded to include a number of improvements at the junction involving the turn-back area extending to the scissors crossover at 6th & King. SFMTA Ops confirmed that the requested changes result in an arrangement that is consistent with standard operation practices and therefore does not involve new or unacceptable risks. CS 1553 Memorandum No. 0081. Conditionally agreed upon verification/resolution of Buy America issue for the switches. 3/14/12: "Buy America" issue no longer applicable. Revised ECP element - remove H&K switch machines and replace with hydraulic/120V switch machines such as Nortrak or equal.	
FD-035	06/26/12	Tunnel Air Replenishment System		CMB-0071	Agree	07/11/12	+1,215,000	Design and construct a fire fighter air replenishment system for Central Subway tunnel as described in SF Fire Code 511.2, Bulletin 5.07 except as noted in the Central Subway Request for Approval of Variance for the ARS to SFFD on March 5, 2012, and subsequently approved by SFFD. Design Impact: ROM - \$215K Construction Impact: ROM - \$1M	
FD-036	06/26/12	Surface Segment Sewers - 4th St.		CMB-0072	Agree	07/18/12	+2,200,000	Replace current unreinforced brick crown from 1906 as it may not be able to withstand loading for construction of the CS trackway section. Reconstruct manholes between Brannan and King Streets as existing are in conflict with proposed rail. Replace service laterals and provide sleeves for future replacement. Additional potential scope option of \$1.5M-Sewer and \$2.3M-Force Main may be requested. If so, it will be Cost to SFPUC	
FD-037	08/27/12	Platform Display System Signs		CMB-0077	Agree (Conditionally)	08/29/12	+500,000	Modify Stations (Contracts 1253, 1254 and 1255) and Surface, Track and Systems (Contract 1256) technical specifications and drawings for the Platform Display System (PDS) to match the changes made to the PDS units being supplied under the Integrated Systems Replacement (ISR) Project, Contract No.1260. 08/29/12: CMB agreement condition upon follow up action to evaluate procurement by CN1260	
FD-038	11/7/2012 06/05/13	Elongated Sidewalk Bulb-out at Chinatown Station		CMB-0121	Agree	06/05/13	+36,000	CTS Sidewalk Bulb out (Southwest corner Stockton/Washington To bring the Central Subway Project - Chinatown Station in closer conformance wit the City's General Plan, SF Planning Department in its May, 2012 GPR Letter (recommends) making design changes specifically the extension of the sidewalk bulb-out at Stockton Street to help create a "station plaza". This elongated bulbout on the southwest corner (SWC) of Stockton and Washington Streets in front of the station headhouse would also include bike racks, benches, trees and other landscape features. 06/05/13 - CSDG desinger estimate cost is not a CS expense. Add funding source will need to be establish by San Francisco Planning to support the investigation study Design Impact: Estimated Construction Cost ROM \$36,200 Construction Impact: Estimated ROM 97 Hrs.: DPW Pavement/Sidewalk Design Modification Estimated ROM 60 Hrs.: CSDG bus door/mirror/tree/bike rack coordination	
FINAL DESIGN TRENDS (Based on All ECPs) - SUBTOTALS								Incorporated budget increases have been absorbed by allocated and unallocated funds and the program budget to date remains \$1,578,300,000.	
OTHER TRENDS									

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X51-001	07/31/10	082-0001	1251 Revised estimate, escalation impact and contingency.	7				+9,532,314	Cost Transfer #0033 (Base \$\$), 0033a (Allocated Contingency), 0036 (Base \$\$ Escalation , 0036a (Allocated Contingency Escalation). [Sep2010 Cost Report]	BT-0033, BT-0033-A, BT0036, BT-0036-A
X51-002	07/31/10	082-0002	Form B Credit for 1251 Utilities	7				(7,697,949)	Reserve and Contingencies removed as per communications with PG&E. This is consistent with the 1250 utility agreement. [Cost Transfer #0034 - Aug2010 Cost Report].	BT-0034
X52-001	07/31/10	083-0001	1252-(TUN) Revised estimate, escalation impact and contingency	7				(2,165,462)	Cost Transfer #0035 (Base \$\$), 0035a (Allocated Contingency), 0037 (Base \$\$ Escalation , 0037a (Allocated Contingency Escalation). [Sep2010 Cost Report]	BT-0037, BT-0037-A
X03-001	08/31/10	003-0001	OEWD's Pilot Training Program	7				+75,000	Workforce training pilot program in conjunction with the City/County's Office of Economic and Workforce Development (OEWD). Provides specialized training to SF residents to perform tunneling work. Central Subway financial responsibility being investigated. Requires further clarification	
X53-001	07/18/11	084-0001	CS 155-2 (DP2) CBP4 - Construction Budget Adjustment YOE	7	CMB-0044	Agree	07/27/11		Adjust construction budget in Section 6 of Contract No. CS 155-2 to Year of Expenditure	
X53-002	10/25/11		Change UMS Advertise Date to April 4th, 2012						DP2 Designer's new proposed dates for Pre-Final and Final Design have pushed out Advertise Dates.	
X53-003	04/11/12	084-0002	CN1253 (UMS) Construction Budget Adjustment 90%					+55,720,600	CMB is currently vetting the 90% estimate of \$221,534,723 which represents a \$XX increase from the 65% base amount in 2010\$. Increase would deplete current YOE dollars and require use of unallocated contingency.	
X53-004	06/26/12	084-0002	Approve/Execute/Certify durations change for UMS Contract						Shortened the duration for Approve/Execute/Certify - UMS Contract from 40 to 24days to reflect an optimistic but doable duration for contractor submittals.	
X54-001	07/13/11	085-0001	CS 155-2 (DP2) CBP5 - Construction Budget Adjustment YOE	7	CMB-0045	Agree	07/27/11		Adjust construction budget in Section 6 of Contract No. CS 155-2 to Year of Expenditure	
X54-002	10/25/11		Change CTS Advertise Date to May 23rd, 2012		CMB-0061	Agree	12/28/11		DP2 Designer's new proposed dates for Pre-Final and Final Design have pushed out Advertise Dates.	
X54-003	11/02/11		Change CTS Advertise Date to February 8th, 2012						Change CTS Advertise Date from May 23, 2012 to February 8, 2012 (-74 working days)	
X54-005	12/06/11		Revisions to CTS Construction Schedule		CMB-0060	Agree	12/28/11			
X54-006	12/07/11	085-0002	CS 155-2 (DP2) CBP4 - Construction Budget Adjustment - allowed	7						
X55-001	07/18/11	086-0001	CS 155-2 (DP2) CBP6 - Construction Budget Adjustment YOE	7	CMB-0046	Agree	07/27/11		Adjust construction budget in Section 6 of Contract No. CS 155-2 to Year of Expenditure	
X55-002	10/25/11		Change MOS Advertise Date to June 25th, 2012						DP2 Designer's new proposed dates for Pre-Final and Final Design have pushed out Advertise Dates.	
X55-003	11/02/11		Change MOS Advertise Date to May 23rd, 2012						Change MOS Advertise Date from June 25, 2012 to May 23, 2012 (-22 working days)	
X55-004	06/26/12		Change MOS Advertise Date to Aug 20, 2012						Change MOS to August 20, 2012	
X56-001	10/25/11		Change STS Advertise Date to July 27th, 2012						DP2 Designer's new proposed dates for Pre-Final and Final Design have pushed out the Station's Advertise Dates. STS Advertise Date is staggered from Station's Advertise Dates.	
X56-002	12/14/11		STS Construction Schedule Revision for Equipment Procurement							
X56-003	06/26/12		Change STS Advertise Date to October 1, 2012						Change STS Advertise Date to October 1, 2012	
X40-001	10/19/11	028-0001	SFPUC CDD Updated Budgets	7		Authorized		+505,127	1250 SFPUC CDD Monthly Service Report Final & 1251 SFPUC CDD Estimate	BT-0172, BT-0177

