

CMB Meeting Minutes #161

DATE: March 20, 2014

MEETING DATE: March 12, 2014

LOCATION: 821 Howard St, Main Conference Room

TIME: 3:00 PM

ATTENDEES: J Funghi (JF), A. Hoe (AH), R. Nguyen (RN), B. Kelleher (BK), M. Kobler (MK), M. Vilcheck,(MV), J. Wang (JW), T. DePooter (TDP), L. Zurinaga (LZ), M. Acosta (MA), J. Park (JP), A. Clifford, (AC), E. Stassevitch (ES), B. Ward (BW), B. Lebovitz (BL)

COPIES TO: Attendees: S. Farhangi (SF), M. Henry (MH), K. Shah (KS), J. Xia (JX), R. Redmond, (RR), V. Chow (VC), S. Jefferis (SJ), S. Tisell (ST), S. Espinal (SE), R. Hansen (RH), M. Latch (ML), D. Kuehn (DK) File No. M544.1.5.0890

REFERENCE Project No. M544.1, Contract No. 149 Task 1-8.02

SUBJECT: Configuration Management Board Meeting # 161– Rev. No. 0

RECORD OF MEETING *(Italicized text indicates status update of open items)*

ITEM #	DISCUSSION	ACTION BY DUE DATE
3- 02/26/14	M. Benson presented revised documentation as a follow up item to Contract 1277 COR 01 - Demolish and remove unforeseen concrete structures under slab on grade. During performance of contract work the Contractor encountered an unknown room under the existing Pagoda floor slab. Contractor was directed to demolish the unknown room and remove the brick, concrete debris, and slab. In order not to delay the 1252 Contractor access to the site the mixed debris was off hauled to Baylands Soil Processing for sorting and testing of the soil. The CMB did not agree with merit for the work as presented and requested the RE to re-examine the load and tonnage figures in the estimate as well as additional time. No action was taken by the CMB on this item. Status: <i>03/12 A. Clifford presented a revised estimate, adjusting the calculations for material load removal and the duration of trucking time. The CMB agreed with the cost of the change for a Not to Exceed amount of \$79,379.17 - AGREE – CMB 0150. Items unresolved from the Contractor’s standpoint will be noted in the CMod and addressed in a future change if required.</i>	
1-	B. Kelleher presented for approval a request for additional funds to close out CN1252 force account change order #06 - Advance Material Purchase. Due to modification of the tunnel alignment at Market Street the Contractor was required to procure additional compensation grout tubes in advance to minimize the standby cost for the Contractor’s equipment. The additional funds requested, \$4,164, reflect a differential in freight cost initially estimated as a prorated cost. The CMB agreed with authorizing additional funds to bring the new total dollar amount not to exceed \$45,634.39.	

ITEM #	DISCUSSION	ACTION BY DUE DATE
2-	A. Clifford presented for consideration; information to evaluate Retention Reduction for Contract 1252. Based on 77% of cost expended to date. The Board expressed continued concern on specific issues as outlined in the presentation bullet points (see attached) as well as the additional concern regarding the risk of the UMS Headwall "battered piles", which may impact the 1300 Contractors work. The CMB agreed to consider partial release of retention after further evaluation of specific concerns. The subject will be further discussed at the next executive level partnering meeting.	
3-	A. Clifford presented an estimate for CN1252 Restoration of the existing 3x5 sewer due to the relocation of the retrieval shaft at north beach. The new approach for sewer work requires a small incremental increase in costs, \$3,661, over previously authorized work; partial credit from approved CMod 20 - sewer slip lining work and CMod 21- sewer modification work in addition to standby costs. (see attached). PCC 20 is being drafted and will be forwarded to the Contractor. The CMB has authorized the work to proceed on force account for a not to exceed amount \$10K with intent to negotiate a lump sum amount AGREE – CMB 0152 . The Contractor is required to present written cost proposal for standby costs in order to complete negotiations.	
4-	M. Vilcheck presented CN1300 Notice of Different Site Condition - Buried Concrete Wall at YBM. During potholing work the Contractor encountered buried concrete wall about 10' below grade at Folsom on west side of 4th Street. The RE requested approval to authorize. The CMB agreed with the evaluation of DSC and provided authorization not to exceed \$15K tracked on force account.	
5-	M. Vilcheck presented CN1300 Notice of Different Site Condition - Abandoned Sewer Line on 4th Street. During excavation the Contractor hit an abandoned 3x5 brick sewer, which was shown on the contract drawing as being slurry filled but was discovered not to have been filled. The RE requested approval to authorize the Contractor to install temporary plugs in the sewer line and backfill the excavation with sack slurry. The CMB agreed with the direction, authorizing the work to be performed on force account with a not to exceed amount of \$10K.	
6-	M. Acosta presented CN1300 Notice of Different Site Condition - Subsurface layers of concrete at 4th Street and Bryant and 4th Street between Brannan and Bluxome. During potholing work for utilities the Contractor encountered subsurface layer of concrete and debris about 12" below the ground surface. The RE requested approval to authorize the Contractor to remove debris from the excavation and to track all extra work on force account. The CMB agreed with the DSC evaluation and provided authorization for the work to be performed on force account with a not to exceed amount of \$20K.	
7-	The Program Trend/Change Control Log dated 03/12 was reviewed. Contract 1252 Construction Manager reported PCC No. 16 - Ret Shaft Sewer Changes and PCC No. 17 - Green Street Sewer were rescinded by SFMTA. An alternate technical solution will be implemented and a new PCC will be issued.	

ACTION ITEMS

ITEM #	MTG DATE	MTG ACTION DATE	DESCRIPTION	BIC	DUE DATE	STATUS
8	07/24/13	07/24/13	1252 – Breakdown of Bid Item GE-3	AC/MLe	03/19/14	Open

2	08/14/13	08/14/13	1252 – CMod #8 (PSI strength) – Statement of occurrence post meeting record of approval which overrode CMB direction	AC/MLe	03/19/14	Open
1	08/28/13	08/28/13	1252 – Outstanding COR's – status update	AC/ML	03/19/14	Open
2 & 3	08/28/13	08/28/13	1252 – COR 97 & 98 – gravel encountered action plan from Engineering of Record (DP1)	AC/MLe	03/19/14	Open
5	08/28/13	08/28/13	1252 – Action plan of execution for the Slip Lining Work (Stockton/Columbus)	AC/MLe	03/19/14	Open
4	09/25/13	10/02/13	1252 – COR #104 - Additional Sewer Slip Lining – SFPUC Response	AC/MLe	03/19/14	Open
5	09/25/13	10/02/13	1300 – PCC CTS 0001 - Contractor Cost Proposal	TDP	03/19/14	Open
2	10/09/13	10/09/13	1252 – Design Clause - confirmation of existing contract language in CN 1252 and CN 1300 regarding existing conditions related to utilities	JW/AH	03/19/14	Open
1	10/16/13	10/16/13	1252 – COR 102 - Determine if the Contractor received 1251 as built information before they began operations	AC/MLe	03/19/14	Open
1	10/16/13	10/16/13	1252 – COR 102 - Graphic showing the location of the headwalls control line	AC/MLe	03/19/14	Open
3	10/23/13	10/23/13	1252 – PCC 014 - Contractor Cost Proposal	JW/AC	03/19/14	Open
4	10/23/13	10/23/13	1252 – COR 05 - Revisit IRL process with Contractor	RR	02/26/14	Open
2	11/06/13	11/06/13	1252 - COR 017 – Notify utility of cost as Form B bill update under CN1250 or CN1251	AC/MLe	03/19/14	Open
3	11/06/13	11/06/13	1252 – Slurry Wall Panels – TIA to be sent to BIH – SFMTA's time savings and premium cost saving time	AC/MLe	03/19/14	Open
2	11/13/13	11/13/13	1300 – Setup a meeting with DT to confirm the use to control traffic	BK	03/19/14	Open
NA	11/13/13	11/13/13	1252 – Confirm who is being billed for the use of PCO's at 5 th St.	AC/MLe	03/19/14	Open
2	11/20/13	11/20/13	1300 – PCC 00X - PGE Traffic Control - Utility verification letter	TDP	03/19/14	Open
7	12/11/13	12/11/13	1252- BART Load Cell - Conference Call	JW	03/19/14	Open
9	12/11/13	12/11/13	1252 - Trend Log Segment Measuring Frequency line item credit (1.3M)	AC/MLe	03/19/14	Open
10	12/11/13	12/11/13	1252 - Precast Tunnel Liner Test - Contractor's Rpt.	ML	03/19/14	Open
2	01/08/14	01/08/14	1300 - PCC - Contractor Cost Proposal - Geoprobes	RR/ST	03/19/14	Open
4	01/08/14	01/08/14	1300 - PCC - Contractor Cost Proposal - Joint Trench Conduits	TDP	03/19/14	Open
4	01/15/14	01/15/14	1300 - CTS confirmation from PG&E of cost for concrete encasement of lines	TDP	03/19/14	Open
10	02/05/14	02/12/14	1300 - STS Letter to SFPUC notification of collapse	MA	03/19/14	Open

Meeting adjourned at 5:00pm

These meeting minutes have been prepared by B. Ward and reviewed by E. Stassevitch, and are the preparer's interpretation of discussions that took place. If the reader's interpretation differs, please contact the author in writing within four (4) days of receipt of these minutes.

Signed:  [initials of preparer & reviewer] Date:  [Date review completed]

Meeting Agenda

Project No. M544.1, Contract No. CS-149
Program/Construction Management
Configuration Management Board (CMB) Meeting No. 161
March 12, 2014
3:00pm – 5:00pm
 Central Subway Project Office
 821 Howard St. 2nd Floor
 Main Conference Room

Attendees:

Alex Clifford	Brian Kelleher	Richard Redmond
John Funghi	Mark Latch	Eric Stassevitch
Albert Hoe	Brad Lebovitz	Beverly Ward
Scott Jefferis	Roger Nguyen	Luis Zurinaga

1. **1252** - FACO 06 - PCC 12 Advance Material Purchase - *Addl Cost NTE*
(Previously discussed at CMB Mtg. #136)
 - Contract Retention Reduction - *Review*
 - Retrieval shaft sewer changes - *Discussion*

2. **1277** - COR 01 - Demolish and remove unforeseen concrete structures found under slab on grade - *Merit, Cost NTE (Previously discussed at CMB Mtg #157 & 160)*

3. **1300** - YBM - DSC - Buried Concrete Wall - *FA Approval*
 - YBM - DSC - Abandoned Sewer Line (Not Slurry Filled) - *FA Approval*
 - STS - DSC - Subsurface Layer of Concrete on 4th Street - *FA Approval*

4. **Trend Log**

5. **Other Business –**

Meeting Attendance Sheet

Project No. M544.1, Contract No. 149
Program/Construction Management
Configuration Management Board S Meeting No. 161
March 12, 2014
3:00 p.m. – 5:00 p.m.
 Central Subway Project Office
 821 Howard, 2nd Floor
 Main Conference Room

Deliver Meeting Attendance Sheet with original signatures/initials to Document Control.

NAME	AFFILIATION	PHONE	E-MAIL (for minutes)	INITIALS
Please enter initials if your name is listed below. Please enter name, affiliation, phone number and email address if your name is not listed below.				
Acosta, Mike	SFMTA	(415) 701-5282	Mike.Acosta@sfmta.com	
Chow, Vivian	SFMTA	(415) 701-5264	Vivian.chow@sfmta.com	
Clifford, Alex	CSP	(415) 243-0953	Alex.Clifford@sfmta.com	AC
DePooter, Ted	CSP	415 701-5295	Ted.DePooter@sfmta.com	TRD
Farhangi, Shahnam	SFMTA	(415) 554-0721	Shahnam.Farhangi@sfmta.com	
Funghi, John	SFMTA	(415) 701-4299	John.Funghi@sfmta.com	JF
Haley, John	SFMTA		John.Haley@sfmta.com	
Henry, Melvyn	SFMTA	(415) 701.5719	Melvyn.Henry@sfmta.com	
Hoe, Albert	SFMTA	(415) 701-4289	Albert.Hoe@sfmta.com	AH
Jefferis, Scott	SFMTA	(415) 238-4679	Richard.Jefferis@sfmta.com	
Kelleher, Brian	SFMTA	(415) 701-5289	Brian. Kelleher@sfmta.com	BK
Kelly, Jim	SFMTA		Jim.Kelly@sfmta.com	
Kuehn, David	STV/PMOC	(510) 464-8053	David.kuehn@stvinc.com	
Latch, Mark	CSP	(415) 701-5294	Mark.Latch@sfmta.com	
Lebovitz, Brad	STV/PMOC	(510) 464-8052	Bradley.lebovitz@stvinc.com	BL

central subway

NAME	AFFILIATION	PHONE	E-MAIL (for minutes)	INITIALS
Nguyen, Roger	SFMTA	(415) 701-4312	Roger.Nguyen@sfmta.com	<i>RN</i>
Park, Joon	SFMTA	(415) 701-4742	Joon.Park@sfmta.com	<i>J.P.</i>
Redmond, Richard	CSP	(415) 701-4288	Richard.Redmond@sfmta.com	
Shah, Kartik	SFMTA	(415) 701.5678	Kartik.Shah@sfmta.com	
Stassevitch, Eric	CSP	(415) 701-4426	Eric.Stassevitch@sfmta.com	<i>ES</i>
Tisell, Steve	CSP	(415) 701.5280	steve.tisell@sfmta.com	<i>ST</i>
Vilcheck Mark	CSP	(415) 701-4293	Mark.Vilcheck@sfmta.com	<i>MV</i>
Wang, Jane	SFMTA	(415) 701-4287	Jane.Wang@sfmta.com	<i>JW</i>
Ward, Beverly	CSP	(415) 701-5291	Beverly.Ward@sfmta.com	<i>BW</i>
Zurinaga, Luis	SFCTA	(415) 716-6956	Luis.zurinaga@sfcta.org	<i>LZ</i>
<i>KOBLER, MIKE</i>	<i>SFMTA</i>	<i>415-850-4750</i>		<i>K</i>

CMB Change No.: **CMB - 131**

Initial Implementing Change Control Procedure No.: **1252 - PCC - 12**

GENERAL

Proposed Change Sponsor: S. Wilson Received by CMB: 08/14/2013
(Date)

Affected Disciplines: Building Protection, Track

Impacts of Change Modifications to Tunnel Alignment at Market Street - Advance Material costs for Core Barrel Choker

Proposed Contract Change (PCC 12) Amount Not To Exceed
~~\$47,500~~ **\$41,470.01**

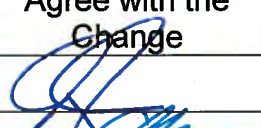
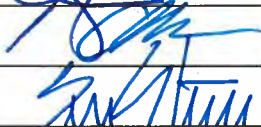
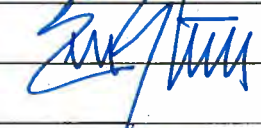
Note:
12 MAR 14 -> Authorized additional \$4,164 for a total of NTE \$45,634.38

Contract(s) Directly Affected by this Proposed Change:

1250	1251	1252	1253	1254	1255	1256
1 <input type="checkbox"/>	2 <input type="checkbox"/>	3 <input checked="" type="checkbox"/>	4 <input type="checkbox"/>	5 <input type="checkbox"/>	6 <input type="checkbox"/>	7 <input type="checkbox"/>
(CP01)	(CP02)	(CP03)	(CP04)	(CP05)	(CP06)	(CP07)

CONFIGURATION MANAGEMENT BOARD APPROVALS

Signatures

	Agree with the Change	Disagree with the Change	Date
Program Director:			8-14-13
Deputy Program Manager:			8/14/2013
PM Project Services:			14 Aug 13
PM Project Construction:			
SFMTA O & M Manager:			
SFMTA Safety and Security			
SFCTA PMO			

Comments

CMB approval contingent upon Matt Fowler (OP) verifying by signature on merit evaluation that additional work is required and \$ figure for cost impact is acceptable

Estimate Summary

General Scope: PCC 12 (Modifications to Tunnel Alignment at Market Street)

Date of work performed:

TBD

Detailed Scope:

Materials required for Modification to Building Protection Compensation Grouting within the Market Street Area (Pipe Arrays 1 through 3 shown on Contract Plans BP-122 Rev.0, BP-123 Rev.0 and BP-124 Rev.0). Contractor shall revise compensation grouting design to conform to Contract requirements given the revised tunnel alignment.