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CENTRAL SUBWAY PROJECT
Project Trend/Change Control Log

Item #	Date Initiated		Change Description	Change Type	CMB No.	Change Status		Projected Cost Impact +Exposure/(Benefit)	Comments	Contract Modification Number
						Approval Action	Approval Date			
X80-001	10/19/11	071-0001	DP1: CS 155-1 Modifications No. 2 & 3	see Prof. Serv		Authorized		+560,585	Modification No. 2: \$395,584.59, Modification No. 3: \$165,000. Individual details listed in Professional Services. See trend 1551-0002,1551-0005 - 1551-0009	BT-0170, BT-0171, BT-0173
X80-002		071-0002	DP1: CS 155-1 Modification No. 4	7		Authorized		+135,898	Modification No. 4: \$135,898 Individual details listed in Professional Services. See trends 1551-0010 - 1551-0014.	BT-0187
X80-003	10/27/11	029-0001	Increase City Auditor Budget	7		Authorized		+50,220	Reduce unallocated Contingency to increase City Auditor budget as authorized in Budget Authorization #51.	BR#51, BT-0181
X80-004	10/27/11	232-0001	Increase SFCTA Budget	7		Authorized		+20,000	Increase SFCTA budget per per Budget Authorization #51 to conduct travel forecast (TEP CEQA Modeling) to satisfy PMO review and SFMTA Fleet Plan light rail vehicle.	BR #51, BT-0182
X80-005	7/1/10, 11/2/10,3/24/11	072-0001	DP2: CS 155-2 Modification No. 1	7		Authorized		+1,010,000	Modification No. 1: \$1,010,600 Individual details listed in Professional Services. See trends 1552-0002, 1552-0003 and 1552-0005.	BT-0194, BR#53
X80-006	5/6/10,7/25/11, 9/22/11	072-0002	DP2: CS 155-2 Exercised Options	see Prof. Serv		Authorized		+366,771	Individual details listed in Professional Services. See trends 1552-0001, 1552-0007 and 1552-0014.	BT-0023, BT-0194
X80-007	11/08/11	151-0001	Operations SFMTA Transit Services			Authorized		+50,000	CSP request services to support and test OCS for 30/45 trolley reroute. Authorized per Budget Revision #53	BR #53, BT-0189
X80-008	09/01/11	081-0001	SFMTA Safety, Training, Security & Enforcement Div PCOs	7		Authorized		+60,000	CSP request PCOs to support 1251 and 1252 construction for one year. Authorized \$60,000 per Budget Revision #45.	BR #45, BT-0190
X80-009	11/30/11	016-0001	SFMTA Sustainable Streets 30 and 45 re-route	7		Authorized		-0-	CSP request services to support 30/45 trolley reroute. \$162,600 was reduced from DPT staffing plan budget.	BT-0188
X80-010	11/09/11	073-0001	DP3: CS 155-3 Modification No. 1	7		Authorized		+152,882	Modification No. 1: \$152,882 Individual details listed in Professional Services. See trends 1553-0012 and 1553-0013.	BT-0203
X80-011	8/9/11, 9/22/11, 10/11/11, 11/9/11	073-0002	DP3: CS 155-3 Exercised Options	see Prof. Serv		Authorized		+1,026,398	Individual details listed in Professional Services. See trends 1553-0001 - 1553-0004, 1553-0006, 1553-0009 - 1553-0011, 1553-0014. Updated from \$971,686 to include Trend 1553-0014.	BT-0203
X80-012		151-0002	OCS materials procured directly for work in reference to CN1251 CMOD#12.		CMB-0065	Agree	1/25/2012, 2/29/12	+126,149	See also Trend 1251-0023	
X80-013	05/30/12	073-0003	DP3: Provide Vibration Impact Analysis and Extend PC Support Technical and CSP Quality Manager	1				+133,906	Amend #2: Vibration Impact Analysis \$38,380 Amend #3: PC Support Tech \$34,483; Quality Manager - \$61,043	TBD
X80-014	12/17/12		For combining the stations and track and systems into Contract 1300 (SSTS)	7					Combined all the Station ad Track and Systems into one Contract. Removed the buffer float activities from the satin to track and systems and overlapped work that can now be done concurrent.	
X80-015	12/17/12		Changing the Bid Opening date and condensing the Contract Duration for Contract 1300	7		Agree	11/14/12		Change bid opening to March 19, 2013 and reevaluated procurement durations, revised calendars for UMS Garage, revised production rates for steel. 11/14/13 CMB Agreed verbally to change @ Mtg #101	
X80-016	02/19/13		Change to Tunnel and CTS interface and buffer float Contract 1300 (SSTS)	7		Agree	01/23/13		Change (MPS) reducing the number of days currently in (from 550 calendar days from NTP to 450 calendar days) for the key interface between CTS and the tunnel completion. Reduced number of days will be issued as part of Add #3 of the 1300 contract. 1/23/13 CMB Agreed verbally to change @ Mtg #111.	
OTHER TRENDS (Based on Various Trends)									Current Forecast reflects a net +3,537,933 unfavorable impact for all Other Trends.	

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CENTRAL SUBWAY PROJECT
Project Trend/Change Control Log

Item #	Date Initiated		Change Description	Change Type	CMB No.	Change Status		Projected Cost Impact +Exposure/(Benefit)	Comments	Contract Modification Number	
						Approval Action	Approval Date				Actual/Forecast
GRAND TOTALS											
Current Forecast reflects an estimated net +66,751,620 unfavorable exposure to the project to date. Incorporated budget increases have been absorbed by allocated and unallocated funds and the program budget to date remains \$1,578,300,000.											
PROFESSIONAL SERVICES TRENDS: The following trends are listed for administrative purposes. The dollar amounts are reflected in the "Other Trends" section of this log in the form of cost (budget) transfers - ie CCOP's. The total values indicated for each professional contract are for tracking all potential cost exposures and may or may not be approved.											
Contract CS-155-1											
1551-0001	07/30/10		071-0001	Conform Contract Terms per Negotiations from 10/2/09 to 11/6/09	7			-0-	To be addressed in Amendment 1.	Amendment 1 Pending	
1551-0002	02/18/11		071-0002	Project Office Delay (Adjustments for OH Rate and Team Productivity impact)	7		Authorized	07/29/11	+101,411	Amendment Signed by ED/CEO 7/29/11 COMPLETE, NO FURTHER ACTION; Amendment #2	Amendment 2 Approved
1551-0003	02/18/11		071-0003	Additional Construction Support for CP-1	7						
1551-0004	02/18/11		071-0004	Redesign Required by Barney's and PUC for CP-2	7						
1551-0005	02/18/11		071-0005	Archeological Monitoring 16.20.C1 (total)	7		Authorized	07/29/11	+197,173	Amendment Signed by ED/CEO 7/29/11 COMPLETE, NO FURTHER ACTION; Amendment #2	Amendment 2 Approved
1551-0006	02/18/11		071-0006	Eyebolts	7		Authorized	07/29/11	+50,000	Amendment Signed by ED/CEO 7/29/11 COMPLETE, NO FURTHER ACTION; Amendment #2	Amendment 2 Approved
1551-0007			071-0007	Secondary Closure Walls 800/838 Market	1		Authorized	07/29/11	+47,400	Amendment Signed by ED/CEO 7/29/11 COMPLETE, NO FURTHER ACTION; Amendment #2	Amendment 2 Approved
1551-0008			071-0008	Delete Optional Tasks - Tasks 1-14	1				-0-	Deleted Options in the amount of (\$529,952) in Amendment has been reversed through Budget Authorization #50. COMPLETE, NO FURTHER ACTION; Amendment #2	Amendment 2 Approved
1551-0009			071-0009	CTS Lowering	1		Authorized	12/08/11	+165,000	Amendment Signed by ED/CEO 12/8/11 COMPLETE, NO FURTHER ACTION; Amendment #3	Amendment 3 Approved
1551-0010			071-0010	Modify Sidewalk Vault Demo ECP-FD009	7		Authorized	12/08/11	+39,311	Amendment Signed by ED/CEO 12/8/11 COMPLETE, NO FURTHER ACTION; Amendment #4	Amendment 4 Approved
1551-0011			071-0011	Modify Stockton St. Sewer ECP-FD010	7		Authorized	12/08/11	+4,112	Amendment Signed by ED/CEO 12/8/11 COMPLETE, NO FURTHER ACTION; Amendment #4	Amendment 4 Approved
1551-0012			071-0012	Reconfigure Utilities N. Side Ellis ECP-FD011	7		Authorized	12/08/11	+2,448	Amendment Signed by ED/CEO 12/8/11 COMPLETE, NO FURTHER ACTION; Amendment #4	Amendment 4 Approved
1551-0013			071-0013	Extend SubSidewalk Closure Wall ECP-FD013	7		Authorized	12/08/11	+85,233	Amendment Signed by ED/CEO 12/8/11 COMPLETE, NO FURTHER ACTION; Amendment #4	Amendment 4 Approved
1551-0014			071-0014	Reroute OCS to Fifth St. ECP-FD014	7		Authorized	12/08/11	+4,794	Amendment Signed by ED/CEO 12/8/11 COMPLETE, NO FURTHER ACTION; Amendment #4	Amendment 4 Approved
1551-0015			071-0015	Retrieval Shaft Redesign - Sewer By Pass	7				+56,000	Awaiting documentation from Designer	
1551-0016			071-0016	Design Measures for Settlement Protection	7				+99,000	Awaiting documentation from Designer	
1551-0017			071-0017	Incorporate Headwalls MOS/UMS	1		Disagree at this time	11/01/11	+42,000	In letter dated November 1, 2011 DPM responded that until concurrence with DP2 on cost transfer or additional documentation, the requested change is denied.	
1551-0018			071-0018	Prepare Utility Composite	-		Disagree	11/01/11	+50,000	DPM responded that Program does not agree in letter dated November 1, 2011.	
1551-0019			071-0019	Incorporate Cross Passages	1		Disagree at this time	11/01/11	+43,000	In letter dated November 1, 2011 DPM responded that until concurrence with DP2 on cost transfer or additional documentation, the requested change is denied.	

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CENTRAL SUBWAY PROJECT
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					Approval Action	Approval Date				Actual/Forecast
Contract CS-155-1 Total								+986,882	Of the total cost exposure shown, Contract Modifications and Authorized Option dollars have been absorbed by DP1 allocated contingency in the amount of \$696,483 of a total budget of \$705,000. Additional potential exposure is +\$391,411. Currently, overall contract value remains unchanged in the amount of \$6,500,000.	
Contract CS-155-2										
1552-0001	05/06/10	072-0001	Optional Task 2.50 and Task 12.07	1		Authorized	05/06/10	+274,775	CS Letter 0473 from ED/CEO Optional Task 2.50 to support art enhancement activities NTE \$164,383 and Optional Task 12.07 to support design structures for Public Art NTE \$110,392.	
1552-0002	07/01/10	072-0002	Expanded Hydraulic Testing at CTS	7		Authorized	07/01/10	+26,100	Estimated cost breakdown is included in the 7/1/10 letter from CSDG and includes signed approval by SFMTA. Amendment No. 1 authorized by ED/CEO on 12/21/11. COMPLETE, NO FURTHER ACTION; Amendment #1	Amendment 1 Approved
1552-0003	10/11/10	072-0003	Additional Soil Borings at UMS Station - Task No. 3.10 Supplemental Investigations	7		Authorized	11/02/10	+34,500	Estimated cost breakdown is included in the 10/11/10 letter from CSDG and includes signed approval by SFMTA. Amendment No. 1 authorized by ED/CEO on 12/21/11. COMPLETE, NO FURTHER ACTION; Amendment #1	Amendment 1 Approved
1552-0004	02/11/11	072-0004	MOS TOD White Paper Proposal	1				+33,847	Conceptual study will assess a potential high rise building at the Moscone station headhouse. Estimated cost breakdown is included in the 2/11/2011 letter from CSDG - On Hold .	
1552-0005	03/24/11	072-0005	CTS - Alternate 5	7		Authorized	03/24/11	+950,000	CS Letter from DED dated 3/24/11 has agreed to a modification of \$950,000 for performing additional work. Amendment No. 1 authorized by ED/CEO on 12/21/11. COMPLETE, NO FURTHER ACTION; Amendment #1	Amendment 1 Approved
1552-0006	06/28/11	072-0006	Audited Overhead Rate Adjustment for Year 2010 - Request for Amendment	7				-0-	Estimated cost of \$406,320 is included in the 6/28/11 letter from CSDG. See 1552-0006 Rev. 1 for most current estimate	
1552-0007	07/25/11	072-0007	REVIT and CAD Support (to City Staff) Option to DP2 Contract (Optional Services Task 12.12)	1		Authorized	07/25/11	+79,200	CS Letter No. 0768 from ED/CEO dated 7/25/11 has authorized Optional Task 12.12 NTE \$79,200.	
1552-0008	08/03/11	072-0008	MOS Revisions to Emergency Ventilation Requirements	-		Provide further Justification	09/07/11	-0-	CMB did not approve a \$60,000 proposed increase. Document References ECP FD028, CSDG letter dated 8/8/11. DPM responded that program does not agree in letter dated 10/17/11.	
1552-0009	08/03/11	072-0009	CTS Revisions to Emergency Ventilation Requirements	-		Provide further Justification	09/07/11	-0-	CMB did not approve an \$80,000 proposed increase. Document References ECP FD029, CSDG letter dated 8/8/11. DPM responded that program does not agree in letter dated 10/17/11.	
1552-0010	08/03/11	072-0010	UMS Revisions to Emergency Ventilation Requirements	-		Provide further Justification	09/07/11	-0-	CMB did not approve a \$100,000 proposed increase. Document References ECP FD030, CSDG letter dated 8/8/11. DPM responded that program does not agree in letter dated 10/17/11.	
1552-0011	08/17/11	072-0011	Audited Overhead Rate Adjustment for Year 2010 - Request for Amendment - Rev. 1	7				+426,322	Estimated cost breakdown is included in the 8/17/11 letter from CSDG. This is a revision to the 6/28/11 letter and cost breakdown.	
1552-0012	08/23/11	072-0012	Station Air Replenishment System	-		Disagree	10/17/11	+36,240	Estimated cost breakdown is included in the 8/23/11 letter from CSDG. DPM responded that program does not agree in letter dated 10/17/11.	
1552-0013	09/01/11	072-0013	COR for Additional Services Related to MOS Constructability Review and Assessment of Alternatives	-				-0-	Cost charged to Task 2.70- part of base work	
1552-0014	09/22/11	072-0014	Proposal for Additional Work to provide Structural Engineering Support (Optional Services)	1		Authorized Optional Task 12.01.C5 only	11/04/11	+35,724	Estimated cost breakdown is included in the 9/22/11 letter from CSDG. Referenced Optional Work Tasks 12.01.C5 and 12.07.C5 - Effort re-evaluated - proposed cost to be resubmitted. CS Letter No. 1232 dated 11/4/11 from Director of Transportation has authorized Optional Task 12.01.C5 for the amount of \$12,796.	Option