Cost Summary:

Labor	Equipment	Material / Sub	Mark Ups	Total
\$ -	\$ -	\$ 33,603.15	\$ 7,868.86	\$ 41,472.01

List of attached documents:

Material Costs

Global Drilling Suppliers, Inc. Invoice 504522-01, 5045928-01 & 5045525-01

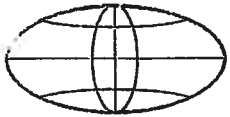
 8/14/13
Prepared By: Matt Hembd Date:

 08/14/2013
Checked By: Ben Volberding Date:

Material	Qty.	Unit	Unit Price*	Extension
Item Code and Description				
06625D77 2M Grout Casing	77	Ea.	\$ 276.52	\$ 21,292.04
Freight (pro-rated) (\$5,200*77/(72+162))*	1	LS	\$ 1,711.11	\$ 1,711.11
1210494 Grout Valve	770	Ea.	\$ 3.31	\$ 2,548.70
Freight (pro-rated) (\$85.32*930/250)*	1	LS	\$ 262.79	\$ 262.79
06625D93 Drag Bit	7	Ea.	\$ 768.00	\$ 5,376.00
Freight (pro-rated) (\$1,682.78*7/(37+6+6+6))*	1	LS	\$ 214.17	\$ 214.17
			Sales Tax 7%	\$ 2,198.34
			Total Direct Cost	\$ 33,603.15

*See Global Drilling Suppliers, Inc. Invoice 504522-01, 5045928-01 & 5045525-01 for Unit Pricing and Freight Costs

Labor	\$	-
Equipment	\$	-
Material	\$	33,603.15
2nd Tier Sub	\$	-
Markup on 2nd Tier Sub - 5%	\$	-
1st Tier Sub OH&P - 15%	\$	5,040.47
1st Tier Sub Bond/Insurance - 0.52%	\$	200.95
Subcontractor Subtotal	\$	38,844.56
Prime - Markup On Subs - 5%	\$	1,942.23
Prime - Bond/Insurance - 1.68%	\$	685.22
Total Amount	\$	41,472.01



GLOBAL DRILLING SUPPLIERS, INC.

OCI Division

INVOICE

phone: 800-453-6400 fax: 814-849-6288 www.globaldrilsup.com
 mail to: 370 Industrial Park Road, Brookville, PA 15825

remit to: 12101 Centron Place, Cincinnati, OH 45246-1704

INVOICE NUMBER

5045222-01

CONJOH

BILL TO:

CONDON-JOHNSON & ASSOC. INC.
 480 ROLAND WAY
 SUITE 200
 OAKLAND CA 94621

SHIP TO:

CONDON-JOHNSON/NICHOLSON J.V.
 100 EAST 4TH STREET
 SAN FRANCISCO CA 94107

RECEIVED
 JUN 03 2013

44-2921

CUSTOMER P.O.NO. 21134

4

CONDON-JOHNSON

CUSTOMER P.O. NO. 21134

4

INVOICE NUMBER		SLSM/N	ORDER DATE	TAKER	CUSTOMER P.O. NUMBER	DATE		
5045222-01		501	04/22/13	501	21134	05/31/13		
INSTRUCTIONS						FRT	PAG NO.	
WHITE BUFFALO PPD/ADD 05 13 13 MIKAEL CALANDO ph#412-445-8577						B	1	
ORDERED	QUANTITY	B.O./RET.	SHIPPED	DISP.	ITEM CODE AND DESCRIPTION	U/M	UNIT PRICE	AMOUNT
125	2		72	B*	06625B85 2M 1-USE CASING CAS 6.625, .432"W, 2 METERS S-S, RH SQUARE MALE X FEMALE CASING THREADS	EA	154.31	11110.32
400	58		162	B*	06625D77 2M GROUT CASING CAS 6.625, .432"W, 2 METERS S-S, RH SQUARE MALE X FEMALE CASING THREADS WITH 5 PAIRS OF GROUT HOLES	EA	276.52	44796.24

1177

1260 6/5/13
 442921
 21134

REFER
 1260 6/5/13
 442921
 21134

FILE

CODE EXPLANATION
 * - STATE TAX APPLICABLE C - CONSIDER COMPLETE
 † - FED./OTHER TAX APPLICABLE D - DIRECT SHIPMENT
 ‡ - STATE & FEDERAL TAX F - FACTORY MINIMUM
 B - BALANCE BACK ORDERED H - RETURNED CYL.

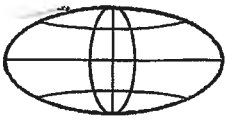
*** THIS IS YOUR INVOICE ***

FREIGHT IN	FREIGHT OUT
0.00	5200.00

NET TERMS: INV 30 DUE: 06/30/13

SUB TOTAL	55,906.56
MISC. CHARGE	
TELE. CHARGE	
FREIGHT TOTAL	5,200.00
FED./OTHER TAX	
STATE TAX	4,192.99
PAYMENT REC'D.	0.00

TOTAL AMT DUE
 65,299.55



GLOBAL DRILLING SUPPLIERS, INC.

OCI Division

INVOICE

phone: 800-453-6400 fax: 814-849-6288 www.globaldrilsup.com
 mail to: 370 Industrial Park Road, Brookville, PA 15825

remit to: 12101 Centron Place, Cincinnati, OH 45246-1704

INVOICE NUMBER:

5045928-01

CONJOH

BILL TO:
 CONDON-JOHNSON & ASSOC. INC.
 480 ROLAND WAY
 SUITE 200
 OAKLAND CA 94621

SHIP TO:
 CONDON-JOHNSON/NICHOLSON J.V.
 420 EAST 4TH STREET
 SAN FRANCISCO CA 94107

CUSTOMER P.O.NO. 21134-DK

1

CUSTOMER P.O.NO. 21134-DK

1

INVOICE NUMBER	SLSMN	ORDER DATE	TAKER	CUSTOMER P.O. NUMBER	DATE		
5045928-01	501	07/01/13	501	21134-DK 1	07/29/13		
INSTRUCTIONS					FRT.	PAGE NO.	
UPS-NDA/AM (PPD/ADD) 07/01/13 DAN KELSEY ph: 512-970-1700					B	1	
ORDERED	QUANTITY	SHIPPED	DISP.	ITEM CODE AND DESCRIPTION	U/M	UNIT PRICE	AMOUNT
1000	8.0/RET	250	*	1210494 GROUT VALVE 9/16" OD REED-TYPE, .200" THICK RUBBER	EA	3.31	827.50

Handwritten red circle around item description.

Handwritten notes: 1260 8/2/13, \$974.88 21134 941170 03

RECEIVED
 JUL 31 2013
 CONDON-JOHNSON

FILE

CODE EXPLANATION
 * - STATE TAX APPLICABLE C - CONSIDER COMPLETE
 # - FED/OTHER TAX APPLICABLE D - DIRECT SHIPMENT
 + - STATE & FEDERAL TAX F - FACTORY MINIMUM
 B - BALANCE BACK ORDERED H - RETURNED CYL

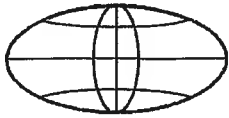
*** THIS IS YOUR INVOICE ***

FREIGHT IN	FREIGHT OUT
0.00	85.32

NET TERMS: INV 30 DUE: 08/28/13

SUB TOTAL	827.50
MISC. CHARGE	
TELE. CHARGE	
FREIGHT TOTAL	85.32
FED./OTHER TAX	
STATE TAX	62.06
PAYMENT REC'D.	0.00

TOTAL AMT DUE
 974.88



GLOBAL DRILLING SUPPLIERS, INC.

OCI Division

INVOICE

phone: 800-453-6400 fax: 814-849-6288 www.globaldrillsup.com
mail to: 370 Industrial Park Road, Brookville, PA 15825

remt to: 12101 Centron Place, Cincinnati, OH 45246-1704

INVOICE NUMBER

5045525-01

CONJOH

BILL TO:

CONDON-JOHNSON & ASSOC. INC.
480 ROLAND WAY
SUITE 200
OAKLAND CA 94621

SHIP TO:

CONDON-JOHNSON/NICHOLSON J.V.
420 EAST 4TH STREET
SAN FRANCISCO CA 94107

CUSTOMER P.O.NO. 21134-17

CUSTOMER P.O.NO. 21134-17

INVOICE NUMBER		SLIP NO.	ORDER DATE	TAKER	CUSTOMER P.O. NUMBER	DATE	
5045525-01		501	05/22/13	501	21134-17	06/12/13	
INSTRUCTIONS						FRF.	PAGE NO.
NEW PENN (PPD/ADD) 05/22/13 DAN KELSEY ph:512-970-1700						B	1
ORDERED	QUANTITY B.O./RET.	SHIPPED	DISP.	ITEM CODE AND DESCRIPTION	U/M	UNIT PRICE	AMOUNT
37		37	*	M889-626 2M MULTI CASING CAS 3.500, 2 METERS S-S, 88.9MM LH SGL LEAD MxF CAS THDS, FLATS PIN END	EA	395.00	14615.00
6		6	*	06625E04 CUTTING SHOE 6 5/8" OD X .432"W X 6" OAL RH SQUARE FEMALE CAS THDS X 5-U7L TEETH	EA	234.00	1404.00
6		6	*	M889-625 DRAG BIT 5 1/2" 3-WING STEP TYPE, 88.9MM LH SGL LEAD MALE CAS THDS, CARBIDE TIPPED	EA	549.00	3294.00
12		6	B*	06625D93 DRAG BIT 7" 3-WING STEP TYPE, 6 5/8" RH SQUARE FEMALE CAS THREADS, CARBIDE TIPPED, WITH 1 1/2" NPT CHECK VALVE	EA	768.00	4608.00

1260 6/24/13

*THROW-AWAY BIT FOR SHAFT.

#27377 2134 941170 03

*** THIS IS YOUR INVOICE ***

CODE EXPLANATION
 * - STATE TAX APPLICABLE C - CONSIDER COMPLETE
 # - FED. OTHER TAX APPLICABLE D - DIRECT SHIPMENT
 + - STATE & FEDERAL TAX F - FACTORY MINIMUM
 B - BALANCE BACK ORDERED H - RETURNED CYL.

FREIGHT IN	FREIGHT OUT
0.00	1682.78

NET TERMS: INV 30 DUE: 07/12/13

SUB TOTAL	23,921.00
MISC. CHARGE	
TELE. CHARGE	
FREIGHT TOTAL	1,682.78
FED./OTHER TAX	
STATE TAX	1,794.08
PAYMENT REC'D.	0.00

TOTAL AMT DUE
27,397.86

PROPOSED CONTRACT CHANGE

Contract No. 1252 Tunnels Date 4/03/2013
 PCC No. 1252-12
 PCC Title Modifications to Tunnel Alignment at Market Street

Description of PCC:

Summary of Changes:

1. Modifications to tunnel alignment as required to avoid conflict with existing micropile. Modifications effect tunnel alignments between:
 NB: STA 134+25.00 and STA 151+69.83
 SB: STA 134+25.00 and STA 151+67.00
 The modified alignments shown on the attached drawing supersedes original NB and SB alignments shown on Contract Plans TC-108 Rev.1 through TC-111 Rev.0, TC-134 Rev.1 through TC-136 Rev.0.
2. Modification to Building Protection Compensation Grouting within the Market Street Area (Pipe Arrays 1 through 3 shown on Contract Plans BP-122 Rev.0, BP-123 Rev.0 and BP-124 Rev.0). Contractor shall revise compensation grouting design to conform to Contract requirements given the revised tunnel alignment. Revised grouting plans shall be submitted to the SFMTA in accordance with Contract Specification section 31 43 14.

Spec Ref. CN 1252 Tunnels

Drawing Nos. CS 1252 TC-108
 CS 1252 TC-134
 CS 1252 BP-122

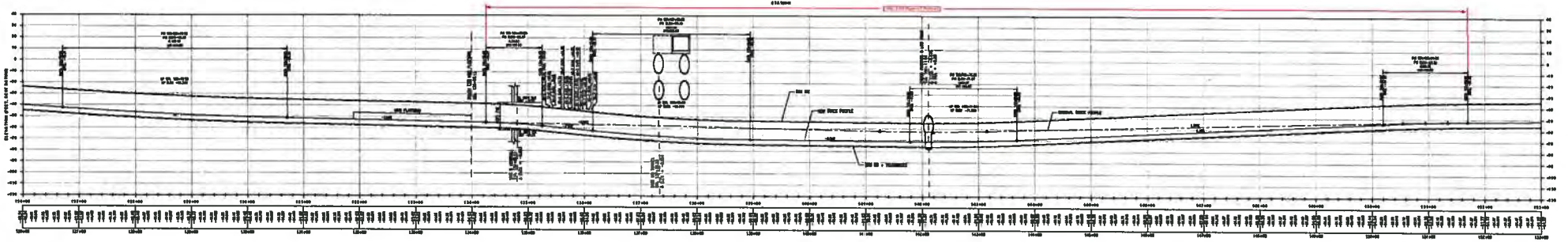
Attachments Attachment A - Revised Tunnel Alignment

The Contractor's proposal in price and time is required on the this proposed contract change to the subject contract in accordance with the General Provisions Section 6.02 B, within 14 days after receipt of a PCC.