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CENTRAL SUBWAY PROJECT
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					Approval Action	Approval Date				
Contract CS-155-2 Total							+1,896,708	Of the total cost exposure shown, Contract Modifications and Authorized Option dollars have been absorbed by DP2 allocated contingency in the amount of \$1,046,324 of a total budget of \$4,890,707. Overall contract value remains unchanged in the amount of \$39,949,959.		
Contract CS-155-3										
1553-0001	08/09/11	073-0001	Authorization to commence optional tasks 9.20J Technical Specifications Item J Facility SCADA	1		Authorized	08/01/11	+259,305	SFMTA letter 0933 from ED/CEO dated 8/9/11 authorizing \$738,787 in optional tasks. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0002	08/09/11	073-0002	Authorization to commence optional tasks 12.05 Architectural Plans	1		Authorized	08/01/11	+257,129	SFMTA letter 0933 from ED/CEO dated 8/9/11 authorizing \$738,787 in optional tasks. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0003	08/09/11	073-0003	Authorization to commence optional tasks 12.13J Facility SCADA Design	1		Authorized	08/01/11	+169,553	SFMTA letter 0933 from ED/CEO dated 8/9/11 authorizing \$738,787 in optional tasks. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0004	08/09/11	073-0004	Authorization to commence optional tasks 12.12E traction power cables for CAD production	1		Authorized	08/01/11	+52,800	SFMTA letter 0933 from ED/CEO dated 8/9/11 authorizing \$738,787 in optional tasks. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0005	09/19/11	073-0005	Proposal to hire Wilson Ihrig to support Acoustics, Noise and Vibration tasks.	7		Authorized	03/08/12	+38,380	Signed by Director of Transportation 3/8/2012. COMPLETE, NO FURTHER ACTION ; Amendment 2	Amendment 2 Approved
1553-0006	09/22/11	073-0006	Proposal to exercise optional task 12.07 Public Art.	1		Authorized	11/04/11	+10,285	Letter dated 9/22/11 to DOM. PMCM response pending. Priced level of Effort assumed for this task. SFMTA Letter 1213 from Director of Transportation dated 11/4/11 authorizing optional task.	
1553-0007	09/27/11	073-0007	Proposal to exercise optional sub-task 12.12 structural design of the OCS attachments.	7				+59,460	Letter dated 9/27/11 to DOM. NOT FOLLOWING CONTRACT REQUIREMENTS FOR PROPER NOTIFICATION OF CHANGE. PMCM has responded to clarify scope in stations and Tunnel Design. Awaiting resubmittal of request by DP3	
1553-0008	10/05/11	073-0008	Scope Clarification - Add Emergency and Location Signage.	7		Disagree A.12.08-Signage	11/09/11		Letter dated 10/5/11 to DOM. NOT FOLLOWING CONTRACT REQUIREMENTS FOR PROPER NOTIFICATION OF CHANGE. CS Memo No. 0882 from DOM to DP3 PM considers A.12.08 Signage as part of the work necessary to complete the trackway elements of the scope. The work in question is considered part of the base scope of services.	
1553-0009	10/11/11	073-0009	Proposal to exercise mechanical optional task A 12.11 to design a water line on the FBS station platform.	1		Authorized	11/04/11	+33,000	SFMTA Letter 1213 from Director of Transportation dated 11/4/11 authorizing optional task. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0010		073-0010	Proposal to exercise Task 12.01 Surface Segment site Drainage	1		Authorized	11/04/11	+108,240	SFMTA Letter 1213 from Director of Transportation dated 11/4/11 authorizing optional task. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0011		073-0011	Proposal to exercise Task 12.02 Sewer Relocation and Analysis Report.	1		Authorized	11/04/11	+81,374	SFMTA Letter 1213 from Director of Transportation dated 11/4/11 authorizing optional task. COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0012	11/09/11	073-0012	CTS - Alternate 5	7		Authorized	01/18/12	+88,855	Contract Modification No. 1 authorized by ED/CEO on 1/18/12 COMPLETE, NO FURTHER ACTION; Amendment #1	Amendment 1 Approved
1553-0013	11/09/11	073-0013	Proposal for Geotechnical Services	7		Authorized	01/18/12	+64,027	Letter dated October 7, 2011 to DOM. PM/CM response pending. Contract Modification No. 1 authorized by ED/CEO on 1/18/12 COMPLETE, NO FURTHER ACTION; Amendment #1	Amendment 1 Approved
1553-0014	02/06/12	073-0014	Authorization to commence optional tasks 12.12E traction power cables for CAD production, Optional task 2.50 Coordination with Art			Authorized	02/06/12	+54,712	SFMTA Letter 1381 from Director of Transportation dated 2/06/12 authorizing optional tasks 12.12e in the amount of \$41,280 and 2.50 for \$13,432 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0015	03/20/12	073-0015	Proposal for additional services in Response to Approved (ECP) FD-034 Operation Performance and Safety Enhancements to 4th and King and 6th and King.	1				+26,026	FD-034 was updated by CMB on 3/14/12. Proposal transmitted via letter CS1553 No. 0012 dated 3/20/12 to DOM. Amendment #4 PENDING	Option
1553-0016	03/23/12	073-0016	Proposal for additional work - Tunnel Air Replenishment System (Exercise Optional Service)	7		Authorized	04/26/12	+173,119	SFMTA letter 1658 from Director of Transportation dated 4/26/2012 authorizing \$173,119 in optional tasks. Included in FD-035 to be presented to CMB on 6/26/12	Option
1553-0017	05/14/12	073-0017	Extension of PC Support Technician	1		Authorized	05/04/12	+34,483	Signed by Director of Transportation 5/14/12 COMPLETE, NO FURTHER ACTION; Amendment #3	Amendment 3 Approved
1553-0018	05/14/12	073-0018	Extension of CSP Quality Manager	1		Authorized	05/04/12	+61,043	Signed by Director of Transportation 5/14/12 COMPLETE, NO FURTHER ACTION; Amendment #3	Amendment 3 Approved