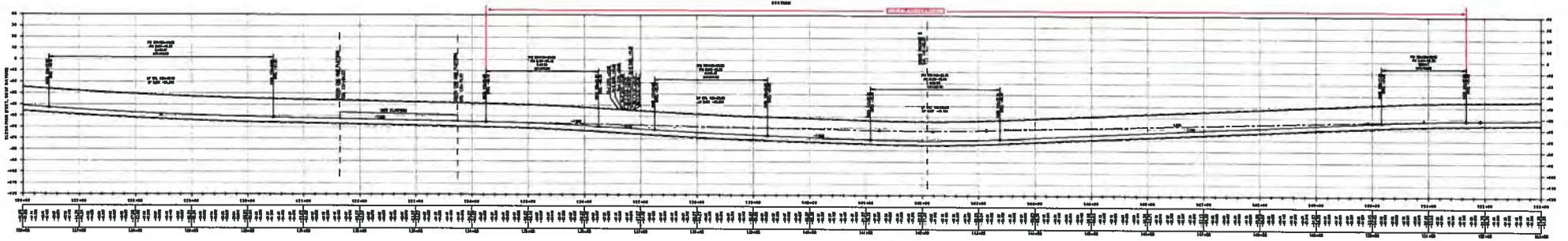
Recommended by:  Date 4/3/13
 Sarah Wilson, Resident Engineer (SFMTA Representative)

Concur in Principle:  Date 4/3/13
 Mark Benson, Construction Manager (SFMTA Representative)

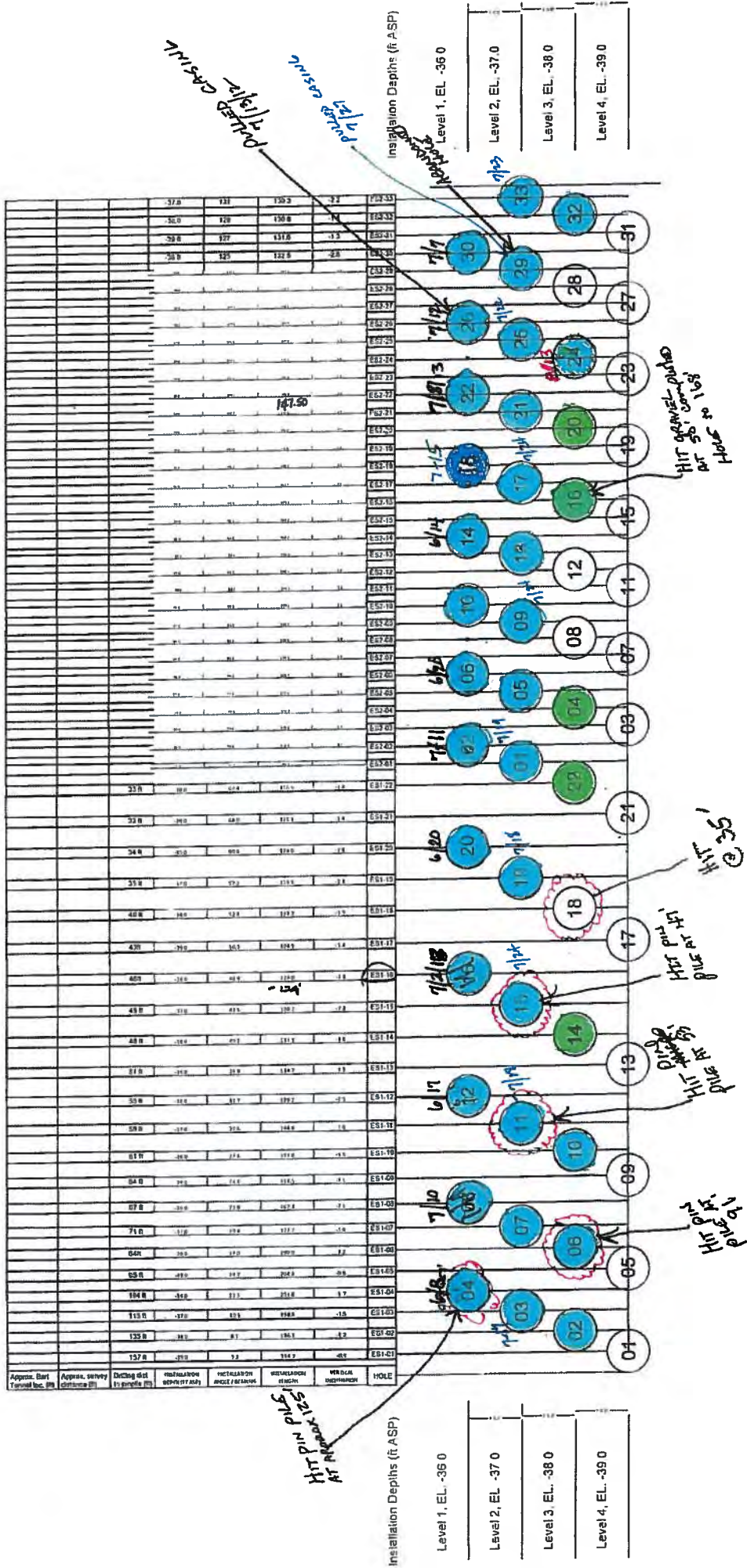
NB PROFILE



SB PROFILE



ELLIS SHAFT



Contract 1252
Contractor Request
Reduction in Retention Withheld
Discussion

1252 Reduce Retention Withheld

Article 9.09 Retention & Escrow Agreement

- ❖ City determines the Work is 77 percent (by \$) or more complete (as of January 2014);
- ❖ Contractor is making satisfactory progress;
- ❖ No specific cause for greater withholding; and
- ❖ Consent of Surety

City Determines Work is 50 percent or more complete

- The Work is 77% complete – based on dollars only

Activity ID	Activity Name	Start	Finish	Performance % Complete	Budgeted Total Cost	Planned Value Cost (PV)	Earned Value Cost (EV)	Actual Total Cost (AC)	SPI	CPI
CENTRAL SUBWAY PROJECT		27-Jan-12 A	18-Jun-15	76.89%	237,541,833.95	194,260,596.39	182,448,945.57	180,229,015.65	0.94	1.01
Construction Phase		27-Jan-12 A	18-Jun-15	76.89%	237,541,833.95	194,260,596.39	182,448,945.57	180,229,015.65	0.94	1.01
Construction Tunnels CN-1252		27-Jan-12 A	18-Jun-15	76.89%	237,541,833.95	194,260,596.39	182,448,945.57	180,229,015.65	0.94	1.01
1252 Tunnel Contract BIH		27-Jan-12 A	18-Jun-15	76.89%	237,541,833.95	194,260,596.39	182,448,945.57	180,229,015.65	0.94	1.01
Contract Milestones		27-Jan-12 A	18-Jun-15	0%	0.00	0.00	0.00	0.00	0.00	0.00
General Conditions		27-Jan-12 A	18-Jun-15	93.69%	90,965,543.95	75,077,364.11	75,400,852.04	75,608,624.15	1.00	1.00
4th & Bryant St TBM Launch Box Construction		30-Mar-12 A	25-Oct-13 A	99.42%	51,358,700.00	51,358,700.00	51,060,535.00	51,060,535.00	0.99	1.00
4th St and Harrison Compensation Grouting		28-Jan-13 A	06-Feb-14	100%	384,500.00	384,500.00	384,500.00	268,783.50	1.00	1.43
Moscone Station Headwalls		14-May-12 A	20-Sep-13 A	99.79%	7,500,000.00	7,500,000.00	7,484,050.00	7,484,000.00	1.00	1.00
UMS Station Headwalls		24-Jul-12 A	22-Nov-13 A	100%	15,400,000.00	15,500,000.00	15,500,000.00	15,500,000.00	1.00	1.00
4th St and Market Compensation Grouting		25-Feb-13 A	06-Feb-14	97.95%	365,000.00	365,000.00	357,500.00	315,590.00	0.98	1.13
Southbound Tunneling		27-Apr-13 A	26-Jun-14	53.93%	23,100,000.00	16,524,390.24	13,590,000.00	13,590,000.00	0.82	1.00
Northbound Tunneling		11-Jul-13 A	11-Sep-14	40.29%	23,850,000.00	14,468,292.68	10,456,000.00	10,456,000.00	0.72	1.00
Ellis St Compensation Grouting		31-Jul-12 A	14-Feb-14	96.08%	3,065,000.00	3,065,000.00	2,945,000.00	2,759,107.00	0.96	1.07
Green St Compensation Grouting		05-Aug-13 A	21-May-14	84.88%	1,626,450.00	1,491,450.00	1,380,493.52	1,455,376.00	0.93	0.95
Retrieval Shaft		20-Aug-13 A	30-Oct-14	36.55%	4,730,000.00	7,835,899.36	3,890,015.00	1,731,000.00	0.50	2.25
Cross Passage 1-5		08-Mar-14	09-Oct-14	0%	4,871,640.00	690,000.00	0.00	0.00	0.00	0.00
Portal Structure		10-Oct-14	21-May-15	0%	8,300,000.00	0.00	0.00	0.00	0.00	0.00
Contract Close Out		16-May-14	18-Jun-15	0%	2,025,000.00	0.00	0.00	0.00	0.00	0.00

Work Complete (con't)

- ❖ General Conditions weighted at 33% of the \$ value of Work - -- includes procurement of TBM machines & pre-cast concrete segments
- ❖ Launch Box represents 22% of the \$ value of Work --- work is complete
- ❖ UMS & YBM Headwalls and Compensation grouting installation represents an additional 11%¹ of the \$ value of work --- at or near 100% complete
- ❖ Tunneling represents 20% of \$ value of the Work --- Southbound is at 68% complete with Northbound at 51% physical complete

¹ Does not include Green St

Satisfactory Progress

- ❖ Contractor has successfully launched both TBM's
- ❖ Both SB & NB TBMs have successfully crossed under BART with no appreciable settlement.
- ❖ NB TBM is currently at anticipated production rates as shown in the baseline schedule.
- ❖ Green Street Shaft is complete.

No Specific Cause for Greater Withholding (Concerns)

- ❖ Seal failure on thrust rams for both SB and NB TBM's
 - ❖ SB 11/16 affected (1 left to be replaced)
 - ❖ NB 9/16 affected (1 left to be replaced)

- ❖ 40 days behind schedule – No approved Recovery Schedule
 - ❖ Working with BIH on alternate schedules (partnering)

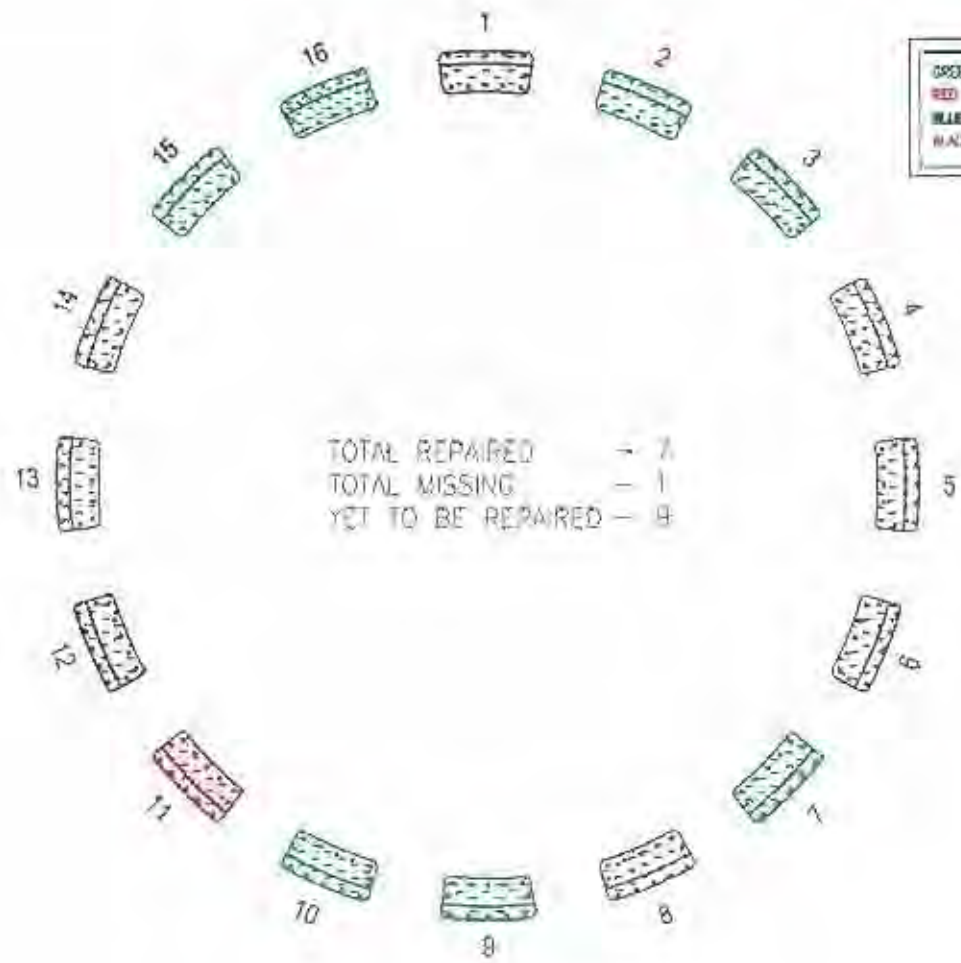
- ❖ Timely completion of Retrieval Shaft
 - ❖ Retrieval Shaft work to be monitored

- ❖ Cross Passages have not started construction
 - ❖ Cross passage submittals being received

Consent from Surety

DISCUSSION

SOUTHBOUND TBM



GREEN - REPLACED AND INSTALLED W/ NEW SEAL DESIGN
RED - UNINSTALLED - THE THRUST RAM IS CURRENTLY IN THE POSITION
BLUE - THRUST RAM SHOWING SIGNS OF FAILURE
BLACK - ORIGINAL CYLINDER SHOWING NO SIGNS OF FAILURE

TOTAL REPAIRED = 7
 TOTAL MISSING = 1
 YET TO BE REPAIRED = 8

Cylinder No.	Status	Cylinder No.	Status
1	OK - Temporary	8	Reinstalled, Replaced 12/15/14. COMPLETE 1/15/14
2	OK - Replaced and repaired COMPLETE 2/21/14	9	Replaced 11/26/2013. COMPLETE 1/14/14
3	OK - Replaced 12/25/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	10	Replaced 2/13/14
4	OK - Temporary	11	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14
5	OK	12	OK - Temporary
6	OK	13	Replaced and replaced with new temporary cylinder 1/15/2014. Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14
7	Replaced 11/26/2013. COMPLETE 1/15/14	14	Replaced and replaced with new temporary cylinder 1/15/2014. Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14
8	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	15	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14
9	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	16	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14

NORTHBOUND TBM



TOTAL REPAIRED = 8
 TOTAL MISSING = 1
 YET TO BE REPAIRED = 8

Cylinder No.	Status	Cylinder No.	Status
1	OK - Temporary	9	OK - Temporary
2	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	10	Replaced 11/26/14. COMPLETE 1/15/14
3	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	11	Replaced Cylinder 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14
4	OK - Temporary	12	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14
5	OK - Temporary	13	OK - Temporary
6	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	14	OK
7	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14	15	OK
8	OK - Temporary 1/15/14. COMPLETE 1/15/14	16	Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. Replaced 11/26/2013. Replaced w/ repaired cylinder 1/15/2014. COMPLETE 1/15/14



CITY AND COUNTY OF SAN FRANCISCO
 MTA
 CENTRAL SUBWAY TUNNELS 1252
 THRUST CYLINDER STATUS
 NB & SB TUNNELS

SHEET NO: 0221-061
 SHEET: 1
 REV:

Date: 3/31/2014
 Scale: AS NOTED

CMB Change No.: CMB - 151

Initial Implementing Change Control Procedure No.: 1252 - PCC - 020

GENERAL

Proposed Change Sponsor: A. Clifford Received by CMB: 03/12/2014
(Date)

Affected Disciplines: Utilities

Impacts of Change Retrieval Shaft 3x5 Sewer Changes



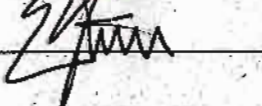

Proposed Contract Change (PCC 020) Amount Not To Exceed
\$10,000

Contract(s) Directly Affected by this Proposed Change:

1250	1251	1252	1253	1254	1255	1256
1 <input type="checkbox"/>	2 <input type="checkbox"/>	3 <input checked="" type="checkbox"/>	4 <input type="checkbox"/>	5 <input type="checkbox"/>	6 <input type="checkbox"/>	7 <input type="checkbox"/>
(CP01)	(CP02)	(CP03)	(CP04)	(CP05)	(CP06)	(CP07)

CONFIGURATION MANAGEMENT BOARD APPROVALS

Signatures

	Agree with the Change	Disagree with the Change	Date
Program Director:			3-12-14
Deputy Program Manager:			3/12/14
PM Project Services:			12 MAR 14
PM Project Construction:			
SFMTA O & M Manager:			
SFMTA Safety and Security			
SFCTA PMO			12 MAR 14

Comments

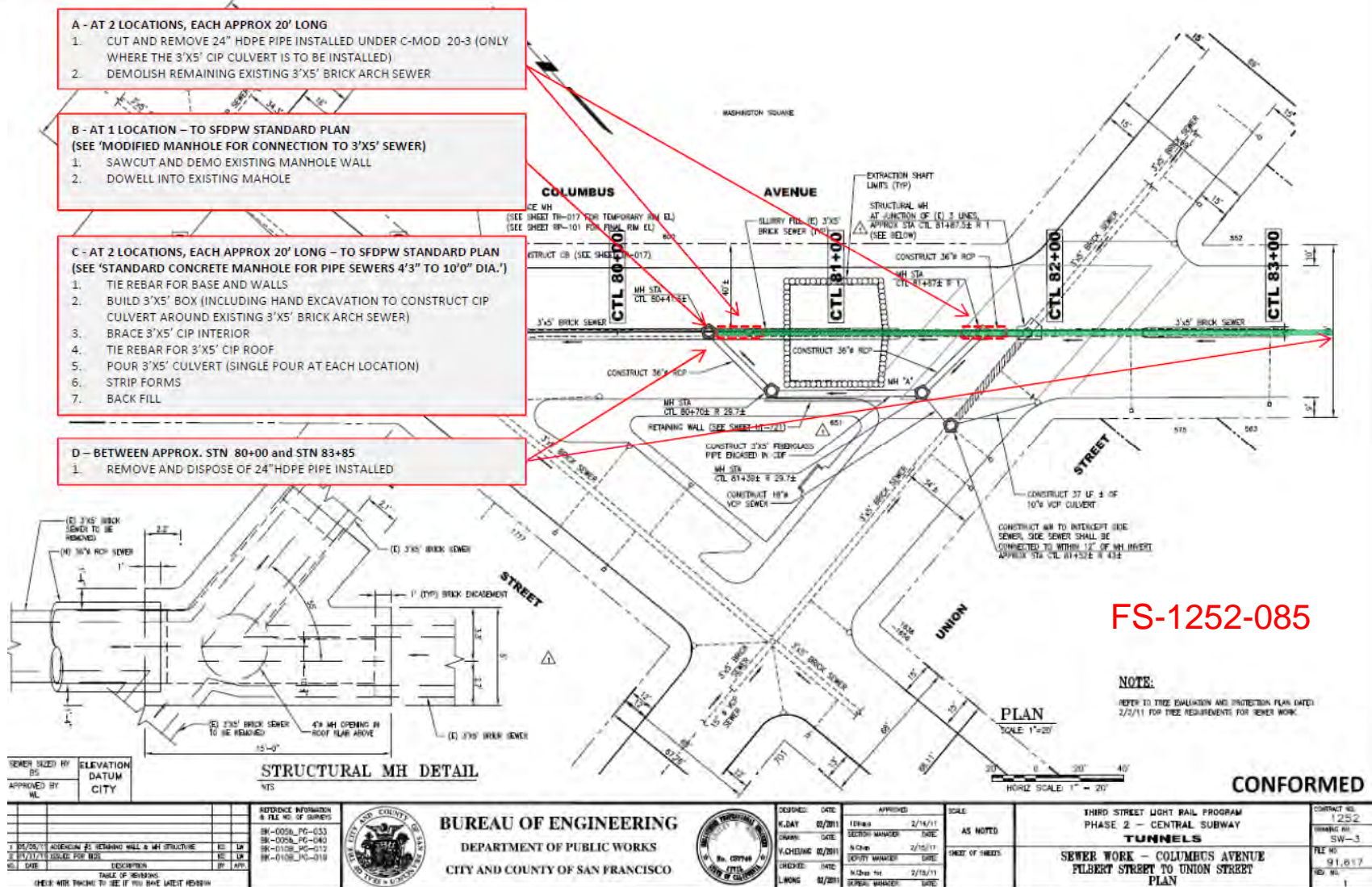
Contractor's written cost for standby required to complete negotiations.

Contract 1252

North Beach Sewers

central T subway

Connecting people. Connecting communities.



FS-1252-085

NOTE:
REFER TO TREE EVALUATION AND PROTECTION PLAN DATED 2/2/11 FOR TREE REQUIREMENTS FOR TENDER WORK.

CONFORMED

DESIGNED BY	ELEVATION DATUM
BC	CITY
APPROVED BY	
ML	

STRUCTURAL MH DETAIL

NO.	DESCRIPTION	DATE	BY	APP.
1	ADDENDUM #2 REMAINING WALL & MH STRUCTURE	02/20/11	ML	BC
2	REVISED TO ADD 3'x5' CIP CULVERT	02/20/11	ML	BC

REFERENCE INFORMATION & FILE NO. OF SURVEYS
BM-0028_PG-033
BM-0028_PG-040
BM-0108_PG-012
BM-0108_PG-019


BUREAU OF ENGINEERING
DEPARTMENT OF PUBLIC WORKS
CITY AND COUNTY OF SAN FRANCISCO



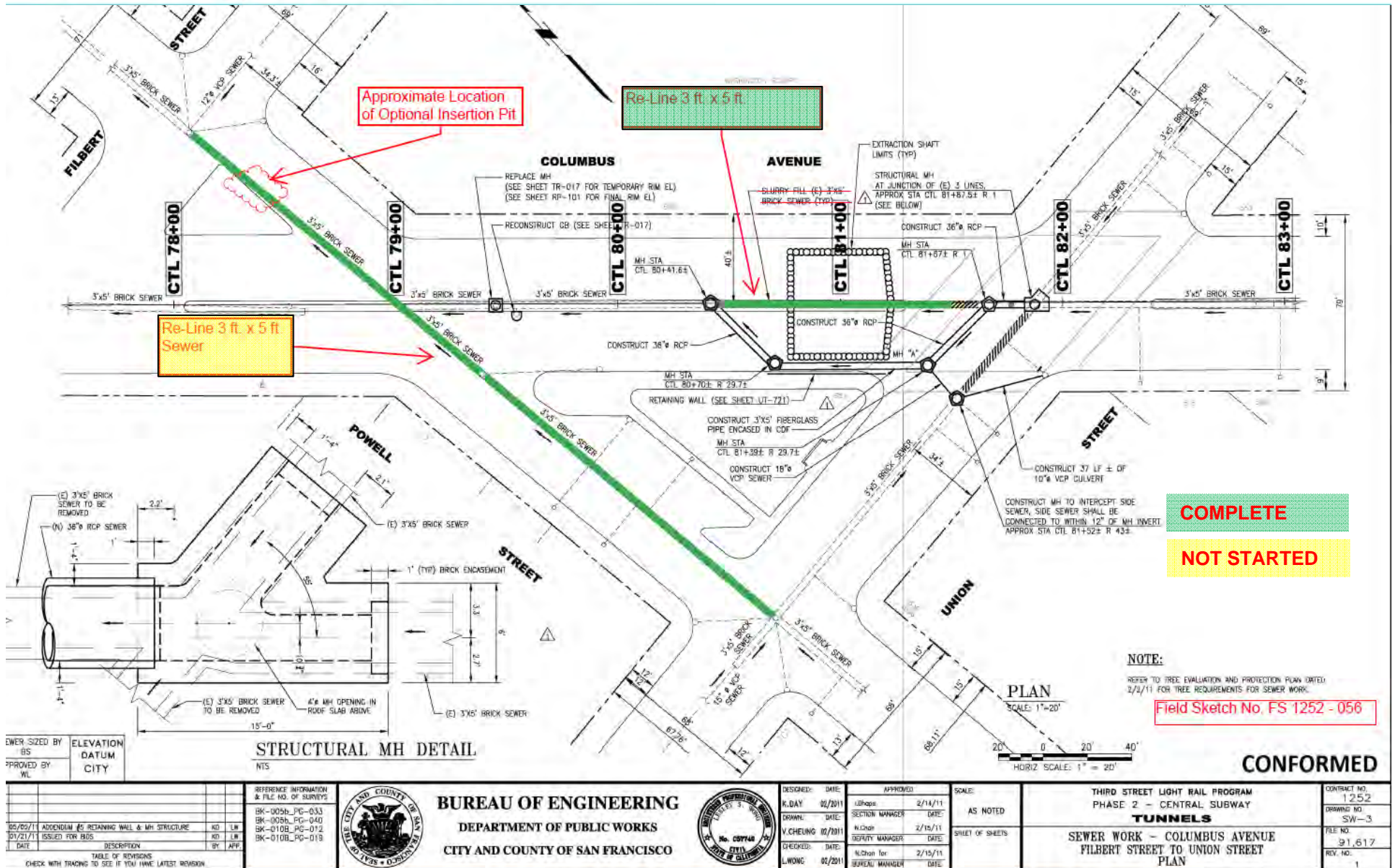
DESIGNED	DATE	APPROVED	SCALE
K. DAY	02/20/11	L. HERR	AS NOTED
DRAWN	DATE	DESIGN MANAGER	SHEET OF SHEETS
V. CHONG	02/20/11	N. CHEN	1 OF 3
CHECKED	DATE	CITY MANAGER	
L. WONG	02/20/11	N. CHEN	

THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY TUNNELS	CONTRACT NO. 1252
SEWER WORK - COLUMBUS AVENUE PULBERT STREET TO UNION STREET PLAN	ISSUING NO. SW-3
	FILE NO. 91.617
	REV. NO. 1

FACO-010 / PCC-20 - SCOPE

 BARNARD IMPREGILO HEALY JOINT VENTURE		21-Feb-2014
<p>420 Fourth Street San Francisco, CA 94107, PH (415) 546-0799, FX (415) 546-3822 CONTRACT 1252: Third Street Lightrail Program Phase 2 - Central Subway Project</p> <p>Change Order No. XXX: Ret Shaft - 3x5 Sewer Restoration (CIP culvert option)</p>		
Conflict - LS Summary		
01. BIH Direct Cost Impacts		
Labor	\$	-
Equipment	\$	-
Material	\$	-
Subcontractors	\$	-
Markup	\$	9,780.90
Bond/Insurance (1.68%)	\$	3,450.70
<i>Subtotal 01</i>	\$	13,231.60
02. Synergy Direct Cost Impacts		
Labor	\$	55,248.00
Equipment	\$	79,583.60
Material	\$	31,706.96
Subcontractors	\$	-
Markup	\$	24,980.78
Bond/Insurance	\$	4,098.71
<i>Subtotal 02</i>	\$	195,618.05
TOTAL AMOUNT		\$ 208,849.66

central T subway



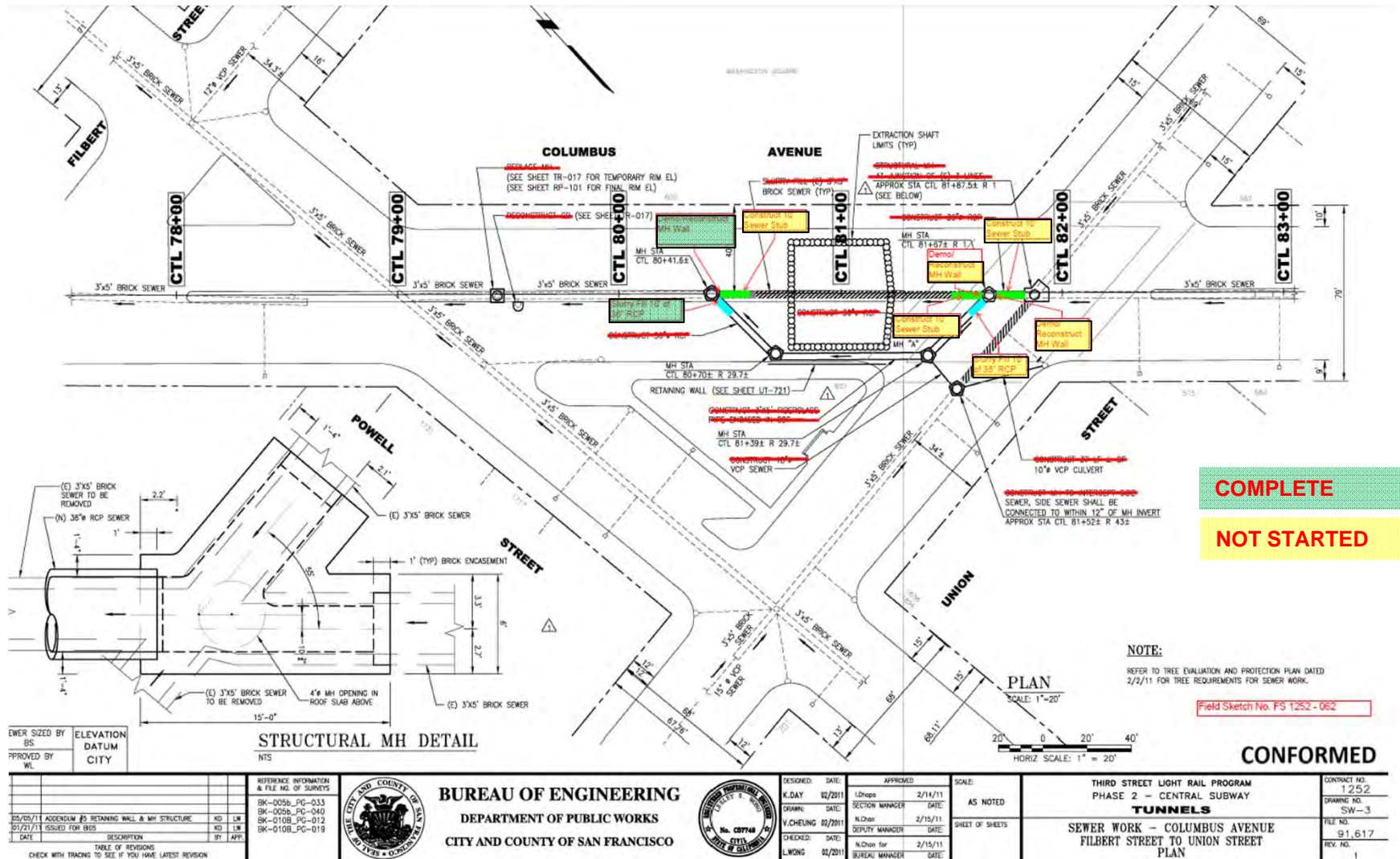
CMOD 20 – SCOPE STATUS

SFMTA

Municipal Transportation Agency

central T subway

Connecting people. Connecting communities.