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CENTRAL SUBWAY PROJECT
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1553-0019	04/09/12	073-0019	Authorization to commence optional task 12.02 Utility Plans for design of repositioned and reconstructed manholes for the 78 inch diameter sewer on 4th St.	1	Authorized	04/09/12	+52,712	SFMTA Letter 1630 from the Director of Transportation dated 4/9/2012 authorizing optional task 12.02 in the amount of \$52,712 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0020	04/09/12	073-0020	Authorization to commence optional task 12.12 civil and structural support for traction power duct banks	1	Authorized	04/09/12	+109,000	SFMTA Letter 1630 from the Director of Transportation dated 4/9/2012 authorizing optional task 12.12 in the amount of \$109,000 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0021	04/26/12	073-0021	Authorization to commence optional task 12.11 design of the tunnel based Air Replenishment System including fill panels, feeder pipes, gauges, valves, fire proofing, anchors, supports, low level alarms, wiring, signage, connections to SCADA and monitoring devices, equipment placement, and impact protection	1	Authorized	04/26/12	+173,119	SFMTA Letter 1658 from the Director of Transportation dated 4/26/2012 authorizing optional task 12.11 in the amount of \$173,119 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0022	04/26/12	073-0022	Increased authorization for optional task 12.12.e CAD production support	1	Authorized	04/26/12	+30,000	SFMTA Letter 1658 from the Director of Transportation dated 4/26/2012 authorizing the increase in optional task 12.12e in the amount of \$30,000 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0023	05/21/12	073-0023	Authorization to commence option task 12.11 structural design support for Fire Protection and Plumbing designs	1	Authorized	05/21/12	+39,204	SFMTA Letter 1688 from the Director of Transportation dated 5/21/2012 authorizing the increase in optional task 12.11 in the amount of \$39,204 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0024	05/21/12	073-0024	Authorization to commence optional task 12.12 structural design support for Traction Power and Power/Lighting designs	1	Authorized	05/21/12	+43,638	SFMTA Letter 1688 from the Director of Transportation dated 5/21/2012 authorizing the increase in optional task 12.12 in the amount of \$43,638 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0025	05/21/12	073-0025	Authorization to commence optional task 12.13 structural design support for Telephone and CCTV designs	1	Authorized	05/21/12	+24,328	SFMTA Letter 1688 from the Director of Transportation dated 5/21/2012 authorizing the increase in optional task 12.13 in the amount of \$24,328 COMPLETE, NO FURTHER ACTION; OPTION	Option
1553-0026	05/21/12	073-0026	Authorization to commence optional task 9.20 Technical Specifications for structural design support for above designs	1	Authorized	05/21/12	+3,039	SFMTA Letter 1688 from the Director of Transportation dated 5/21/2012 authorizing the increase in optional task 9.20 in the amount of \$3,039 COMPLETE, NO FURTHER ACTION; OPTION	Option
Contract CS-155-3 Total							+2,046,831	Of the total cost exposure shown, Contract Modifications and Authorized Option dollars have been absorbed by DP3 allocated contingency in the amount of \$1,168,995 of a total budget of \$4,598,725. Overall contract value remains unchanged in the amount of \$19,919,526.	

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CMB LOG

CENTRAL SUBWAY PROJECT - Configuration Management Board Change Control Log

*Dollar value listed for Cost Impact amount reflects the CMB agreed amount, the actual Cost Impact amount may differ as shown on the Project Trend Log to reflect actual final agreed to value. Amount on Trend log will not be greater than agreed CMB amount without prior additional action by CMB.

Initial Implementing Procedure				CMB		
Change Form No.	Title of Change	Schedule Impact	*Cost Impact (X,000)	CMB No.	Agree / Disagree	Final Decision Record Date
1250-0002	Chgs to Sewer on 4th between Howard and Folsom		\$ 107	CMB-0001	Agree	07/12/10
1250-0003	Quantity Adjustment for JT-6 and JT-7		\$ 192	CMB-0002	Agree	08/04/10
1250-0004	Demolition of existing brick and concrete footing on 4th between Howard and Folsom		\$ 170	CMB-0003	Agree	07/12/10
1250-0005	Modify AWSS at 4th/Bryant and 4th/Harrison		\$ 586	CMB-0004	Agree	08/04/10
1250-0006	Install four additional piles and reinforce existing foundation at 401 4th Street		\$ 130	CMB-0005	Agree	08/25/10
FD-001	Narrowing of Platform at UMS		\$ (800)	CMB-0006	Agree	08/04/10
FD-004	UMS Structural Configuration		\$ (8,000)	CMB-0007	Agree	08/18/10
FD-006	Station Headwalls - UMS/MOS		\$ -	CMB-0008	Agree	08/18/10
FD-002	Redux of Station Emergency Ventilation Fans - UMS/CTS/MOS		\$ (3,000)	CMB-0009	Agree (Conditionally)	08/25/10
FD-005	UMS Alternate Station Access/Vent Shaft @ Union Square		\$ (22,500)	CMB-0010	Agree	12/29/10
FD-016	Underpinning of Mandarin Tower		\$ 5,000	CMB-0011	Over Taken by FD-021	01/19/11
FD-017	CTS Ground Improvement		\$ 10,300	CMB-0012	Over Taken by FD-021	01/19/11
FD-018	UMS Apple Store Entrance		\$ -	CMB-0013	Disagree	11/10/10
FD-019	MOS TOD Configuration		TBD	CMB-0014	Agree	11/10/10
FD-020	UMS Emergency Stair #4 Relocation		TBD	CMB-0015	Agree	11/10/10
1250-0016	Additional AWSS modification at 4th/Harrison		\$ 156	CMB-0016	Agree	11/10/10
FD-008	Trolley Re-route @ Columbus & Powell		\$ 2,000	CMB-0017	Agree	09/15/10
FD-007	Cross-passages		\$ -	CMB-0018	Agree	09/15/10
FD-009	ECP to Modify Sidewalk Vault Demolition and Construction Sequence to Facilitate Construction of Joint Utilities Trench and Future UMS Station		\$ 3,000	CMB-0019	Agree	12/15/10
FD-010	Redesign Stockton Street Sanitary Sewer to conform to SFPUC Sewer Criteria that preclude placement of relocated sanitary sewer under sidewalks		\$ 2,000	CMB-0020	Agree	11/17/10
FD-011	Reconfigure Routing of Water, Sewer and Gas laterals from buildings on north side of Ellis Street		\$ 500	CMB-0021	Agree	11/17/10
FD-012	Recognize cost transfers from design allowances allocated to contingency to explicit line items in final design estimates		\$ -	CMB-0022	Disagree	11/17/10
FD-013	Increase Extent of Sub-sidewalks Vault Secondary Closure Walls and Waterproofing		\$ 4,100	CMB-0023	Agree	11/17/10
FD-014	Include OCS system for rerouting trolley buses to Fifth Street		\$ 3,100	CMB-0024	Agree	12/15/10
FD-015	Subsidewalk Vault Secondary Closure Walls for Buildings at 800 Market and 838 Market		\$ 1,000	CMB-0025	Agree	11/17/10
1250-0022	Install additional 30 feet of DIP and new AWSS at 4th/Harrison		\$ 160	CMB-0026	Agree	01/12/11
FD-021	CTS Lowering and Stations Reconfiguration		\$ (18,000)	CMB-0027	Agree	01/19/11
1250-0026	Supporting working around and protecting SFWD existing facilities		\$ 66	CMB -0028	Agree	02/09/11
1251-003	Extending Sewer lines on O'Farrell and Geary - CMod 001		\$ 149	CMB-0029	Agree	02/23/11

CMB LOG

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1250-0030	Differing site conditions during utility installations - CMod #16 - Public only		\$ 238	CMB-0030	Agree	04/13/11
1251-0004	Additional work related to secondary basement closure wall, 150 Stockton Street		\$ 190	CMB-0031	Agree	04/13/11
FD-023	Change to the emergency walkway configuration in the cross cavern at CTS		\$ -	CMB-0032	Agree	07/27/11
FD-024	Change No. 5 diamond X-over on surface segment to tandem (universal) crossover		\$ (350)	CMB-0033	Agree	08/24/11
FD-025	Bid Option for TBM Retrieval Shaft		\$ (10,000)	CMB-0034	Agree	04/13/11
1251-0002	Installation of concrete slab under joint trench at Stockton and O'Farrell CMod #02 <i>(Not to Exceed Total)</i>		\$ 71	CMB-0035	Agree	04/20/11
1250-0029	Adjustment to Additional Excavation and Backfill Bid Item CMod #17		\$ 112	CMB-0036	Agree	05/04/11
1250-0034	Supplement Bid Item TR -6 DPT Traffic Control Allowance CMod #18		\$ 69	CMB-0037	Agree	05/04/11
1251-0009	Supplement Bid Item TR-07- Allowance for Manual Traffic Control with Add'l funds		\$ 263	CMB-0038	Agree	06/01/11
1251-0006	Revising layouts of utilities, northern portion of Stockton Street		\$ 399	CMB-0039	Agree	06/08/11
FD-026	Air Replenishment Systems in the three underground stations		\$ 1,245	CMB-0040	Agree	07/13/11
1250-0036	<i>Rental/Maintenance costs for support of Archeological Trenches - 05/30: SFMTA Board approval due to contract value exceeding the 25% threshold remaining item will be handled through the claims process. CM will provide and update at a later date. 09/12/12: Rental/Maintenance costs for support of Archeological Trenches (Part 1 of 4), (Part 2 of 4), (Part 3 of 4), (Part 4 of 4)</i>		\$ 597	CMB-0041	Agree	09/12/12
1251-0012	Compensate Contractor for additional expenses to modified installation of AWSS on O'Farrell East on Stockton St CMod #07 representing change, now CMod #06 (Refer to Approve CMB-0050, which notes the contract modification amount for this work)		-	CMB-0042	Agree	08/03/11
FD-027	1 Stockton Street (Apple Store) Secondary Closure Wall		\$ 500	CMB-0043	Agree	08/03/11
X53-001	CS 155-2 (DP2) CBP4 (UMS) - Construction Budget Adjustment YOY		-	CMB-0044	Agree	07/27/11
X54-001	CS 155-2 (DP2) CBP5 (CTS) - Construction Budget Adjustment YOY		-	CMB-0045	Agree	07/27/11
X55-001	CS 155-2 (DP2) CBP6 (MOS) - Construction Budget Adjustment YOY		-	CMB-0046	Agree	07/27/11
1250-0012 & 0013	Differing site conditions during utility installations - CMod #21 - Private only		\$ 98	CMB-0047	Agree	08/03/11
1250-0024	Costs associated with support, workaround and protection of existing AT&T utilities - CMod #19		\$ 95	CMB-0048	Agree	08/24/11
1251-0007	Costs associated with design changes to utility layouts along Ellis Street - CMod #05		\$ (435)	CMB-0049	Agree	08/24/11
1251-0012	Additional expenses related to modified installation of the AWSS on O'Farrell East of Stockton St - CMod #06 (Ref. CMB-0042 for original CMod presented for this work)		\$ 278	CMB-0050	Agree	09/21/11
1251-0017 & 0018	Installation of AT&T manholes 5829 and 5830 located on Geary at Stockton - CMod #07		\$ 56	CMB-0051	Agree	09/21/11
FD-028	MOS Revisions to Emergency Ventilation Requirements		\$ 500	CMB-0052	Agree	09/07/11