CMOD 21 – SCOPE STATUS

No. Activity	Credit Factor	Original Days	Hourly rates:								Labor totals	Foreman Truck	Tool Truck	Air Compressor with hammer	Fusion Machine 824 + Gen.	Concrete Saw	3-Axel Truck	Backhoe	Breaker only	Mini Excavator	Fusion Machine 618	Paver	Cat PR275	Roller	Arrow Boards	Road Signs	Traffic Cones 100	Steel Plates	Hydraulic Jacks	Confined Space Equipment	Equipment Totals												
			Credit Days	67.03	67.06	49.78	38.22	26.89	0.00	83.12																						83.12	30.62	102.60	13.63	66.84	62.87	42.26	63.79	43.75	47.79	346.34	40.82
Traffic Control																																											
Per day Traffic Control (credit)	NA	10.50	-15.6667								1	2																															
Sewer																																											
Modify Manhole (x3 walls demo / restore)																																											
Excavate / Shore Side of Manhole (credit x2)	-0.67	3.00	-2.00	1	1	2																																					
Inferior Saw cut Wall (credit x2)	-0.67	1.50	-1.00	1	1	2																																					
Demo Wall (credit x2)	-0.67	2.00	-1.33	1	1	2																																					
Dowell into Existing Manhole (credit x2)	-0.33	0.50	-0.33	1	1	2																																					
Install Rebar (credit x2)	-0.67	1.50	-1.00	1	1	2																																					
Form Interior and Exterior (credit x2)	-0.33	0.50	-0.33	1	1	2																																					
Pour Wall (credit x2)	-0.67	0.50	-0.33	1	1	2																																					
Strip Forms (credit x2)	-0.67	0.50	-0.33	1	1	2																																					
Sack / patch interior (credit x3)	-1.00	0.50	-0.50	1	1	2																																					
Backfill and base and trench and pave (credit x3)	-1.00	2.00	-2.00	1	1	2																																					
Construct 3x5 sewer stubs 7-10ft x3																																											
Excavate 7-10' x3 stubs (credit x3)	-1.00	1.50	-1.50	1	1	2																																					
Construct 3x5 interior form using Bender board (credit x3)	-1.00	1.00	-1.00	1	1	2																																					
Install Rebar (credit x3)	-1.00	1.00	-1.00	1	1	2																																					
Form and Brace Exterior of 3x5 sewer (credit x3)	-1.00	0.50	-0.50	1	1	2																																					
Pour 3x5 Sewer (credit x3)	-1.00	0.50	-0.50	1	1	2																																					
Strip Forms (credit x3)	-1.00	0.50	-0.50	1	1	2																																					
Backfill and base and t trench and pave (credit x3)	-1.00	1.00	-1.00	1	1	2																																					
Slurry Fill 36" Stubs																																											
Form and Fill Stubs x2 (credit x1)	-0.50	1.00	-0.50	1	1	2																																					
											Shift Duration: 8hrs																																
												\$																															
											Additional 25% Credit for reduced work hours (6hrs. Vs. 8hrs.)		\$																														
											Total		\$																														
												\$																															

Multiplied C-Mod 21 estimate by
 - factor of -0.67 for credit (where 2 of 3 manholes were not complete)
 - 1 where no work was complete

SUB	Labor	\$ (44,017.20)
	Equipment	\$ (34,026.71)
	Materials	\$ (14,448.08)
	Permits and Fees/Subs	\$ (2,000.00)
	Subtotal 1	\$ (94,491.99)
	OH&P Markup on Labor, Materials and Equipment (15%)	\$ (13,873.80)
	OH&P Markup on Permits and Fees (5%)	\$ (100.00)
Subtotal 2	\$ (108,465.79)	
SPM Bonds and Insurance (2.14%)	\$ (2,314.66)	
Subtotal 3 (Total SPM COR)	\$ (110,780.45)	
PRIME	OH&P on Subcontractor Costs (5%)	\$ (5,539.02)
	Subtotal 4	\$ (116,319.47)
	BIHV Bonds and Insurance (1.68%)	\$ (1,954.17)
TOTAL CREDIT AMOUNT	\$ (118,273.64)	

CMOD 21 – CSP CREDIT ESTIMATE

\$208,849.66	- PCC-020	– QUOTE
-\$ 86,914.50	- CMOD-20	– CREDIT ESTIMATE (CSP)
-\$ 118,273.64	- CMOD-21	– CREDIT ESTIMATE (CSP)
<hr/>		
\$ 3,661.52	- Approximate additional cost	
+\$ 42,000.00	- standby plates & barriers (TBD ROM Synergy)	
<hr/>		
\$ 45,661.52		

DISCUSSION

CMB Change No.: CMB - 0149

Initial Implementing Change Control Procedure No.: 1277 - COR # 01

GENERAL

Proposed Change Sponsor: A. Clifford Received by CMB: 03/12/2014
(Date)

Affected Disciplines: Demolition

Impacts of Change: Demolish and remove unforeseen concrete structures found under slab on grade


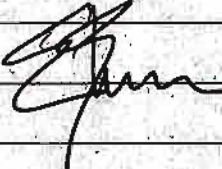

Change Order Request (COR 01) Amount Not To Exceed \$79,379.17

Contract(s) Directly Affected by this Proposed Change:

1250	1251	1252	1253	1254	1255	1256	1277
1 <input type="checkbox"/>	2 <input type="checkbox"/>	3 <input type="checkbox"/>	4 <input type="checkbox"/>	5 <input type="checkbox"/>	6 <input type="checkbox"/>	7 <input type="checkbox"/>	8 <input checked="" type="checkbox"/>
(CP01)	(CP02)	(CP03)	(CP04)	(CP05)	(CP06)	(CP07)	

CONFIGURATION MANAGEMENT BOARD APPROVALS

Signatures

	Agree with the Change	Disagree with the Change	Date
Program Director:			
Deputy Program Manager:			
PM Project Services:			12 Mar 14
PM Project Construction:			
SFMTA O & M Manager:			
SFMTA Safety and Security			
SFCTA PMO			12 MAR 14

Comments

Amount agreed to recognizing that items remain unresolved in the Contractor's mind and will be dealt with in the future change of request

COR No. 001 Cost Estimate Summary

General Scope:

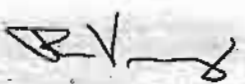
- Demolish and remove unforeseen concrete structures found under slab on grade
- Off haul mixed demolition debris found under slab on grade
- Furnish and install additional rakers to support the backyard of 732 Union Street (not shown in Contract drawings)
- Estimate excludes credit for Contract rakers not installed. Also excludes additional costs for labor premium costs related to SFMTA requested accelerated work on 8/31 and 9/2.

Total SFMTA Estimated Amount:

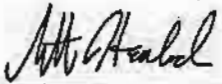
Labor (L)	\$	19,249.52
Equipment (E)	\$	8,135.04
Material (M)	\$	19,910.23
Services (S)	\$	21,974.98
Prime - LM&E Markup - 15%	\$	7,094.22
Prime - S Markup - 5%	\$	1,098.75
Insurance - 1.394%	\$	1,079.83
Bond - 1.08%	\$	836.60
Total Estimated Amount	\$	79,379.17

List of attached documents:

- Contractor Cost Proposal/SFMTA Estimated Cost Analysis
- As-built Estimate w/ CAD takeoff drawings
- Original Estimate (prepared by J. Blazek)
- Supporting invoices/photos/etc.


 Prepared By: Ben Volberding

2/26/2014
 Date:


 Checked By: Matt Hembs

2/26/2014
 Date:

1277 COR No. 001									
No.	Activity Name	Contractor Bid (MVE EE)	SFMTA Engineer's Estimate (EE)	Delta (E-EE)	Summary of Delta				
1	Labor (L)	\$ 51,620.38	\$ 35,245.52	\$ 16,374.87	-Hard to differentiate what work was additional vs. Contract in MH cost proposal. -More hours included in MH cost proposal than calculated in EE.				
1	Equipment (E)	\$ 15,230.04	\$ 8,185.04	\$ 7,045.00	-Hard to differentiate what work was additional vs. Contract in MH cost proposal. -More equipment hours included in MH cost proposal than calculated in EE.				
1	Materials (M)	\$ 55,915.05	\$ 19,910.23	\$ 36,004.82	-MH (including On Mountain disposal) fees for disposing concrete debris (~\$1,200/load). SFMTA EE uses Brisbane rate for concrete debris disposal (\$300/load).				
1	Services (S)	\$ 37,342.83	\$ 21,974.98	\$ 15,367.85	-MS including more total trucking loads than included in SFMTA EE.				
	O&P Markup (L&M-E - 15%)	\$ 20,282.57	\$ 7,094.22	\$ 13,188.35				O&P Markup (L&M-E - 15%)	\$ -
	O&P Markup (S - 5%)	\$ -	\$ 1,098.75	\$ (1,098.75)				O&P Markup (S - 5%)	\$ -
	Insurance (2.11%)	\$ 3,878.45	\$ 1,073.83	\$ 2,794.62	MH provided SFMTA with documentation supporting actual insurance markup of 1.934%			Insurance (1.934%)	\$ -
	Bond (1.0%)	\$ 2,597.60	\$ 836.60	\$ 1,761.00				Bond (1.0%)	\$ -
	Total	\$ 186,871.10	\$ 79,979.17	\$ 106,891.93				Total Unapproved Increase	\$ -

Hourly rates:

Activity Description	Quantity	Units	Production (CNTY/HR)	Days	Hourly Rates				Labor totals	Equipment Rates					Equipment Totals	
					Laborer G3	Operator G3	Apprentice Laborer P3 (Flagger)	Carpenter		Excavator JD 225D	Compactor (Ride-on roller) Wecker RD11	Loader JD 210LE	Generator	Work Truck F250		
						\$ 58.57	\$ 78.44	\$ 48.02	\$ 77.75		\$ 106.75	\$ 14.08	\$ 44.04	\$ 4.28	\$ 26.90	
						ST Rate				Active Rate						
Demolish and dispose additional concrete structures found under base contract floor slab	217.70	CY	6.00	4	2	1	1		\$ 9,784.00	1				1	\$ 5,346.00	
Backfill with Class II AB	1,019.00	TN	50.00	4	1	2	1		\$ 6,347.28		1	1		1	\$ 2,040.48	
Additional rakers: Four concrete footing	N/A	N/A	N/A	1	2				\$ 937.12				1	1	\$ 249.52	
Additional rakers: Install rakers	N/A	N/A	N/A	1	1		1		\$ 2,181.17				1	1	\$ 499.04	
									\$ 19,249.52						\$ 8,135.04	

SFMTA Notes/Assumptions:

- Estimate assume all labor at ST rate
- Quantities used are from B. Volberding CAD takeoff
- Crew/equipment spreads and production rates estimated by SFMTA 1252 Project Staff
- Estimate does not include credit for Contract rakers not installed. This credit will be estimated separately.
- Demo labor notes: 1 operator on excavator, 2 labors supporting, 1 flagger
- Backfill labor notes: 1 operator on roller, 1 operator on loader, 1 laborer supporting, 1 flagger
- Raker installation labor notes: Carpenter not required during footing installation

Material	Quantity	Units	Unit Price	Extension	SFMTA Notes
Concrete debris disposal fees	217.70	CY			Quantity from additional concrete demo takeoff
Additional volume due to bulking factor (75%)	163.28	CY			
12 CY/load	32.00	Loads	\$ 360.00	\$ 11,520.00	Disposal fee U/P used is for Brisbane dump (quote supplied by MH)
ABII Material Cost	1,019.00	TN	\$ 4.30	\$ 4,381.70	ABII U/P used is for Brisbane recycling. U/P calculated from total quantity/cost shown on 'AB2 Spreadsheet' provided by MH.
Class II AB					
Raker Materials (Non-taxed):					Lumber, steel and misc costs for raker system as designed by RP Associates (MH's engineer/designer). Prices included are all from invoices provided by MH/reviewed by SFMTA.
6x6 DFI	1	Ea	\$ 332.78	\$ 332.78	A
6x8 DFI	1	Ea	\$ 417.60	\$ 417.60	B
2x4 DFI	1	Ea	\$ 201.99	\$ 201.99	C
4x8 CD Ply	1	Ea	\$ 242.69	\$ 242.69	D
Steel Angles	1	Ea	\$ 175.98	\$ 175.98	K
Lag screws	1	Ea	\$ 34.10	\$ 34.10	M
Washers	1	Ea	\$ 1.31	\$ 1.31	N
Thru bolts	1	Ea	\$ 12.18	\$ 12.18	O
Hex nuts	1	Ea	\$ 1.04	\$ 1.04	P
Rebar	1	Ea	\$ 195.26	\$ 195.26	U
Tie wire	1	Ea	\$ 7.13	\$ 7.13	S
Vinyl sinkers	1	Ea	\$ 16.01	\$ 16.01	T
Tax (8.75%)	1	LS	\$ 143.33	\$ 143.33	
Raker Materials (Taxed):					
Epoxy	1	Ea	\$ 619.99	\$ 619.99	J
Steel Angles	1	Ea	\$ 145.51	\$ 145.51	L
1/2" AT x 36"	1	Ea	\$ 14.79	\$ 14.79	Q
Concrete	1	Ea	\$ 1,446.84	\$ 1,446.84	R
				\$ 19,910.23	

Services	Qty.	Unit	Unit Price	Extension	SFMTA Notes
Soil survey	1	LS	\$ 830.00	\$ 830.00	Material testing required for soil under basement slab. Contained in 1ea Phoenix Environmental invoice supplied by MH (Invoice #: 2691).
Additional engineer inspection time	4	Hours	\$ 130.00	\$ 520.00	Additional inspection time for Contractor's EOR. Inspected parapet wall, wall between Pagoda and Pelligrini's and wall between Pagoda and 732 Union Street (where additional rakers were required) (Hourly rate shown in invoice #: 13001341).
Revised raker design	1	LS	\$ 5,220.00	\$ 5,220.00	For rakers not shown in Contract drawings. Contained in 1ea RP Associates invoice supplied by MH (Invoice #: 13001340).
Subcontracted welding	1	LS	\$ 1,772.48	\$ 1,772.48	Outside welding required for additional rakers. Scope included welding of threaded rod onto existing structural steel beams to connect wooden raker. Contained in 2ea Tom's Metal invoices supplied by MH (Invoice #4170722552 and #4170722572).
TRUCKING:					
Trucking - Removal of concrete debris	32.00	Loads			Load calculation shown on material cost estimate sheet
Trucking time	48.00	Hours	\$ 95.00	\$ 4,560.00	1 hour trip TO Brisbane + 0.5 hour load/unload time. Does NOT include time to return after loading. Hourly cost based on Foxcover and Dominguez and Sons invoices provided by MH.
Trucking - Import ABII base	1,019.00	TN			
Total loads	51.00	Loads			1,019 tons at 20 tons/load
Shared trucking (removal of concrete debris AND Import ABII base in one trip)	32.00	Loads			Same number of loads as import ABII line item
	48.00	Hours	\$ 95.00	\$ 4,560.00	1 hour trip FROM Brisbane + 0.5 hour load/unload time. Does NOT include time to travel to Brisbane before loading. Hourly cost based on Foxcover and Dominguez and Sons invoices provided by MH.
Import only trucking (trucking time for solely importing ABII to jobsite)	19.00	Loads			Remaining ABII Import loads above concrete offhaul loads
	47.50	Hours	\$ 95.00	\$ 4,512.50	2 hour round trip to/from Brisbane + 0.5 hour load/unload time. Hourly cost based on Foxcover and Dominguez and Sons invoices provided by MH.
				\$ 21,974.98	