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Change Form No.	Title of Change	Schedule Impact	*Cost Impact (X,000)	CMB No.	Agree / Disagree	Final Decision Record Date
FD-029	CTS Revisions to Emergency Ventilation Requirements		\$ 1,000	CMB-0053	Agree	09/07/11
FD-030	UMS Revisions to Emergency Ventilation Requirements		\$ 1,000	CMB-0054	Agree	09/07/11
1251-0014	Increase Bid Item WD-10 Allowance for Add'l Excavation and Backfill - CMod #09		\$ 200	CMB-0055	Agree	09/14/11
1251-0027	AT&T installation additional cost to accelerate work - CMod #10		\$ 281	CMB-0056	Agree	11/30/11
1251-0019 & 0028	Installation of AT&T manholes 5828, 5831, 113, 5832 and 403 - CMod #11		\$ 188	CMB-0057	Agree	10/26/11
1251-0020, 0030 & 0031	PG&E Field Changes - Cost will be part of the Form B calculations and be included in the reimbursement from PG&E. (Formally CMod #12 New CMod number is pending assignment)		\$ 89	CMB-0058	Agree	11/02/11
1251-0024 & 0032	PG&E Field Changes - (Formally CMod #13 New CMod number is pending assignment) 1/25: Revision to the number of Kill Tie in locations will reduced this CMod to \$315K when conditions are satisfactorily met. 04/11 : Discovery of the actual number of Kill/Tie locations being 26 instead of 15 will require confirmation from PG&E of the locations and the new cost of scope of work . 05/02: Previously approved CMod for \$349K on 11/16/11. Revised CMod approved for 12 Kill hole locations and "not to exceed" value. PGE email concurrence presented 05/23/12		\$ 303	CMB-0059	Agree	11/16/11
X54-005	Revisions to MPS - CTS Construction Schedule		\$ -	CMB-0060	Agree	12/28/11
X55-002	MPS - STS Construction Schedule Revision for Procurement of Equipment		\$ -	CMB-0061	Agree	12/28/11
FD-034	STS 4th and King Operational Performance and Safety Enhancements. 12/14/11: Agree contingent up "Buy America" waiver. 03/14: "Buy America" contingency no longer applicable. Revised ECP change element - Removing H&K switches and replace with hydraulic/120V. Revised CSP Design Criteria to reflect element design change. CMB approval of replacement agreed on 03/14/12. Revision made to the approved ECP at Mtgs. #71 & #72.		\$ 450	CMB-0062	Agree	12/14/11
CS-155-2	CS 155-2 (DP2) CTS Construction Budget Adjustment - (65% Submittal)		\$ 45,261	CMB-0063	Agree	12/21/11
1251 -0038 (TO-035)	CN1251 (3EA) Kiosk Removal; previous work, and Addl (2EA) new work (see CMB 0080 approved on 10/24/12 for \$29,267.83) for all (5EA) Kiosks.		\$ -	CMB-0064	Agree	01/04/12
1251-0023	Overhead Contact System (OCS) Facilities along Folsom St. between 4th & 5th Streets and Installation of Foundations and Poles - CMod #12 (Formally CMod #15) Condition agreed upon verification material from Contractor to be procured is available. (Verified 02/01/12). 02/29: CMod negotiated cost has been revised to incorporate original three items and the add'l OCS work on Folsom St.		\$ 542	CMB-0065	Agree	02/29/12
CS-155-2	CS 155-2 (DP2) UMS Construction Budget Adjustment - (65% Submittal)		\$ 27,986	CMB-0066	Agree	02/01/12
CS-155-2	CS 155-2 (DP2) MOS Construction Budget Adjustment - (65% Submittal)		\$ 22,947	CMB-0067	Agree	02/01/12
1251 - 0033	Installation of Secondary Closure Wall at 1 Stockton St. (Apple Store) - CMod #13 (Formally CMod #14) (Negotiated Direct Cost \$264K, & Negotiated OH Cost \$143K)		\$ 409	CMB-0068	Agree	03/07/12

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Change Form No.	Title of Change	Schedule Impact	*Cost Impact (X,000)	CMB No.	Agree / Disagree	Final Decision Record Date
1251-0034	Additional work related to discovery of conflicting utilities within the subsidewalk basement at 17-19 Stockton Street. (Worst case scenario - "Not to Exceed" value) <i>CM team will report actual amount value at a later date.</i>		\$ 346	CMB-0069	Agree	04/04/12
1251-0040, 0041, & 0042	Installation of approximately 150LF in additional AT&T trenching (including conduit packages) and the installation of AT&T Vault No.133 (Reimbursement to SFMTA for this compensation value via the Form B process - CMod #14		\$ 100	CMB-0070	Agree	04/11/12
FD-035	Tunnel Air Replenishment System (ARS)		\$ 1,215	CMB-0071	Agree	07/11/12
FD-036	Surface Segment Sewers - 4th St. Base and SFPUC Option		\$ 2,200	CMB-0072	Agree	07/18/12
1252-025	UMS Reduced Duration - 08/01/12 : <i>Condition upon incorporating correctness to backup documentation as required by & documented in CMB mtg.</i>		\$ -	CMB-0073	Agree (Conditionally)	08/01/12
1251-0050, 0067, 0074 & 0075	Trend #50 - Installation of MUNI 770 Pole at 5th and Folsom Trend #67 - OCS Feeder Work On Folsom Trend #74 - Traffic Signal Modifications At 5th And Folsom Trend #75 - Unforeseen Conditions During Pole Foundation Installation On Folsom		\$ 1,100	CMB-0074	Agree	08/15/12
1251-0068	Additional time related overhead (TRO) costs resulting from the 29EA calendar day Contract extension		\$ 107	CMB-0075	Agree (Conditionally)	10/03/12
1251-0043, 0072 & 0080	Trend #43 FAR for Demolition of existing AT&T Vault #113 and 403 Trend #72 Additional AT&T trench from Vault #5829 and 127 Trend #80 Additional work related to installation of AT&T Vaults #5828, 5829, 5830, 5831, 5832 and 5833.		\$ 129	CMB-0076	Agree (Conditionally)	10/03/12
FD-037	Platform Display System Signs - 08/29/12 : Condition follow up action to evaluate procumbent by CN1260)		\$ 500	CMB-0077	Agree (Conditionally)	08/29/12
1251-0037, 0044 & 0045	Gucci Store, Macy's West and Union Square Garage Sewer Installations		\$ 71	CMB-0078	Agree	09/12/12
1251-0052, 0065 and 0070	Trend #52 - Reroute Unforeseen existing waterline discovered along Geary St (east of Stockton St.) Trend #65 - Additional fittings for AWSS lateral and chipping of pole foundation at Market/Ellis Streets Trend #70 - Core drill primary wall and install interior plumbing at Armani and Disney for the completion of fire service water cutover work.		\$ 65	CMB-0079	Agree	10/10/12