LABOR RATES: CENTRAL SUBWAY PROJECT

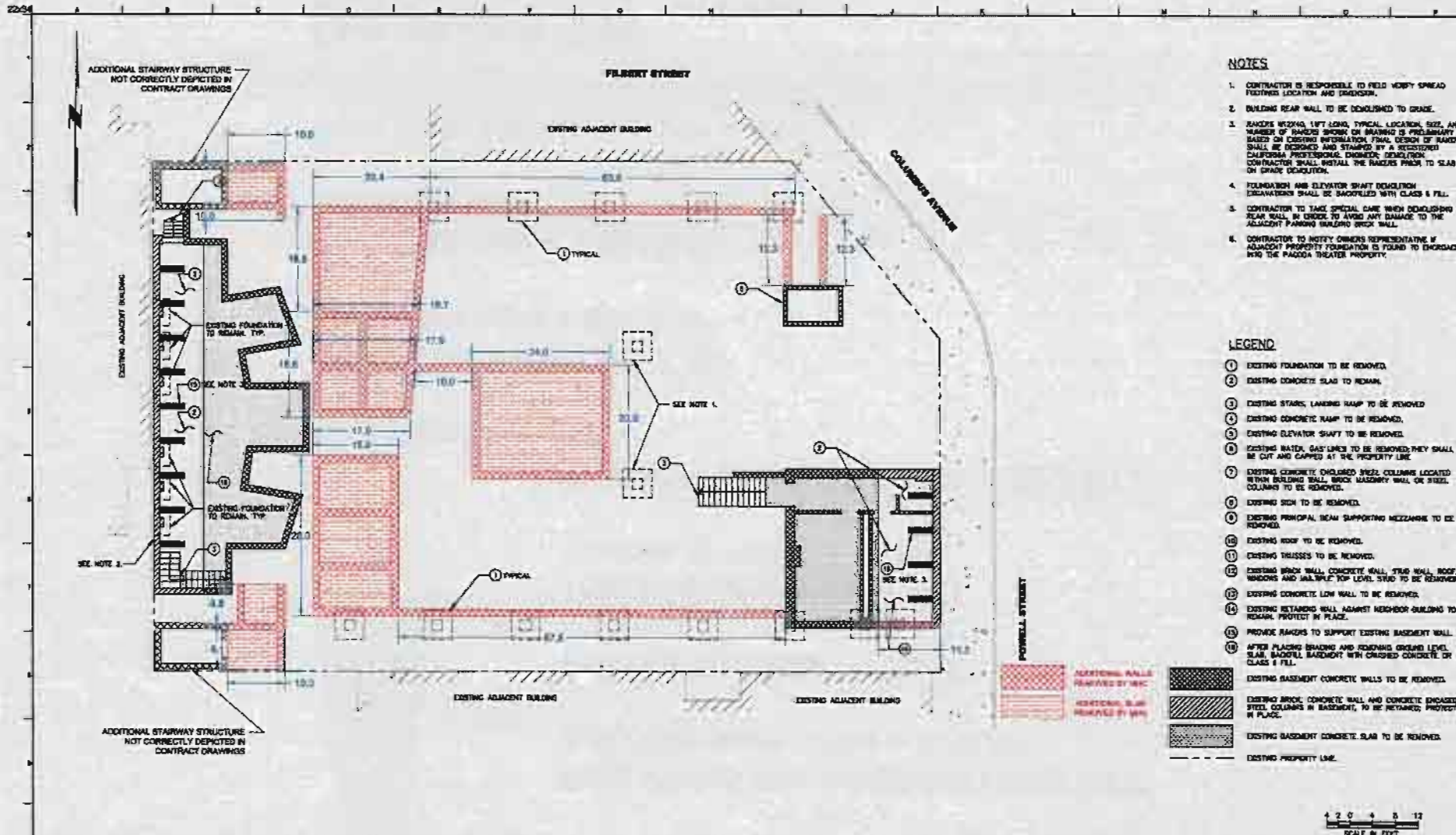
CONTRACTOR: MH Construction

LABOR CLASSIFICATION -

	Calculated by SFMTA Central Subway Staff (not on MH labor rate submittal)													
	Laborer - Group 3 ST	Laborer - Group 3 OT (x1.5)	Laborer - Group 3 DT (x2.0)	Operator Engineer - Group 3	Operator Engineer - Group 3 OT (x1.5)	Operator Engineer - Group 3 DT (x2.0)	Apprentice Laborer - Period 3 ST*	Apprentice Laborer - Period 3 OT (x1.5)*	Apprentice Laborer - Period 3 DT (x2.0)*	Carpenter - Area 1 ST**	Carpenter - Area 1 OT (x1.5)**	Carpenter - Area 1 DT (x2.0)**		
BASE INCOME														
BASE HOURLY RATE	27.64	41.46	55.28	38.01	54.02	72.02	20.73	31.10	41.46	37.50	56.25	75.00		
TAXABLE VACATION	2.63	2.63	2.63	3.72	3.72	3.72	2.63	2.63	2.63	2.40	2.40	2.40		
TAXABLE OTHER	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
BASE INCOME SUBTOTAL	\$30.27	\$44.09	\$57.91	\$39.73	\$57.74	\$75.74	\$23.36	\$33.73	\$44.09	\$39.90	\$58.65	\$77.40		
FRINGE BENEFITS														
HEALTH & WELFARE	6.64	6.64	6.64	12.63	12.63	12.63	6.64	6.64	6.64	9.55	9.55	9.55		
PENSION / ANNUITY	8.96	8.96	8.96	9.92	9.92	9.92	8.96	8.96	8.96	10.55	10.55	10.55		
VACATION / HOLIDAY	2.63	2.63	2.63	3.72	3.72	3.72	2.63	2.63	2.63	2.40	2.40	2.40		
TRAINING	0.39	0.39	0.39	0.62	0.62	0.62	0.39	0.39	0.39	0.63	0.63	0.63		
OTHER PAYMENTS	0.73	0.73	0.73	0.34	0.34	0.34	0.73	0.73	0.73	1.85	1.85	1.85		
FRINGE BENEFITS SUBTOTAL	\$19.35	\$19.35	\$19.35	\$27.23	\$27.23	\$27.23	\$19.35	\$19.35	\$19.35	\$24.98	\$24.98	\$24.98		
LESS: TAXABLE VACATION	-\$2.63	-\$2.63	-\$2.63	-\$3.72	-\$3.72	-\$3.72	-\$2.63	-\$2.63	-\$2.63	-\$2.40	-\$2.40	-\$2.40		
FRINGE BENEFITS (Non-taxable)	\$16.72	\$16.72	\$16.72	\$23.51	\$23.51	\$23.51	\$16.72	\$16.72	\$16.72	\$22.58	\$22.58	\$22.58		
GOVERNMENT BURDENS														
FEDERAL - SOCIAL SECURITY	6.20%	1.88	2.73	3.59	2.46	3.58	4.70	1.45	2.09	2.73	2.47	3.64	4.80	
FEDERAL - MEDICARE	1.45%	0.44	0.64	0.84	0.58	0.84	1.10	0.34	0.49	0.64	0.58	0.85	1.12	
FEDERAL - UNEMPLOYMENT INSURANCE	1.20%	0.36	0.53	0.69	0.48	0.69	0.91	0.28	0.40	0.53	0.48	0.70	0.93	
STATE - UNEMPLOYMENT INSURANCE	6.20%	1.88	2.73	3.59	2.46	3.58	4.70	1.45	2.09	2.73	2.47	3.64	4.80	
CITY & COUNTY - PAYROLL EXPENSE TAX	1.50%	0.45	0.66	0.87	0.60	0.87	1.14	0.35	0.51	0.66	0.80	0.88	1.16	
GOVERNMENT BURDENS SUBTOTAL	16.55%	\$5.01	\$7.30	\$9.58	\$6.58	\$9.56	\$12.53	\$3.87	\$5.58	\$7.30	\$6.60	\$9.71	\$12.87	
INSURANCE & OTHER BURDENS														
WORKER'S COMPENSATION & SUBROGATION WAIVER	Workers Comp	20.06%	6.07	8.84	11.62	7.97	11.58	15.19	4.69	6.77	8.84	8.00	11.77	15.63
PROJECT INSURANCE	General Liability	1.05%	0.32	0.46	0.61	0.42	0.61	0.80	0.25	0.35	0.46	0.42	0.62	0.81
	Excess	0.61%	0.18	0.27	0.35	0.24	0.35	0.46	0.14	0.21	0.27	0.24	0.36	0.47
INSURANCE & OTHER SUBTOTAL		21.72%	\$6.57	\$9.58	\$12.58	\$8.63	\$12.54	\$16.45	\$5.07	\$7.33	\$9.58	\$8.67	\$12.74	\$16.87
TOTAL COST PER HOUR	\$58.57	\$77.68	\$96.79	\$78.44	\$103.34	\$128.24	\$49.02	\$63.35	\$77.68	\$77.75	\$103.68	\$129.60		

*Per Laborers' Master Agreement, Level 3 apprentice gets 75% on journeyman base pay + full benefits: http://www.nccdlaborers.org/agreements/northern_california_laborers_mia/2012lma.pdf

**Per Carpenters' Fund Master Agreement: <http://www.carpenterfunds.com/PDF/Employers/Wage/CarpenterRates-Ext.pdf>

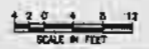


NOTES

1. CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY SPREAD FOOTING LOCATION AND DIMENSION.
2. BUILDING REAR WALL TO BE DEMOLISHED TO GRADE.
3. RAKERS BEING 1 FT LONG, TYPICAL LOCATION, SIZE, AND NUMBER OF RAKERS SHOWN ON DRAWING IS PRELIMINARY BASED ON EXISTING INFORMATION. FINAL DESIGN OF RAKERS SHALL BE DESIGNED AND STAMPED BY A REGISTERED CALIFORNIA PROFESSIONAL ENGINEER. DEMOLITION CONTRACTOR SHALL INSTALL THE RAKERS PRIOR TO SLAB OR CHAIR DEMOLITION.
4. FOUNDATION AND ELEVATOR SHAFT ISOLATION DRAINAGE SHALL BE SCHEDULED WITH CLASS 1 FILL.
5. CONTRACTOR TO TAKE SPECIAL CARE WHEN DEMOLISHING REAR WALL IN ORDER TO AVOID ANY DAMAGE TO THE ADJACENT PARSONS BUILDING BRICK WALL.
6. CONTRACTOR TO NOTIFY OWNERS REPRESENTATIVE IF ADJACENT PROPERTY FOUNDATION IS FOUND TO ENCRUST INTO THE PAGOODA THEATER PROPERTY.

LEGEND

- ① EXISTING FOUNDATION TO BE REMOVED.
- ② EXISTING CONCRETE SLAB TO REMAIN.
- ③ EXISTING STAIRS, LANDING RAMP TO BE REMOVED.
- ④ EXISTING CONCRETE RAMP TO BE REMOVED.
- ⑤ EXISTING ELEVATOR SHAFT TO BE REMOVED.
- ⑥ EXISTING WATER, GAS LINES TO BE REMOVED; THEY SHALL BE CUT AND CAPPED AT THE PROPERTY LINE.
- ⑦ EXISTING CONCRETE ENCLOSED STEEL COLUMNS LOCATED WITHIN BUILDING WALL BRICK BATTERY WALL OR STEEL COLUMNS TO BE REMOVED.
- ⑧ EXISTING SIGN TO BE REMOVED.
- ⑨ EXISTING PRINCIPAL BEAM SUPPORTING MEZZANINE TO BE REMOVED.
- ⑩ EXISTING ROOF TO BE REMOVED.
- ⑪ EXISTING TRUSSES TO BE REMOVED.
- ⑫ EXISTING BRICK WALL, CONCRETE WALL, TRAP WALL, ROOF, WINDOWS AND HALF SPLIT TOP LEVEL, STUB TO BE REMOVED.
- ⑬ EXISTING CONCRETE LOW WALL TO BE REMOVED.
- ⑭ EXISTING RETAINING WALL AGAINST NEIGHBOR BUILDING TO REMAIN; PROTECT IN PLACE.
- ⑮ PROVIDE RAKERS TO SUPPORT EXISTING BASEMENT WALL.
- ⑯ AFTER PLACING BRACING AND REINFORCING GROUND LEVEL SLAB, BACKFILL BASEMENT WITH CRUSHED CONCRETE OR CLASS 1 FILL.
- ⑰ EXISTING BASEMENT CONCRETE WALLS TO BE REMOVED.
- ⑱ EXISTING BRICK, CONCRETE WALL AND CONCRETE ENCLOSED STEEL COLUMNS IN BASEMENT, TO BE REMOVED; PROTECT IN PLACE.
- ⑲ EXISTING BASEMENT CONCRETE SLAB TO BE REMOVED.
- ⋯ EXISTING PROPERTY LINE.



ARUP
 Arup North America Ltd.
 800 Mission Street, Suite 700
 San Francisco, CA 94105, USA
 Tel: +1 415 774 2000 Fax: +1 415 774 2001
 www.arup.com

**BARNARD
 IMPREGILO
 HEALY**
 BARNARD / IMPREGILO HEALY, INC.
 480 4TH STREET
 SAN FRANCISCO, CA 94107

**THIRD STREET LIGHT RAIL
 PROGRAM PHASE 2
 CENTRAL SUBWAY
 RUNNING TUNNELS**



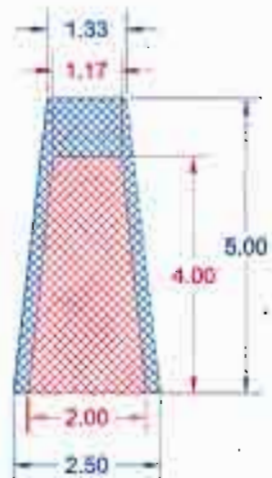
Project No.	PAGOODA THEATER DEMOLITION PLAN BASEMENT LEVEL
Date	11/19/2018
Scale	AS SHOWN
Drawn By	PRELIMINARY
Checked By	PCH
Designed By	LNH
Plot No.	218434
Sheet No.	TD-19

CONFORMED

2018 11/19/2018 10:57:00 AM 415 774 2000 415 774 2001 www.arup.com 480 4TH STREET SAN FRANCISCO, CA 94107 62019 11/19/2018 10:57:00 AM 415 774 2000 415 774 2001 www.arup.com

CONCRETE DEMO QUANTITY TAKEOFF:

Wall Demo Takeoff:



Area shaded red represents typical small wall dimensions: Area = 6.33 SF

Area shaded blue represents typical large wall dimensions: Area = 9.58 SF

Average Wall Area = 7.96 SF

Total Wall Length = 574.1 LF
(accounts for wall joint overlaps)

Total Wall Concrete Volume = 169.2 CY

Slab Demo Takeoff:

Open slab area = 1,310.5 SF

Slab under walls = 655.4 SF

Total Slab Area = 1,965.9 SF

Thickness = 8"

Total Slab Concrete Volume = 48.5 CY

Concrete Demo Total:

Total Concrete Volume = 217.7 CY

(compare to 198CY included in J. Blazek estimate)

ADDITIONAL ABII QUANTITY TAKEOFF:

ABII Backfill:

Total 4'-0" Depth ABII Area: 1,180 SF (4,720 CF)

Total 4'-6" Depth ABII Area: 2,679 SF (12,056 CF)

Total ABII Backfill Volume: 16,776 CF

ABII Ramp:

Ramp Dimensions: (L:25' W:15' H:5') [5:1 Slope]

Ramp Volume: 938 CF

Total:

Total Volume: 17,714 CF

Total Weight: 1,019 Tons

(average ABII Density (compacted to 95%): 115 LB/CF)

SFMTA Contract No. 1277

Contractor: MH Construction Management Co.

EVALUATION OF MERIT**COR 001:**

Recommendation: Accept justification of Merit for COR 001, for the following additional work:

- Demolish and remove unforeseen concrete structures found under slab on grade
- Off haul mixed demolition debris found under slab on grade
- Furnish and install additional backfill required to meet finish grade
- Furnish and install rakers to support the backyard of 732 Union Street

There is no time extension requested or recommended for this work.

Facts: On September 13th, 2013, while performing contract work for the demolition of the Pagoda Theater the contractor, MH Construction (MHC), encountered an unknown room under the existing Pagoda floor slab at the northwest corner of the site (RFI #4). MHC also submitted a separate RFI #3 indicating that there were numerous unknown objects beneath the theater floor slab containing bricks, concrete debris, and additional concrete slabs. The obstructions were not indicated on the CN 1277 design drawings and were needed to be removed prior to the follow on CN 1252 construction of Cutter Soil Mix (CSM) Walls, installation of the compensation grout pipes, and excavation of the retrieval shaft.

The SFMTA directed MHC to proceed with demolition of the unknown room and removal of the brick, concrete debris and slab. On September 16th, 2013, the SFMTA provided further direction to remove all of the obstructions within the foot print of the CSM wall construction and at the compensation grout pipes. MHC was directed to excavate a trench one foot wider than the width of the CSM walls to a depth of 4' below the existing basement slab to ensure all obstructions were removed prior to installation of the CSM panels. MHC was also directed to excavate a 4' wide trench for the compensation grout location to ensure the obstructions at those areas were also removed prior to installation of the grout pipes. The work was tracked under force account.

MHC proceeded with the activities and found additional rooms and debris under the floor slab. The material removed from beneath the existing slab needed to be sorted prior to removal from the site. Additionally, the soil removed from beneath the concrete slab contained contaminated materials and needed to be tested prior to disposal. MHC provided an alternate disposal site for the material (Ox Mountain); however, the cost for disposal of the mix material and contaminated material to Ox Mountain was significantly higher when compared with the typical disposal cost for CN 1252.

Due to the fact that MHC was already behind schedule, the decision was made for MHC to remove the mixed debris to Baylands Soil Processing for sorting and testing as to not delay the turnover of the site to the CN 1252 contract. The cost to off haul the material to Baylands is included in COR 001. The cost for the soil sorting and testing was evaluated under COR 004.

SFMTA Contract No. 1277

Contractor:

MH Construction Management Co.

Additional direction was given to backfill the excavated areas for the CSM walls and compensation grouting trenches with class 2 AB to finish grade (Elev. +59'). On September 24th the contractor completed spreading, grading and compaction AB fill throughout the entire site to finish grade. The site was turned over to CN 1252 on September 25th, 2013.

COR 001 also included the additional rakers required to support the backyard of the adjacent property 732 Union St. MHC submitted RFI #2 indicating that the backyard of the 732 Union St was 6' above the Pagoda slab and needed to be supported. MHC and their engineer provided a raker design which MHC installed against the existing brick wall of the Pagoda Theater. The work was tracked under force account. See Field Sketch FS 1277-01 for approximate location of the installed rakers.


Justification: The contract documents do not indicate the presence of the obstructions described above and therefore the obstructions are differing site conditions. City personnel have verified that the removal of the obstructions was required in order to allow for construction of the Retrieval shaft under the 1252 contract.

The contract documents do not provide for adequate support for the backyard at 732 Union St. City personnel have verified that the additional raker supports were required to support the existing brick wall in order to maintain the backyard.

RE has evaluated the conditions outlined in Article 3.04 and found that: conditions differ materially and conditions caused the scope of work to increase.

Change Type: (2) Unforeseen Condition

By:



Alex Clifford
Construction Manager



Date



Configuration Management Board
Approval



Date

CP/PCC _____

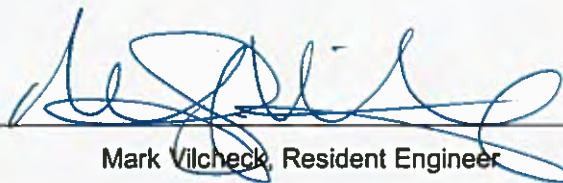
Contract No. 1300

DSC FINDING OF FACT

DIFFERING SITE CONDITIONS - REFERENCE APPROPRIATE SPECIFICATIONS IN CONTRACT GENERAL PROVISIONS AND SPECIAL PROVISIONS

1. REPORTED DATE OF OCCURRENCE Wednesday, March 5, 2014
2. DATE OF CONTRACTOR'S WRITTEN NOTICE Wednesday, March 5, 2014
3. TYPE OF WORK CONTRACTOR WAS PERFORMING Slurry Wall Test Panel Excavation, TB-1
4. NATURE OF OBSTRUCTION
Abandoned 3'x5' sewer, shown on plans to be slurry filled, but found to be hollow.
Buried ~8' below the surface of 4th St, east of slurry wall test panel, TB-1, north of TODCO driveway.
5. LOCATION OF OBSTRUCTION _____
6. METHOD USED TO OVERCOME OBSTRUCTION
3'x5' abandoned sewer would be plugged downstream of test panel and filled with controlled density fill.
7. SIGNIFICANT IMPACT ON CONSTRUCTION PROGRESS, IF ANY. (SUPPLIED BY THE CONTRACTOR)
COST TBD
TIME TBD
8. CONCLUSION, WHETHER-OR-NOT OBSTRUCTION CAN BE CLASSIFIED AS A DIFFERING SITE CONDITION (BASED ON CONTRACT DOCUMENTS).
The empty 3'x5' sewer was not identified in Contract Documents and can be classified as a differing site condition.
9. ATTACHMENTS:
SPECIFICATION _____
DRAWINGS SW-3
INSPECTION RPT _____
OTHER TPC Letter SFMTA-76

Prepared by:



Mark Vilcheck, Resident Engineer

03/11/14
Date

SFMTA Representative

Tutor Perini Corporation
530 Bush St, Suite 302
San Francisco, CA 94108
Tel: 415/638-6052

Tutor Perini
Over a century of excellence

SFMTA - 76

3/5/2014

SF Municipal Transportation Agency
821 Howard Street
2nd Floor
San Francisco, CA 94103
Attn: Richard Redmond

Reference: Third Street Light Rail Program Phase 2 – Central Subway Stations, Surface, Track and Systems, Contract No. 1300, TPC 10176000

Subject: **Notice of Differing Site Condition - YBM Abandoned Sewer Line (Not Slurry Filled)**

Dear Mr. Redmond,

Pursuant to General Provision 3.04 A.2 please consider this letter as TPC's Notice of Differing Site Conditions.

1. During the excavation of TB-1, our subcontractor, Layne Christensen Company excavated thru an abandoned brick sewer line located immediately east of TB-1. The excavation was approximately 8' deep when the clamshell hit the top corner of the sewer line which collapsed and drained the slurry from the excavation. The abandoned brick sewer was not filled with cement slurry.
2. The sewer line is shown on Drawing SW-3, Sheet 56 as an abandoned and slurry filled sewer running from 153+00 to 156+80.
3. Drawing No. SW-3, Sheet No. 56, has a Note "Remove (E) 18 Dia. PEP and 3' x 5' SLURRY FILLED BRICK SEWER (TYP.)"
4. Although the Sewer Line is abandoned, it was not slurry filled as required by plan. TPC recommends the following procedure to bring the sewer line into compliance for construction of TB-1:
 - a. Install temporary plugs in the line, north and south side of TB-1
 - b. Backfill the excavation with one sack slurry to ensure the sewer line is plugged adequately to withstand head pressure from bentonite slurry when TB-1 construction is resumed.
 - c. Wait until the following day to re-excavate and dispose of 1 sack slurry material
5. TPC recommends that SFMTA inspect the sewer line to confirm that it is abandoned but not slurry filled and issue a PCC and monitor the slurry backfill operation. TPC will prepare EWA tickets for tracking and payment purposes including stand-by time for on-site equipment.

If you have any questions, please contact the undersigned at 415-638-6052.

Respectfully,
Tutor Perini Corporation



Patrick Jennings
Project Manager

cc: File, 10-2.0076

Tutor Perini Corporation
530 Bush St, Suite 302
San Francisco, CA 94108
Tel: 415/638-6052

Tutor Perini
Over a century of excellence

SFMTA - 76

3/5/2014

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821 Howard Street
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2. The sewer line is shown on Drawing SW-3, Sheet 56 as an abandoned and slurry filled sewer running from 153+00 to 156+80.
3. Drawing No. SW-3, Sheet No. 56, has a Note "Remove (E) 18 Dia. PEP and 3' x 5' SLURRY FILLED BRICK SEWER (TYP.)"
4. Although the Sewer Line is abandoned, it was not slurry filled as required by plan. TPC recommends the following procedure to bring the sewer line into compliance for construction of TB-1:
 - a. Install temporary plugs in the line, north and south side of TB-1
 - b. Backfill the excavation with one sack slurry to ensure the sewer line is plugged adequately to withstand head pressure from bentonite slurry when TB-1 construction is resumed.
 - c. Wait until the following day to re-excavate and dispose of 1 sack slurry material
5. TPC recommends that SFMTA inspect the sewer line to confirm that it is abandoned but not slurry filled and issue a PCC and monitor the slurry backfill operation. TPC will prepare EWA tickets for tracking and payment purposes including stand-by time for on-site equipment.

If you have any questions, please contact the undersigned at 415-638-6052.

Respectfully,
Tutor Perini Corporation

A handwritten signature in blue ink, appearing to read "Patrick Jennings", is written over the printed name and title.

Patrick Jennings
Project Manager

cc: File, 10-2.0076

CP/PCC _____

Contract No. 1300

DSC FINDING OF FACT

DIFFERING SITE CONDITIONS – REFERENCE APPROPRIATE SPECIFICATIONS IN CONTRACT GENERAL PROVISIONS AND SPECIAL PROVISIONS

- 1. REPORTED DATE OF OCCURRENCE Thursday, February 27, 2014
- 2. DATE OF CONTRACTOR'S WRITTEN NOTICE Friday, February 28, 2014
- 3. TYPE OF WORK CONTRACTOR WAS PERFORMING Temporary guidewall excavation, YBM line "D"

4. NATURE OF OBSTRUCTION
Buried reinforced concrete wall, ~8" thick, greater than 10' deep, arranged in apparent rectangle
~75' north of Folsom on west side of 4th, beneath sidewalk

5. LOCATION OF OBSTRUCTION _____

6. METHOD USED TO OVERCOME OBSTRUCTION
An excavator-mounted breaker would be needed to remove the buried concrete wall interfering with guidewalls.

7. SIGNIFICANT IMPACT ON CONSTRUCTION PROGRESS, IF ANY.(SUPPLIED BY THE CONTRACTOR)

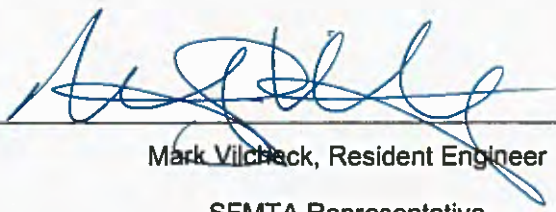
COST TBD

TIME TBD

8. CONCLUSION, WHETHER-OR-NOT OBSTRUCTION CAN BE CLASSIFIED AS A DIFFERING SITE CONDITION (BASED ON CONTRACT DOCUMENTS).
Obstruction was not identified in Contract Documents and can be classified as a differing site condition.

9. ATTACHMENTS:
- SPECIFICATION _____
 - DRAWINGS _____
 - INSPECTION RPT _____
 - OTHER TPC Letter SFMTA-72 with Photos and Drawings

Prepared by:



Mark Vilchick, Resident Engineer

03/11/14

Date

SFMTA Representative

Tutor Perini Corporation
530 Bush St, Suite 302
San Francisco, CA 94108
Tel: 415/638-6052

Tutor Perini
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SFMTA - 72

2/28/2014

SF Municipal Transportation Agency
821 Howard Street
2nd Floor
San Francisco, CA 94103
Attn: Richard Redmond

Reference: Third Street Light Rail Program Phase 2 – Central Subway Stations, Surface, Track and Systems, Contract No. 1300, TPC 10176000

Subject: **Notice of Differing Site Condition – YBM Buried Concrete Wall**

Dear Mr. Redmond,

Pursuant to General Provision 3.04 A.2 please consider this letter as TPC's Notice of Differing Site Conditions.

1. While potholing for utilities at "D" line TPC encountered a concrete wall that is not shown on the contract plans. We excavated to a depth of 10' below grade without locating the bottom of wall. The wall width is 8" and we believe the height of the buried wall is over 10'.
2. Neither Drawing No. CV-011, Demolition Plan for Lot at 4th and Folsom, or ES-100, Excavation and Ground Support General Plan show or make reference to the buried concrete wall
3. The wall is in the footprint of the guide walls required for the construction of the Slurry Wall. We have marked up the attached Drawing CV-011 indicating the location of the buried concrete wall that has been exposed. Please note that SFMTA was advised of this DSC via email on 2/27/14 and witnessed by an SFMTA Inspector.
4. TPC recommends that SFMTA inspect the exposed wall, confirm that it is an abandoned structure, issue a PCC and monitor the demolition and disposal of the buried concrete wall. TPC will prepare EWA tickets for tracking and payment purposes.

If you have any questions, please contact the undersigned at 415-638-6052.

Respectfully,
Tutor Perini Corporation



Patrick Jennings
Project Manager

cc: File, 10-2.0073







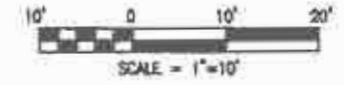
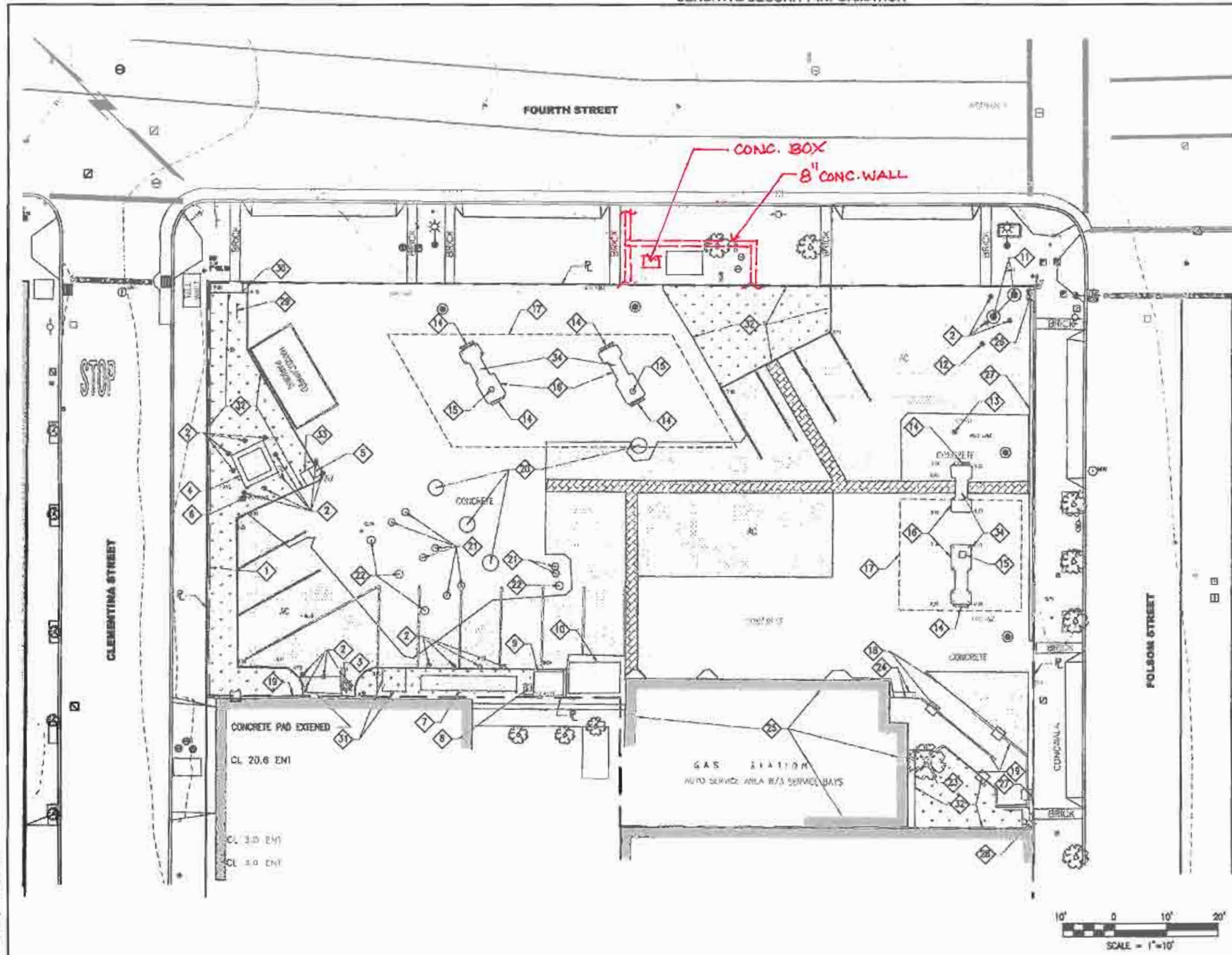