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1251-0038, 0039, 0054, 0062 and 0073	Trend #38 - Reimburse Synergy for JCDecaux's removal of 2EA kiosks and other Central Subway related work (TO #35 for this work approved on 01/04/12 - CMB 0064). Trend #39 - Furnish and install 2EA benches along 5th Street. Trend #54 - Additional waterproofing at FIDM and north end of Crate and Barrel. Trend #62 - Additional waterproofing at Macy's Men's wall . Trend 73 - Additional work to install water and electrical utilities for flower stand on Geary west of Stockton.		\$ 86	CMB-0080	Agree	10/24/12
1251-0077	Additional Work Related to Qwest, TCG and UCCO Infrastructure - Contingent CMod pig to include evidence from all utilities that cost are reimbursable to SFMTA		\$ 41	CMB-0081	Agree (Conditionally)	10/31/12
1251-0047, 0058, 0076, and 0078	Muni Vault 900A, Additional SL Work, Additional OCS Work		\$ 76	CMB-0082	Agree	10/31/12
1251-0049, 0053, 0059 and 0064	Contract Acceleration work and Additional Rebar work at 1 Stockton Street Trend #49 Additional cost related to installation of additional rebar at 1 Stockton Street Trend #53 Additional costs to accelerate Contract work related to AT&T and PG&E vaults Trend #59 Additional costs to accelerate Contract work as required to clear Barneys frontage		\$ 41	CMB-0083	Agree	11/07/12
1252	Retrieval Shaft Options (Board Action) for approval presented to the Directors by J.F.		-	CMB-0084	Agree	12/05/12
1252-COR26	MOS - Asbestos Pipe at south headwall - Not To Exceed \$30K		\$ 30	CMB-0085	Agree	01/09/13
1252-COR02	Pre-excavation to remove concrete encased sewer line. \$13K increase of original \$45K for a total of \$58K Not To Exceed		\$ 58	CMB-0086	Agree	12/19/12
1252-COR15	Additional work to restrain the AWSS line - Not To Exceed \$96K		\$ 96	CMB-0087	Agree	12/19/12
1252-COR06	PG&E live electrical delays - Not To Exceed \$7,600		\$ 8	CMB-0088	Agree	01/09/13
1252-COR27	MOS Oil Filled Pipe at south headwall - Not To Exceed \$8,600		\$ 9	CMB-0089	Agree	01/09/13
1252-COR22	MOS Asbestos Pipe at north headwall - Not To Exceed \$6K		\$ 6	CMB-0090	Agree	01/09/13
1252-COR01	AWSS Conflict with Low Pressure Water - Not To Exceed \$16,900		\$ 17	CMB-0091	Agree	01/16/13
1252-COR03	AT&T Vault Conflict -4th & Harrison - Not To Exceed \$21,089		\$ 21	CMB-0092	Agree	01/16/13
1252-COR07	Oil Filled pipe @ Launch Box - Not To Exceed \$10K		\$ 10	CMB-0093	Agree	01/16/13
1252-COR10	MOS - Demo Pea duct bank / SF City 911 fiber optic line - Not To Exceed \$4,915		\$ 5	CMB-0094	Agree	01/16/13
1252-COR11	MOS - Traffic Signal line re-route south headwall - Not To Exceed \$37,500		\$ 38	CMB-0095	Agree	01/16/13
1252-COR12	MOS - Archaeological Standby North Headwall - Not To Exceed \$16,892		\$ 17	CMB-0096	Agree	01/16/13
1252-COR29	MOS - Todco scaffolding reimbursement - Not To Exceed \$1,717		\$ 2	CMB-0097	Agree	01/16/13

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1252-COR41	LB Impacts due to live PG&E electrical lines (Flagging) Not To Exceed \$1,245		\$ 1	CMB-0098	Agree	01/16/13
1252-PCC01	Revisions to Moscone North Headwall Elevation (Top) <i>Not To Exceed \$20,500</i>		\$ 21	CMB-0099	Agree	01/16/13
1252 PCC09	UMS Headwall - Unknown Buried Pipe - <i>Not To Exceed \$75K</i>		\$ 75	CMB-0100	Agree	01/16/13
1252-COR.38	Manhole in West side Guidewalls Footprint - <i>Not To Exceed \$4,052.17</i>		\$ 4	CMB-0101	Agree	01/23/13
1252-COR39	MOS - 16" Steel Pipe Removal at North east Headwall - <i>Not To Exceed \$2,951.99</i>		\$ 2	CMB-0102	Agree	01/23/13
1252-COR46	UMS - Unmarked Steel Pipes (6", 12" and 14") & Duct Bank - <i>Not To Exceed \$7,461.14</i>		\$ 7	CMB-0103	Agree	01/23/13
1252-COR47	Retrieval Shaft - Unmarked 10" Steel Pipe / Duct Bank Removal - <i>NTE \$2,201.54</i>		\$ 2	CMB-0104	Agree	01/23/13
1252-PCC11	Cap and Removal of 48" Steel Pipe - Not To Exceed \$150,000		\$ 150	CMB-0105	Agree	01/23/13
1252-PCC07	BART Annex Wall - Not To Exceed \$15,380		\$ 15	CMB-0106	Agree	01/30/13
1251-05, 46, 51, 61, 63, 66, 69, 79, 81	CMod #24 PG&E: 1. Trend 05 – Addll wok to work around and shift existing PG&E conduits on Geary Blvd. at 2 locations 2. Trend 46 – Additional cost for enlarging PG&E Vault No. 584 on O'Farrell east of Stockton 3. Trend.51 – Intercept existing PG&E ductbank and terminate into Vault No. 5464 Market/Ellis 4. Trend 61 – Excavate for PG&E service cut-over at 177 Stillman 5. Trend 63 – Excavation and restoration for PG&E gas tie-in and kill holes (Pt.2) 6. Trend 66 – Intercept PG&E conduits at Vault No. 573 at Geary east of Stockton 7. Trend 69 – Removal of abandoned PG&E gas valve (8EA and installation of anodes 8. Trend 79 – Additional work related to the installation of PG&E Vault Nos. 467, 571, 573, 586 and 594 (including joint trench leading to these vaults). 9. Trend 81 – Use of regular concrete as opposed to cover PG&E electrical ductbanks.		\$ 333	CMB-0107	Agree	02/13/13
1252-COR031	Retrieval Shaft Unmarked 12" Steel Pipe - <i>Not To Exceed \$1,250</i>		\$ 1	CMB-0108	Agree	02/27/13
1252COR-044	MOS - 16" Steel pipe removal at headwall - <i>Not To Exceed \$1,800</i>		\$ 2	CMB-0109	Agree	02/27/13
1252-COR050	MOS - 12" Steel pipe removal at Union/Columbus Streets - <i>Not To Exceed \$2,600</i>		\$ 3	CMB-0110	Agree	02/27/13
1252-COR058	10" Steel pipe removal at Union and Columbus Streets - <i>Not To Exceed \$860.86</i>		\$ 1	CMB-0111	Agree	02/27/13
1252-COR066	MOS - 12" Steel pipe removal intersection of Union and Columbus - <i>Not To Exceed \$3,800</i>		\$ 4	CMB-0112	Agree	02/27/13
1252-PCC03	Modified Stockton St. Detour Signage Plan - <i>Not To Exceed \$6,152.60</i>		\$ 6	CMB-0113	Agree	02/27/13
1252-COR048	Removal of 3" x 5" brick sewer at UMS South Headwall - <i>Not To Exceed \$12,474.65</i>		\$ 12	CMB-0114	Agree	03/13/13
1252-COR070	LB Jet Grout Quantity Overrun - Bid Item ES-3- <i>Not To Exceed \$599,900</i>		\$ 600	CMB-0115	Agree	04/03/13
1252-COR079	Compensation Grouting - Old Navy - <i>Not To Exceed \$70,000</i>		\$ 70	CMB-0116	Agree	04/26/13
1252-COR079	Compensation Grouting - Old Navy (Inefficiencies) - <i>Not To Exceed \$152,000</i>		\$ 152	CMB-0117	Agree	05/15/13

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1252-COR073	MOS - 16" Pipe removal at NE Headwal - NTE \$989		\$ 1	CMB-0118	Agree	05/08/13
1252-COR076	MOS - 16" Pipe removal at SE Headwal - NTE \$1,116		\$ 1	CMB-0119	Agree	05/08/13
1252-COR081	12" Waterline Tee Connection (MOS) South - NTE \$3,000.63		\$ 3	CMB-0120	Agree	05/15/13
FD-038	Elongated Sidewalk Bulb-out Chinatown Station at Stockton St. (CTS) 1254R - 1300 Pkg		\$ 36	CMB-0121	Agree	06/05/13
	Total		\$ 82,893			