- NOTES:**
- 4.5' HIGH CHAIN LINK FENCE TO BE REMOVED
 - STEEL BOLLARDS TO BE REMOVED
 - LIGHT POLE AND CONCRETE BASE TO BE REMOVED
 - GAS VAPOR TANK IN STEEL HOUSING, ON CONCRETE PAD ALL TO BE REMOVED
 - AIR AND WATER DISPENSER ON CONCRETE BLOCK TO BE REMOVED
 - 3 STEEL VENT PIPES TO BE REMOVED
 - STEEL STRUCTURE AND CONCRETE PAD TO BE REMOVED (TIRE STORAGE)
 - VENT RISER PIPE ON CONCRETE PAD BOTH TO BE REMOVED
 - CONCRETE PAD TO BE REMOVED
 - METAL TRASH ENCLOSURE AND RAMPED CONCRETE PAD TO BE REMOVED
 - 76 SIGN, CONCRETE BASE, STEEL SUPPORTS AND FOOTINGS TO BE REMOVED
 - ELECTRICAL GROUND BOX TO BE REMOVED
 - FILL BOX TO BE REMOVED
 - STEEL U-BAR BARRIER TO BE REMOVED
 - STEEL OVERHEAD CONCRETE AND STEEL SUPPORT ENCASED IN ALUMINUM TO BE REMOVED
 - CONCRETE GAS PUMP ISLAND TO BE REMOVED
 - STEEL GAS PUMP OVERHANG TO BE REMOVED
 - CONCRETE ACCESSIBLE RAMP TO BE REMOVED
 - STEEL HANDRAIL TO BE REMOVED
 - 3' STEEL TANK LIDS (UNDERGROUND TANKS, LIDS AND RELATED FACILITIES ARE TO BE REMOVED BY OTHERS)
 - 1.3' STEEL TANK LIDS (6 TO BE REMOVED)
 - 1.5' STEEL TANK LIDS (4 TO BE REMOVED)
 - TREE TO BE REMOVED
 - CONCRETE STAIRS TO BE REMOVED
 - GAS STATION CONSTRUCTED OF STEEL TO BE REMOVED. REMOVE METAL BUILDING, SERVICE BAYS, HYDRAULIC LIFTS, CONCRETE BASE AND FOUNDATION AND ALL RELATED FACILITIES
 - 2.0'x1.5' STEEL GRATE IN TO BE REMOVED
 - STEEL SLOT DRAIN TO BE REMOVED
 - STREET LIGHT TO BE REMOVED
 - HANDICAPPED SIGN TO BE REMOVED
 - BRICK PATCH TO BE REMOVED
 - METAL CLOTHING BOX TO BE REMOVED
 - LANDSCAPING TO BE REMOVED
 - CONCRETE PAD TO BE REMOVED
 - + GAS PUMPS TO BE REMOVED



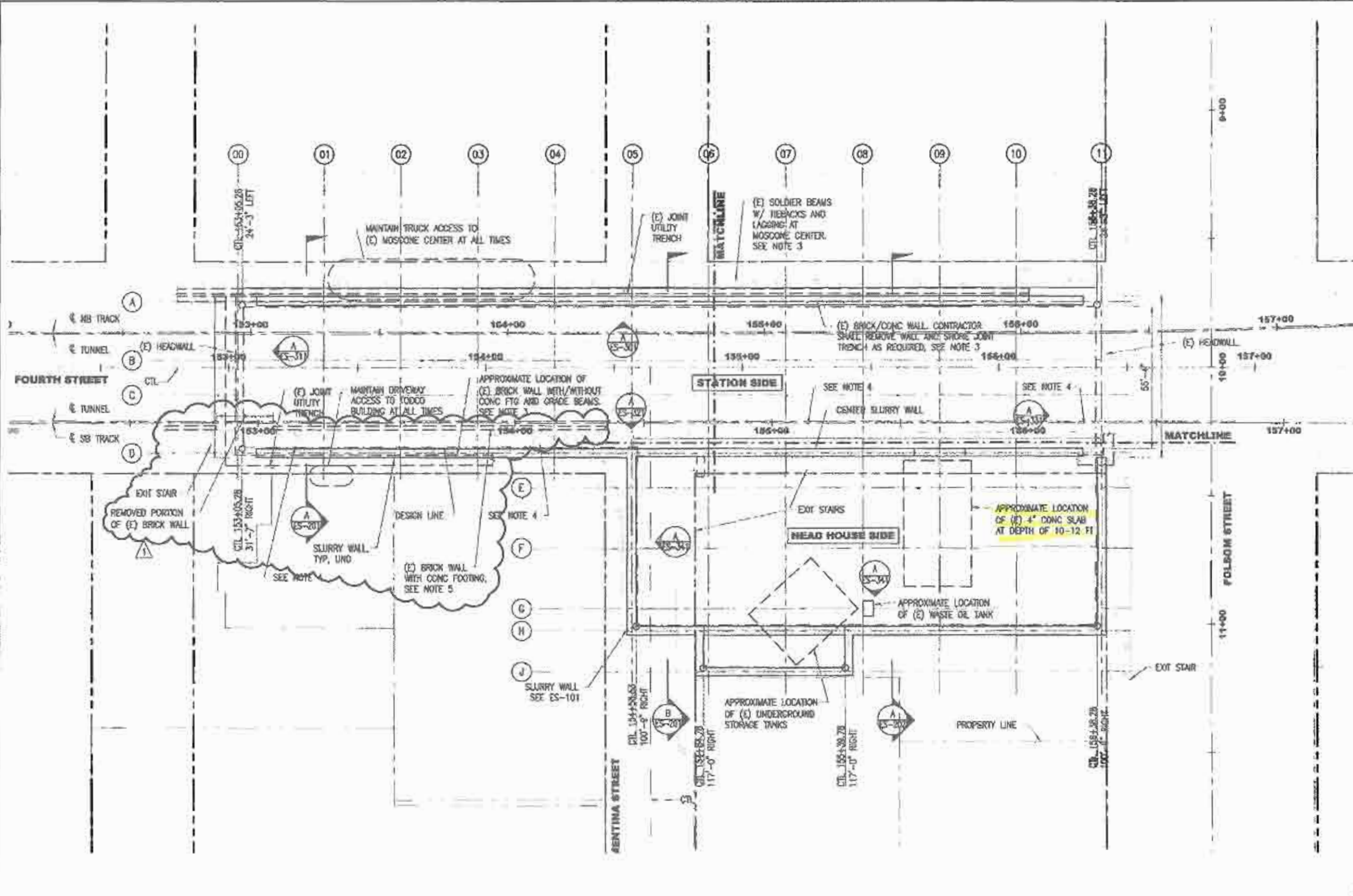
\\evdnc003\m544.1\FinalDesign\DRAWINGS\CS155-2\1255\SHEET FILES\02_CIVIL\01_CIVIL\12550201CVD11.dwg
 Kyle Fri Feb 03, 2012 - 1:50 pm CY-011

02/15/2012 ISSUED FOR BID DATE DESCRIPTION	central subway design group	APPROVED R. SHREVE J. KYLE V. HARRIS R. SHREVE A. READ R. SHREVE DATE: 02/15/2012		CITY AND COUNTY OF SAN FRANCISCO MUNICIPAL TRANSPORTATION AGENCY APPROVED FOR DIRECTOR OF TRANSPORTATION	THIRD STREET LIGHT RAIL PROGRAM PHASE 2 - CENTRAL SUBWAY MOSCONE STATION	CONTRACT NO. 1255 DRAWING NO. CL-21613
					CIVIL DEMOLITION PLAN LOT AT 4TH AND FOLSOM	SHEET NO. CV-011 OF 0

CONFORMED

WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 48 CFR PARTS 15 AND 1600. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSON WITHOUT A "NEED TO KNOW", AS DEFINED IN 48 CFR PARTS 15 AND 1600, EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION.

- NOTES**
- SEE ES-113 FOR NOTES AND LEGEND.
 - SEE 1250 CONTRACT #7 DRAWINGS FOR (E) JOINT UTILITY TRENCH.
 - SEE DRAWING ES-221 FOR EXISTING TIEBACKS AND (E) WALLS/FOUNDATIONS.
 - DCS POLE FOUNDATION. SEE STRUCTURAL DRAWINGS.
 - (E) BRICK WALL (APPROXIMATELY 11' DEEP BELOW ROAD BED) WITH CONCRETE FOOTING (APPROXIMATELY 2'-6" WIDE AND 1'-0" THICK) IS FOUND DURING THE NORTH HEADWALL INSTALLATION. APPROXIMATELY 22' LONG SECTION IS REMOVED AT HEADWALL AREA. EXTENT OF THE WALL IS NOT KNOWN.
 - SEE ES-402A FOR (E) HEADWALLS.



PLAN

FOR ORIGINAL SIGNATURES, SEE CL-21705 REV 0

\\S:\envs\003\m844\1\FinalDesign\DRAWINGS\03\1255\SHEET FILES\03_STRUCTURAL\02_EXCAVATION AND GROUND SUPPORT\12550302EST00_R1.dwg
 kasmith Thu Jun 10 2013 3:15 pm ES-100

DATE	DESCRIPTION	BY	CHKD
04/18/2013	ISSUED FOR CONTRACT 1300 APPROX 3	MM	
02/13/2012	ISSUED FOR 90		

SOHA ENGINEERS
 PROJECT: 3 C-MOSCO
 DESIGNER: F. SHEN
 CHECKER: M. WOODRUFF
 APPROVER: A. BEAD
 DATE: 02/13/2012

REV. 0
 SEALED BY
 F. KHODAYAR

CITY AND COUNTY OF SAN FRANCISCO
MUNICIPAL TRANSPORTATION AGENCY
 APPROVED
 EDWARD D. REISKIN
 DIRECTOR OF TRANSPORTATION

THIRD STREET LIGHT RAIL PROGRAM
 PHASE 2 - CENTRAL SUBWAY
 MOSCONE STATION
**EXCAVATION AND GROUND SUPPORT
 GENERAL PLAN**

PROJECT NO.	1255
DRAWING NO.	CL-21705
REVISION NO.	ES-100
REVISION	1
SHEET NO.	161

CONFORMED

CP/PCC _____

Contract No. 1300 (STS)

DSC FINDING OF FACT

DIFFERING SITE CONDITIONS - REFERENCE APPROPRIATE SPECIFICATIONS IN CONTRACT GENERAL PROVISIONS AND SPECIAL PROVISIONS

1. REPORTED DATE OF OCCURRENCE 02/25/2014

2. DATE OF CONTRACTOR'S WRITTEN NOTICE 03/04/2014

3. TYPE OF WORK CONTRACTOR WAS PERFORMING Potholing

4. NATURE OF OBSTRUCTION Found existing sub-surface layers of concrete about 12" below ground surface at 14 locations between the dates 02/25/14 and 03/10/14

5. LOCATION OF OBSTRUCTION 4th Street between Bryant and Brannan and at 4th/Bluxome

6. METHOD USED TO OVERCOME OBSTRUCTION Remedial work has not begun. Recommended method to overcome obstruction is to break and remove concrete

7. SIGNIFICANT IMPACT ON CONSTRUCTION PROGRESS, IF ANY. (SUPPLIED BY THE CONTRACTOR)

COST Rough order of Magnitude \$20,000. Exact cost TBD.

TIME Not on critical path. No time extension required.

8. CONCLUSION, WHETHER OR NOT OBSTRUCTION CAN BE CLASSIFIED AS A DIFFERING SITE CONDITION (BASED ON CONTRACT DOCUMENTS).

9. ATTACHMENTS: SPECIFICATION N/A

DRAWINGS N/A

PHOTOGRAPHS See attached photos

INSPECTION RPT See 02/25/2014 Daily Report

OTHER TPC Letter 74

SIGNATURE  TITLE Resident Engineer

3/10/14





Tutor Perini Corporation
530 Bush St, Suite 302
San Francisco, CA 94108
Tel: 415/638-6052

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SFMTA - 74

3/4/2014

SF Municipal Transportation Agency
821 Howard Street
2nd Floor
San Francisco, CA 94103
Attn: Richard Redmond

Reference: Third Street Light Rail Program Phase 2 – Central Subway Stations, Surface, Track and Systems, Contract No. 1300, TPC 10176000

Subject: **Notice of Differing Site Condition – STS Subsurface Debris**

Dear Mr. Redmond,

Pursuant to General Provision 3.04 A, please see attached correspondence from Valverde Construction, dated February 25, 2014, providing Notice of Differing Site Conditions for construction debris found in existing grade along 4th Street.

If you have any questions, please contact the undersigned at 415-638-6052.

Respectfully,
Tutor Perini Corporation



Patrick Jennings
Project Manager

cc: File, 10-2.0074

VALVERDE CONSTRUCTION, INC.
General Engineering Contractors
License No. 276469
10918 Shoemaker Ave.
Santa Fe Springs, California 90670
Tel (562) 906-1826 Fax (562) 906-1918

February 25, 2014

Tutor Perini Corporation
530 Bush St.
San Francisco, CA. 94108

Attention: Gary Elgenson

Reference: Central Subway Ph. 2 – Contract No. 1300

Subject: Notice of Differing Site Condition –Subsurface Debris at STS

Dear Gary:

Per article 3.04-A.2&3 of the General Provisions, Valverde Construction hereby notifies Tutor Perini of a differing site condition due to existing debris (brick, concrete, asphalt, and rubble) and a potential concrete street encountered under the existing street at various locations at the STS site.

Location: 4th Street eastside between Brannan and Bluxome sta. 176+35
4th Street eastside between Brannan and Bluxome sta. 176+75
4th Street westside between Bryant and Freelon sta. 172+00 (concrete street under street)
Additional locations encountered daily (pictures forthcoming and stations forthcoming)

Nature and Extent: Debris and concrete street was encountered during potholing for utilities up to 5 feet deep (see attached photos). The debris and concrete street under the street is an indicator of improperly backfilled and/or constructed subgrades.

How conditions affect work:

Debris creates additional work dealing with unstable soil which affects trench wall stability and shoring. Debris must be separated from native soil used for backfill and hauled away. Required relative compaction around debris filled unstable soil may not be achieved. The concrete street requires additional work to break out the street and haul away. Both will impede progress. Existing shallow utilities may be embedded in the concrete which creates a risk of damage when the concrete is broken up and removed. These conditions may contribute to the trench collapses encountered during our sawcutting work.

Recommendations: Monitor potholes and excavations to determine if debris encountered is isolated or widespread. Remove debris from excavations. Owner to confirm whether concrete is indeed a concrete street and provide limits

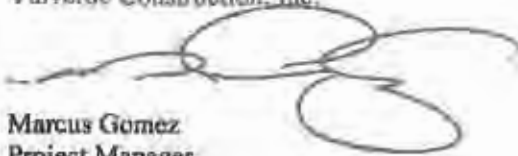
or confirm if the concrete is part of an encasement for an existing utility and define its limits.

Baseline conditions: The contract documents do not show debris under the street or a concrete street under the street. Per SFDPW and Greenbook specifications and standards as referenced in the contract specification 01 42 00.56, the subgrade below the street pavement should be and would be expected to be at 95% relative compaction and free of debris.

Valverde Construction reserves its right for compensation for any costs associated with this differing site condition.

Sincerely,
Valverde Construction, Inc.

Marcus Gomez
Project Manager

A handwritten signature in black ink, appearing to read 'Marcus Gomez', is written over the printed name and title. The signature is fluid and cursive, with several loops and a long horizontal stroke extending to the left.

IGBF15



SAFETY
Always use proper technique
and wear your seat belt







FENCE

ASPHALT









SURFACE STREET

BURIED STREET



SURFACE
STREET ↗

BURIED
STREET

